

## ADMINISTRATIVE REPORT

**Title:** DAV 25-112084\C – 455 St Mary's Rd

**Issue:** For consideration of Variances associated with the establishment of a residential and commercial mixed-use building

**Critical Path:** Director of Planning, Property, and Development

### AUTHORIZATION

Author	Department Head	CFO	CAO
Devin Clark, RPP, MCIP	n/a	n/a	

### DECISION AND CONDITIONS

The Urban Planning and Design Division **approves** the application to vary the “C2” Development and Design Standards of Winnipeg Zoning By-law No. 200/2006 as follows:

- 1) for the construction of a mixed use commercial and residential building to permit:
  - a) a front yard of 16 feet (4.88 metres) instead of 21 feet (6.40 metres);
  - b) a front yard of 16 feet (4.88 metres) instead of 21 feet (6.40 metres) to a rooftop patio;
  - c) a north side yard setback of 7 feet (2.13 metres) instead of 16 feet (4.88 metres);
  - d) a south side yard setback of 7 feet (2.13 metres) instead of 16 feet (4.88 metres);
  - e) a south side yard setback of 11 feet (3.35 metres) instead of 16 feet (4.88 metres) to a rooftop patio;
  - f) a building height of 56.5 feet (17.23 metres) instead of 49 feet (14.94 metres) for commercial uses;
- 2) for the establishment of an accessory parking area to permit:
  - a) a parking area accessed directly off a lane instead of an aisle or driveway;
  - b) insufficient buffering of a parking or loading area located within 20 feet (6.1 metres) of a side or rear lot line adjacent to a residential zoning district.

Subject to the following condition(s):

1. That, if any variance granted by this order is not established within two (2) years of the date hereof, this order, in respect of that Variance shall terminate.

#### REASON FOR THE REPORT

- The applicant is proposing to build a residential and commercial mixed use building and requires Variance[s] for the front and side yards, building height and parking location requirements of the *Winnipeg Zoning By-Law*.
- This report provides information regarding the subject application's compliance with *The City of Winnipeg Charter*, section 249.

#### FILE/APPLICANT DETAILS

**FILE:** DAV 25-112084C  
**RELATED FILES:** DCU 112079/2025C, 25-112070 ZR  
**COMMUNITY:** Riel Committee  
**NEIGHBOURHOOD #:** 5.508 - (GLENWOOD)

**SUBJECT:** To vary the "C2" Development and Design Standards of Winnipeg Zoning By-law No. 200/2006 as follows:  
1) for the construction of a mixed use commercial and residential building to permit:  
a) a front yard of 16 feet (4.88 metres) instead of 21 feet (6.40 metres);  
b) a front yard of 16 feet (4.88 metres) instead of 21 feet (6.40 metres) to a rooftop patio;  
c) a north side yard setback of 7 feet (2.13 metres) instead of 16 feet (4.88 metres);  
d) a south side yard setback of 7 feet (2.13 metres) instead of 16 feet (4.88 metres);  
e) a south side yard setback of 11 feet (3.35 metres) instead of 16 feet (4.88 metres) to a rooftop patio;  
f) a building height of 56.5 feet (17.23 metres) instead of 49 feet (14.94 metres) for commercial uses;  
2) for the establishment of an accessory parking area to permit:  
a) a parking area accessed directly off a lane instead of an aisle or driveway;  
b) insufficient buffering of a parking or loading area located within 20 feet (6.1 metres) of a side or rear lot line adjacent to a residential zoning district.

**LEGAL DESCRIPTION:** 455 St Mary's Rd  
PCL A PLAN 65064 103 ST B

**APPLICANT:** Chris Gilmour (2 Architecture Inc)

213 Notre Dame Ave Unit 1004  
Winnipeg, MB R3B 1N3

**OWNER:** MANITOBA INUIT ASSOCIATION INC.  
1000 Notre Dame Ave 3  
Winnipeg, MB R3E 0N3

## HISTORY

N/A

## DISCUSSION

### CRITERIA FOR APPROVAL

Pursuant to Section 247(3) of *The City of Winnipeg Charter*, an application for a Variance with respect to a property may be approved if the Variance:

- (a) is consistent with *Plan Winnipeg* and any applicable secondary plan;
- (b) does not create a substantial adverse effect on the amenities, use, safety and convenience of the adjoining property and adjacent area, including an area separated from the property by a street or waterway;
- (c) is the minimum modification of a zoning-by-law required to relieve the injurious effect of the zoning by-law on the applicant's property; and
- (d) is compatible with the area in which the property to be affected is situated.

### SITE DESCRIPTION

- The subject property is located on St Mary's Rd between Guay Ave. and Morier Ave., in the Glenwood neighbourhood of the St Vital ward.
- The subject site is 10,116 square feet (939.8 square metres) in area. The site is currently vacant.
- The subject site is accessed by a concrete back lane.
- Under the *Complete Communities Direction Strategy 2.0*, the site is within the Urban Mixed Use Corridor policy area. The property is zoned "C2" Commercial Community District.



Figure 1: Aerial photo of subject site and surrounding area (flown 2024)

**SURROUNDING LAND USE AND ZONING (See Figure 2)**

**North:** Commercial use zoned “C2” Commercial Community

**South:** Commercial use zoned “C2” Commercial Community

**East:** A public lane then single family dwellings zoned “R2” Residential Two-Family

**West:** St. Mary’s Road then park land zoned “PR 1” Parks and Recreation Neighbourhood



Figure 2: Zoning of the site and surrounding area.

## DESCRIPTION OF THE PROPOSED DEVELOPMENT

- The applicant intends to establish a five storey residential and commercial mixed-use building, consisting of a daycare, 12 transitional dwelling units and office space for the Manitoba Inuit Association (MIA).
- The building height is proposed to be 56 feet 3 inches with setbacks that include a 16-foot front yard, a 31-foot rear yard and 7-foot side yards.
- An open-air patio is proposed on the roof.
- The daycare space will occupy the ground floor and will be 3,300 square feet in floor area. An outdoor play area is provided at the rear of the building and will be enclosed with a 6-foot-tall wooden fence.
- Garbage pickup is identified from off the public lane and plans indicate that it will be private pick up.
- 10 parking stalls are proposed that are to be located behind the ground floor daycare use, with access from the rear lane.
  - A secure bicycle parking area is provided within the parking lot with room for 6 bicycles.
  - 2 vehicle stalls are designated for the residential uses
  - 7 stalls designated for the office uses
  - 1 stall will be provided for daycare drop off and pick up.

- Landscaping is to be provided in a manner which exceeds the Zoning By-law, which includes a minimum of 3 trees and 17 shrubs. The proposal indicates 4 trees and 25 shrubs are to be located within the front yard along St. Mary's Blvd. with additional shrubs further inset within the side yards.
- An associated Conditional Use application is required for the ground floor not entirely screening the parking at the rear of the building.

## **COLLABORATIVE PLANNING AND CONSULTATION**

### *With the Urban Planning and Design Division*

The applicant reached out to discuss the proposal in advance of the formal application being submitted.

### *With the Public*

The applicant has indicated that consultation with the adjacent neighbours was undertaken.

## **ANALYSIS AND ISSUES**

### **COMPLETE COMMUNITIES DIRECTION STRATEGY 2.0**

- The *Complete Communities Direction Strategy 2.0* (CCDS 2.0) is a citywide secondary plan that guides growth, development, and land use in the city of Winnipeg.
- The site is designated as Urban Mixed-Use Corridor under CCDS 2.0. Key goals and policies most relevant to this proposal include:

*Goal 3 Ensure Corridors provide a comfortable pedestrian environment and attractive public realm.*

#### *Urban Mixed-Use Corridors*

*3.1 Ensure that development on Urban Mixed-Use Corridors is designed to prioritize a comfortable pedestrian environment and attractive public realm through the use of design elements which may include, but are not limited to the following:*

*3.1.1 Providing minimal building setbacks from the public right-of-way to define the street edge.*

*3.1.2 Having entrances provide direct access to the public sidewalk.*

*3.1.3 Promoting a fine-grained and transparent street-level façade with active ground floor uses.*

*3.1.4 Encouraging balconies and public spaces, such as plazas, patios, or other pedestrian amenities.*

*3.1.5 Minimizing impacts of vehicular access and parking on the pedestrian environment by having vehicular access off of existing lanes.*

*3.1.6 Encouraging the location of parking above- or below-grade, or behind the primary building. Parking located beside the building may be acceptable if the parking area occupies a minimal area and high-quality screening from the public right-of-way is provided.*

*3.1.7 Promoting pedestrian-scale signage*

*3.2 Fine-grained local commercial uses and vertical mixed-use buildings with ground floor commercial uses will be encouraged.*

*3.3 Where appropriate, integrate large format retail into the streetscape for Urban Mixed Use Corridors by:*

*3.3.1 Providing minimal building setbacks from the public right-of-way to define the street edge and having entrances provide direct access to the public sidewalk.*

*3.3.2 Reinforcing a pedestrian-oriented scale by establishing a rhythm of vertical articulation using approaches such as smaller retail units, small variations in building step backs, building materials and/or colours, fenestration patterns, and canopies and signage.*

*3.4 Mitigate negative shadow impacts and ensure adequate sunlight penetration experienced by rear- and side-adjacent residential properties and the public sidewalk by:*

*3.4.1 Using Corridor right-of-way width as a guide to determine maximum building height, where appropriate.*

*3.4.2 Using upper-level building step backs, where appropriate.*

*3.5 Design the public realm and right-of-way to promote improvements to the pedestrian and cycling environment, including pedestrian- and transit-oriented streetscaping enhancements.*

*3.6 Discourage auto-oriented land uses.*

*All Corridors*

*3.10 Consider the use of tools such as local area planning, zoning, and design guidelines to further implement these design principles.*

*3.11 Promote simple, quick, direct, and universal access to transit stops, junctions, hubs, and stations through site design.*

*Goal 4 4.0 Use Corridors to provide local commercial amenities and opportunities for the surrounding neighbourhood and community. Policies Urban Mixed-Use Corridors*

*4.1 Maintain ground floor commercial space when redeveloping properties in Urban Mixed-Use Corridors.*

*4.2 Discourage auto-oriented commercial uses such as those that cater to the drive-by or traveling customer as well as those requiring outdoor storage or sales in Urban Mixed Use Corridors. Such uses may be considered only where:*

*4.2.1 The proponent can demonstrate a high degree of compliance with the design considerations articulated in the policies under Goal 3.*

*4.2.2 Negative impacts can be minimized through appropriate means.*

*4.2.3 Strong on-site pedestrian connectivity between buildings, parking areas, public sidewalks, and/or transit stops is provided.*

## CITY OF WINNIPEG ZONING BY-LAW 200/2006

- *The “C2” district is intended to accommodate more intensive commercial sites that do not have a local or neighbourhood orientation. The district is intended to include attractive commercial, institutional, recreational, limited residential and service facilities needed to support the surrounding neighbourhoods and the broader community. C2 districts are generally located in Community or Regional mixed use centres or along Community or Regional mixed use corridors.*
- Dimensional Standards for the “C2” zoning district are as follows for commercial uses:
  - Minimum Front Yard – 0 Feet
  - Minimum Rear Yard – 25 Feet
  - Minimum Interior Side Yard – 0 Feet
  - Minimum Corner Side Yard – 0 Feet
  - Maximum Building Height – 49 Feet
- Dimensional Standards for the “C2” zoning district are as follows for residential uses:
  - Minimum Lot Area – 9,000 square Feet
  - Minimum Lot Area – 800 square Feet
  - Minimum Front Yard – 25 Feet
  - Minimum Rear Yard – 25 Feet
  - Minimum Interior Side Yard – 8 feet then an additional 2 feet for every floor above the ground floor
  - Minimum Corner Side Yard – 20 feet
  - Maximum building height 70 feet

## Parking Requirements

- The Zoning By-law prescribes the following parking requirements for the proposed development.
  - For affordable housing units within a multi-family dwelling .15 stalls are required per affordable unit. No visitor parking is required and .5 bike parking stalls are required per affordable housing unit. At 11 units the total parking requirements are 2 stalls, with no visitor parking and 6 secure bicycle parking stalls are required.
  - For daycare centres 1 space is required for every 3 employees on the maximum shift. The applicants indicate that a max of 6 employees will be on shift and as such, this results in 2 stalls being required.
  - For the office uses the parking category of Social Services Facility is being used, which requires 1 stall for every 550 square feet of floor area. The floor area for the social services use is 4,666.56 square feet. As such, 8 stalls are required.

## SMALL SCALE AND LOW-RISE RESIDENTIAL DEVELOPMENT GUIDELINES FOR MATURE COMMUNITIES

- The *Small Scale and Low-Rise Residential Development Guidelines for Mature Communities (Infill Guidelines)* were adopted by Council on June 24, 2021.
- The *Infill Guidelines* do not apply to mixed use developments.

### **Alignment with Complete Communities 2.0**

The subject proposal aligns with the above applicable policies for Mature Communities in the following ways:

- The development will help achieve residential intensification targets.
- The development will positively contribute to the surrounding neighbourhood by providing additional housing options.
- The proposed development will better utilize existing infrastructure including the street network, active transportation paths to the east, water and wastewater service capacity and city services such as library, recreation and education.
- The proposed development provides necessary affordable housing.
- The proposed development meets the design policies mentioned above. Namely the building is positioned near the front and corner side property lines to create a pedestrian orientation, has a height and scale that is well below the corridor right-of-way width, uses a variety of building materials, adds to the architectural interest of the area, includes features such as balconies that increase activity along the street edge, and takes vehicular access from the rear laneway.

## VARIANCES

### *Reasons for Variances and Analysis*

The Urban Planning Division supports infill development along Urban Mixed Use Corridors because it utilizes existing infrastructure in an efficient manner, has a minimal impact on the existing form and adds a moderate increase in density.

### **Reduced Front Yard Setback**

- Where a residential structure is developed on a block where at least 80 percent of the lots are developed, the structure must be developed with a front yard consistent with the average of the existing front yards within that block or portion of the block. The average front yard setback on the subject portion of St. Mary's Road is 21 feet. The applicant is proposing a front yard of 16 feet to the residential portion of the building on the second and third storeys. For this reason, a variance is required.
- The Complete Communities Direction Strategy 2.0 encourages locating multi-family buildings at or near the front and corner side property lines in order to create a pedestrian orientation that is respectful of the area context.
- Looking at the immediate context to the north, the existing building is located right up to the front yard property line. To the south, the existing building is setback 40 feet and contains surface parking within the front yard. The proposed building lands slightly less than in the middle of these two properties.

- Further, the way that mixed use buildings are calculated based on the dimensional standards table, the residential portion is measured in a different way than the commercial portion. The “C2” district permits 0-foot front yard setbacks for commercial uses.
- Considering the above, and with consideration to the front yard setback providing space for sod, trees and shrubs, the Urban Planning Division is supportive of this variance for reduced front yard setback.

### **Reduced Side Yards**

- Side yards in the commercial zoning district are permitted down to 0 feet. However, for residential portions of the building, the side yard setbacks are required to be 8 feet plus 2 feet for every floor above the ground floor to a maximum of 20 feet. The proposed residential uses in the building are located on the second and third storey, and the roof top patio is intended to be available to both residential and commercial uses. As such, the side yard requirements are 16 feet and the proposal is for side yards of 7 feet on both the north and south elevations and for a setback of 11 feet instead of 16 feet for the rooftop patio on the south elevation.
- Policies for Urban Mixed Use corridors encourage a pedestrian oriented built form. This includes providing ground floor commercial amenities and parking at the rear of the building. Adjacent commercial uses have narrow side yards, and as such, the proposed reduced yards are considered to be compatible and supportable within the established context.
- The rooftop patio is a desirable amenity that is to be located at the front of the building facing St. Mary’s. The reduced yard setbacks to this rooftop patio of 11 feet instead of 16 feet is also supportable for the previously noted reasons.

### **Parking Accessed from Lane instead of Driveway or Aisle**

- The proposal requires a variance as parking is accessed from the lane rather than a driveway or aisle.
- The Urban Planning Division often supports parking in this configuration on urban lots where the objectives are to maximize the pedestrian orientation of the building and space is limited in the rear yard.
- For this reason, the Urban Planning Division approves this variance.

### **Buffering from Parking Area**

- Where a multi-family use abuts a residential district, buffering between the parking area and the existing residential uses should be provided. The applicant is proposing no buffering of parking located within 20 feet (6.1 metres) of a rear lot line adjacent to a residential district to accommodate the requirements for parking stalls and garbage enclosures. For this reason, a variance is required.
- The Urban Planning and Design Division is supportive of the variance to provide no buffering of parking located within 20 feet (6.1 metres) of a rear lot line adjacent to a residential district as the parking area is already established and any buffering would reduce the number of parking stalls which would necessitate an additional variance to permit fewer parking stalls than the Zoning By-Law requires. In addition, the space not used for buffering will enable space for refuse bins.

## CONDITIONS OF APPROVAL

The Urban Planning Division typically recommends a condition of plan approval for developments of this scale and on highly visible corridors. However, the use specific standard for residential and commercial mixed-use buildings in the “C2” zoning district already require this. As such, the Urban Planning Division is not recommending any conditions with the approval of the variance, with the exception of the standard two years to establish condition.

## REASONS FOR DECISION

In the context of Section 247(3), the Urban Planning and Design Division **approves** the application for the following reasons:

- (a) is consistent with *Plan Winnipeg* and any applicable secondary plan;  
*In that the application is consistent with the Complete Communities Direction Strategy 2.0 policies for Urban Mixed Use Corridors.*
- (b) does not create a substantial adverse effect on the amenities, use, safety and convenience of the adjoining property and adjacent area, including an area separated from the property by a street or waterway;  
*In that the height, setbacks and massing of the proposed project aligns with the Urban Mixed Use Corridor objectives of having the massing height fall below the right-of-way width. The right of way width is approximately 85 feet and the building height is just over 56 feet.*
- (c) is the minimum modification of a zoning-by-law required to relieve the injurious effect of the zoning by-law on the applicant's property; and  
*In that, the proposed variances would allow the applicant to introduce a building on site in a manner that is consistent with the policies guiding development under the Urban Mixed-Use Corridor policy area within the Complete Communities Direction Strategy 2.0.*
- (d) is compatible with the area in which the property to be affected is situated.  
*In that, the development is designed to be compatible within the surrounding context. Five storey buildings are deemed appropriate for site redevelopment along Urban Mixed Use Corridors.*

## CONSULTATION

In preparing this report there was internal consultation with: N/A

## OURWINNIPEG 2045 POLICY ALIGNMENT

### OurWinnipeg 2045 Goal – City Building

*A successful strategy for sustainable city growth uses a variety of planning approaches. It strikes a balance between ‘growing out’ and ‘growing up’, offering choices ranging from traditional, single-family neighbourhoods to denser forms of urban housing and new neighbourhoods designed around a mix of land uses and a sustainable transportation system (p.17).*

*Building a city of complete communities provides the following additional benefits:*

- *Minimization of environmental impacts per capita, in support of better air quality, resource efficiency, waste management, and climate change mitigation*
- *Adaptability to climate change and resilience to natural hazards*
- *Promotion of more active lifestyles and increased social interaction (p.17).*

### **City Building Objectives**

**City Building Objective 1.** Responsibly plan, prioritize and accommodate growth in areas that best support Complete Communities principles, to achieve this Plan's sustainable development goals:

*Facilitate growth and change strategically within Winnipeg's unique Transformative Areas and Established Neighbourhoods, to enhance the ability of the urban environment to contribute towards this Plan's goals.*

**City Building Objective 2.** Integrate resilient land use, transportation and infrastructure planning, and investments:

*Ensure that land use, transportation, and infrastructure planning are aligned to provide the conditions for compact, complete and connected communities, supported by sustainable transportation options and municipal infrastructure capacity.*

**City Building Objective 3.** Facilitate development opportunities that complete established communities, and plan new communities as complete and connected from the outset:

*New and existing communities are complete when they demonstrate: universally designed environments; mixed-income neighbourhoods; a continuum of housing types; multi-modal connections within and to elsewhere in the city; heritage conservation; opportunities for physical activity, social interaction, and access to healthy food, daily needs, employment, education, recreation, and green infrastructure (p.31).*

## **WINNIPEG CLIMATE ACTION PLAN ALIGNMENT**

### **Strategic Opportunity #4: Facilitate Compact, Complete Development and Increase Density**

*4.1 Increase Strategic Infill Development that Provides Access to and Capitalizes on Existing and Planned Corridors with Frequent Transit Service*

*4.2 Ensure New Areas of Growth are Designed According to the Principles of Complete Communities (Primary Responsibility: Planning, Property and Development Department)*

One of the Vision Statement's in the Climate Action Plan is that:

*Winnipeg facilitates compact development and healthy neighbourhoods that are connected through a network of active and transit-oriented mobility options. This means that the City strategically integrates climate action within both land use and transportation decision-making.*

**Strategic Opportunity #3 Advancing Sustainable Transportation – Increasing Mobility Options and Shift to Zero Emission Vehicles**

*Increasing access to mobility options and fuel switching are key directions for reducing emissions from transportation. Key directions aim to directly shift residents out of single occupancy vehicles through sustainable transportation options with lower or no*

*emissions (walking, cycling, public transit, carshare, and carpooling) or indirectly enable diverse mobility choices through the development of higher density, and more complete communities (p.vi)*

*3.1 Increase Use and Efficiency of Public Transit Systems*

*3.2 Increase the Density of Urban Development Along Key Transit Corridors.*

*Strategic Opportunity #4 Facilitate Compact, Complete Development and Increase Density*

*4.1 Increase Strategic Infill Development that Provides Access to and Capitalizes on Existing and Planned Corridors with Frequent Transit Service*

*4.3 Pursue Transit-Oriented Development*

*Strategic Opportunity #5 Low Carbon and Energy Efficient Buildings*

*In order to lower energy and greenhouse gases used in Winnipeg's new and existing buildings, the City and community need to find better ways to understand the energy use and emissions from buildings and provide builders with the tools and resources (where needed and feasible) to support these retrofits and designs.*

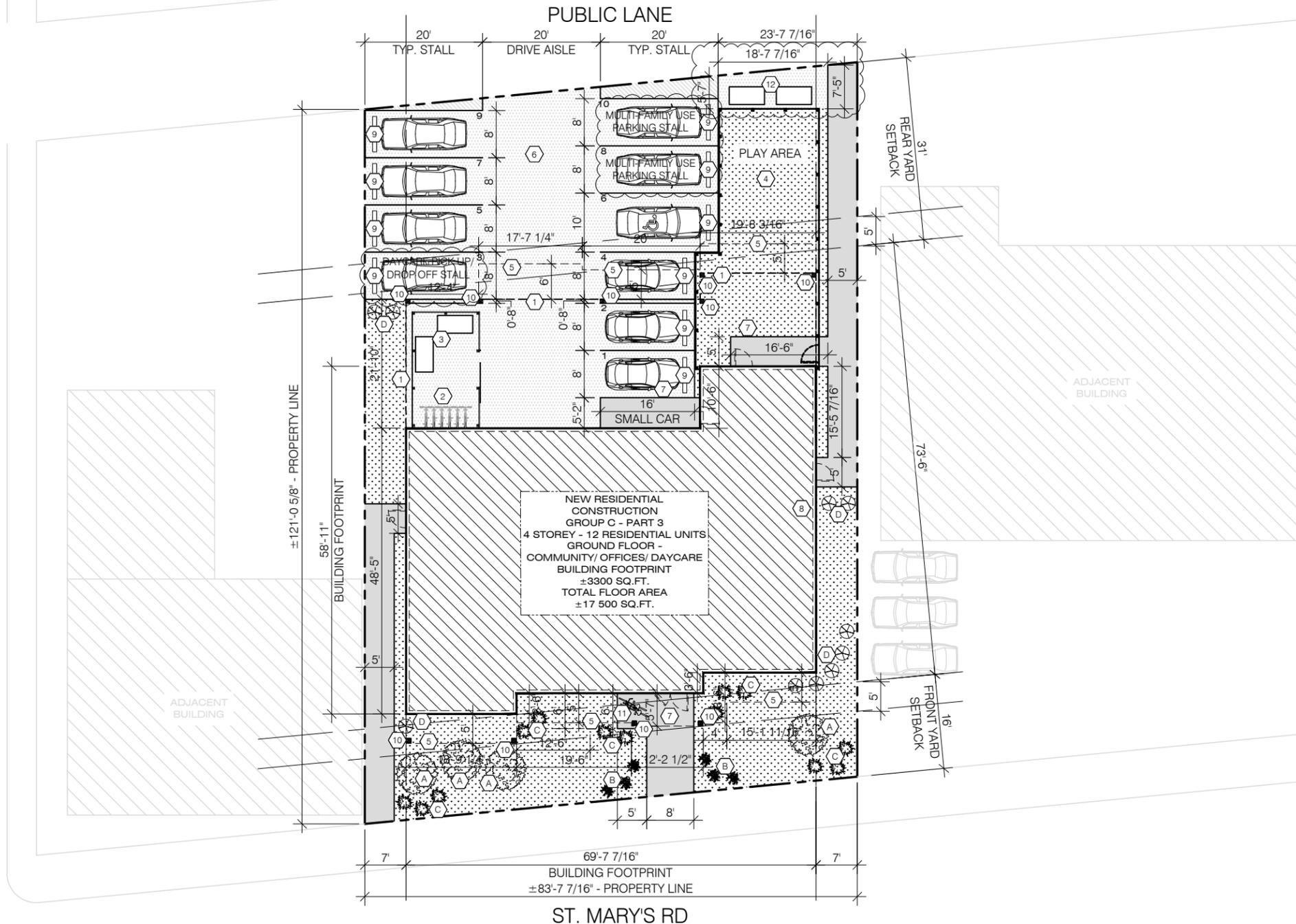
*5.2 Improve energy performance of new buildings*

## **WINNIPEG POVERTY REDUCTION STRATEGY ALIGNMENT**

Consideration was given to whether this report connects to any of the specific Goals and Objectives in the *PRS* and it was determined the *PRS* is not applicable to this specific report.

## **SUBMITTED BY**

Department: Planning, Property and Development  
Division: Urban Planning and Design  
Prepared by: Devin Clark, RPP, MCIP  
Date: Tuesday, March 11, 2025  
File No. DAV 25-112084\C



### ZONING SUMMARY

ZONED: C2  
 TYPE: NEW MIXED USE BUILDING  
 12 RESIDENTIAL UNITS, DAYCARE, OFFICE SPACE

LEVELS: 4 STOREYS  
 LOT AREA: ±940.4 SQ.M. (±10,122 SQ.FT.)  
 BUILDING FOOTPRINT AREA: ±306.6 SQ.M. (±3300 SQ.FT.)  
 BUILDING TOTAL FLOOR AREA: ±1625.8 SQ.M. (±17,500 SQ.FT.)  
 PARKING AREA: ±282.7 SQ.M. (±3043 SQ.FT.)

BUILDING HEIGHT: 17.1 M. (56'-3")

FRONT YARD (WEST): 16'-0" FT PROVIDED  
 REAR YARD (EAST): 32'-0" FT PROVIDED  
 SIDE YARD (NORTH): 7'-0" FT PROVIDED  
 SIDE YARD (SOUTH): 7'-0" FT PROVIDED

PARKING STALLS PROVIDED: 10  
 NON-BARRIER FREE PARKING STALLS: 9  
 BARRIER-FREE STALLS: 1  
 LOADING AREA: N/A  
 BICYCLE PARKING PROVIDED: 8

**STREET EDGE AND BUILDING FOUNDATION LANDSCAPING**  
 4 DECIDUOUS TREES PROVIDED (TBC)  
 29 SHRUBS PROVIDED

**SIDE AND REAR SITE EDGE BUFFERING**  
 6' HIGH OPAQUE FENCE PROVIDED AS BUFFER ON SOUTH SIDE OF PROPERTY. EXISTING OPAQUE FENCE ON NORTH SIDE.

**SOFT LANDSCAPE COVERAGE**  
 - SOFT LANDSCAPING AREA / LOT AREA: 2606 SQ.FT.  
 - 2606 SQ.FT. / 10,122 SQ.FT. = 25.7%

- NOTES CONCERNING BUILDING LOCATION:**
- THIS SITE PLAN IS BASED ON INFORMATION FROM THE SURVEYOR'S STAKING CERTIFICATE. 2 ARCHITECTURE IS TO BE ADVISED BEFORE START OF CONSTRUCTION OF ANY UN-SHOWN FEATURES ON THIS OR THE ADJACENT SITES THAT MIGHT IMPACT ON THE PROJECT EITHER DURING CONSTRUCTION OR DURING FUTURE USE.
  - THE 'NORTH' ORIENTATION REFERS TO NOMINAL NORTH RATHER THAN TRUE OR MAGNETIC NORTH.
  - ANY DIMENSIONS THAT SHOW THE LOCATION OF EXISTING FEATURES ARE APPROXIMATE ONLY, AND ARE TO BE CONFIRMED BEFORE CONSTRUCTION START AS REQUIRED BY A CERTIFIED MANITOBA LAND SURVEYOR.
  - THE CORNERS OF THE FOUNDATION FOOTPRINT ARE TO BE LOCATED ON SITE BEFORE CONSTRUCTION START BY A CERTIFIED MANITOBA LAND SURVEYOR.

- NOTES REGARDING PARKING:**
- BARRIER FREE PARKING STALLS SHALL BE CLEARLY IDENTIFIED BY PAINTING THE INTERNATIONAL SYMBOL OF ACCESSIBILITY IN WHITE ON BLUE BACKGROUND.

- NOTES:**
- ALL PARKING STALLS, DIRECTION ARROWS, HANDICAP SIGN AND THE LOADING BAY ARE TO BE PAINTED IN YELLOW.
  - IF REQUIRED AT THIS LOCATION, CONTRACTOR TO SUPPLY AND INSTALL OPTIONAL SITE EQUIPMENT, CONCRETE PATIO FURNITURE, PLANTER AND BICYCLE RACK - PROJECT MANAGER TO GIVE INSTRUCTION.

DO NOT SCALE THE DRAWINGS. NO REPRODUCTION MAY BE MADE WITHOUT THE PERMISSION OF 2 ARCHITECTURE INC. THE GENERAL CONTRACTOR AND ALL SUB-TRADES SHALL CHECK AND VERIFY ALL DIMENSIONS AND DATA NOTED HEREIN WITH CONDITIONS OF THE SITE AND SHALL BE HELD RESPONSIBLE FOR REPORTING ANY DISCREPANCIES TO THE ARCHITECT. THIS DRAWING IS NOT TO BE USED FOR CONSTRUCTION UNTIL APPROVED, SIGNED, AND SEALED IF REQUIRED.

PRELIMINARY DRAWINGS  
 CONCEPT ONLY  
 NOT FOR CONSTRUCTION

- ISSUED**
- REISSUED FOR ZONING FEBRUARY 18, 2025
  - REISSUED FOR ZONING FEBRUARY 5, 2025
  - ISSUED FOR ZONING JANUARY 29, 2025
  - ISSUED FOR CLASS B COSTING NOVEMBER 25, 2024
  - ISSUED FOR ZONING REVIEW OCTOBER 24, 2024
  - ISSUED FOR PLANNER REVIEW AUGUST 22, 2024
  - ISSUED FOR CLASS C COSTING COORDINATION AUGUST 7, 2024
  - CLIENT INITIATED CHANGE JULY 25, 2024
  - REISSUED FOR CLIENT AND CONSULTANT REVIEW JULY 11, 2024
  - REISSUED FOR CLIENT AND CONSULTANT REVIEW JULY 4, 2024
  - ISSUED FOR CLIENT AND CONSULTANT REVIEW JUNE 13, 2024

ADDRESS  
 455 ST. MARY'S RD  
 WINNIPEG, MB

PROJECT  
 MANITOBA INUIT ASSOCIATION  
 TRANSITIONAL HOUSING

SHEET TITLE  
 SITE & PARKING PLAN

SCALE  
 AS NOTED

DESIGNER  
 CG

DRAWN BY  
 JC / DD

DATE  
 FEBRUARY 18, 2025

SHEET NUMBER  
Z0.1R

### SITE PLAN KEYNOTES

- 1 OUTLINE OF BUILDING ABOVE
- 2 SECURE ENCLOSED BICYCLE PARKING (6)
- 3 ENCLOSED WASTE / RECYCLING AREA (PRIVATE COLLECTION)
- 4 PLAY AREA - ENCLOSED GREEN SPACE
- 5 OUTLINE OF BALCONY ABOVE
- 6 G.C. TO COORDINATE ELECTRICAL SERVICE PANEL CAPACITY AND INSTALLATION OF ELECTRICAL CONDUIT TO OUTDOOR PARKING SPACES.
- 7 ENTRANCE DOORS ILLUMINATED BY RECESSED SOFFIT LIGHTING
- 8 WATER METER LOCATION
- 9 CONCRETE CURB STOP
- 10 EXTERIOR HSS COLUMN
- 11 CIRCLE BICYCLE RACK. MINIMUM 2 BIKE CAPACITY. TO BE CONFIRMED WITH ARCHITECT
- 12 GARBAGE AND RECYCLING STAGING AREA FOR PRIVATE COLLECTION

**1 SITE PLAN**  
 Z0.1R 3/64" = 1'-0"

SITE DEVELOPMENT - GENERAL SITE & LANDSCAPING					
ITEM	SYMBOL	REMARKS	ITEM	SYMBOL	REMARKS
MEDIUM SIZED DECIDUOUS TREE (LOCATED MIN. 6'-6" OF A PATH) GOLDEN CASCADE LINDEN SPECIES: TILIA CORDATA SIZE AT PLANTING: ±8' - 10' TALL MATURE HEIGHT: ±40' LOWEST ACCEPTABLE BRANCH AT MATURITY: ±6' ACCEPTED CALIPER: ±2 1/2" REFER TO TREE NOTES		A	SHRUB #2 - SCHMIDT ALPINE CURRANT SPECIES: RIBES ALPINUM 'SCHMIDT' SIZE AT PLANTING: ±1'-0" X ±2'-0" 2 GALLONS AT TIME OF PLANTING MATURE HEIGHT SIZE: ±3'-0" MATURE SPREAD: ±3'-0"		C
SHRUB #1 - DANICA ARBORVITAE SPECIES: THUJA OCCIDENTALIS SIZE AT PLANTING: ±1'-0" X ±2'-0" 2 GALLONS AT TIME OF PLANTING MATURE HEIGHT SIZE: ±1'-8" MATURE SPREAD: ±1'-8"		B	SHRUB #3 - JADE CAROUSEL BARBERRY SPECIES: BERBERIS THUNBERGII SIZE AT PLANTING: ±1'-0" X ±2'-0" 2 GALLONS AT TIME OF PLANTING MATURE HEIGHT SIZE: ±3'-0" MATURE SPREAD: ±4'-0"		D
* NOTE ALL SHRUBS TO BE OF SPECIES THAT CAN REMAIN HEALTHY WHEN TRIMMED SO AS TO MAINTAIN A HEIGHT OF NOT MORE THAN 30 INCHES AT MATURITY					
<p><b>TREE NOTES:</b></p> <p>* TREES MUST BE PLANTED NOT LESS THAN 6.56 FT (2.0 M) FROM A PUBLIC SIDEWALK, A MINIMUM OF 3.28 FT (1.0 M) FROM A PROPERTY LINE, AND SHOULD BE LOCATED AT SUFFICIENT DISTANCE FROM THE DWELLING TO ALLOW FOR FULL CANOPY AND ROOT SYSTEM DEVELOPMENT.</p> <p>* NOTE PLANTING SHOWN ON SITE PLAN IS DIAGRAMMATIC. FINAL PLANTING TO BE CONFIRMED WITH OWNER.</p>					
<p><b>LANDSCAPE PAVING AREA:</b>                  CONCRETE OR ASPHALT</p>					
<p>ASPHALT SURFACE</p>					
<p>GRASS</p>					

2 architecture

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ADDRESS  
455 ST. MARY'S RD  
WINNIPEG, MB

PROJECT  
MANITOBA INUIT ASSOCIATION  
TRANSITIONAL HOUSING

SHEET TITLE  
WEST ELEVATION

SCALE  
AS NOTED

DESIGNER  
CG

DRAWN BY  
JC / DD

DATE  
FEBRUARY 18, 2025

SHEET NUMBER  
**Z2.0R**

2 architecture



**1 WEST ELEVATION**  
Z2.0R 1/8" = 1'-0"

**ELEVATION KEYNOTES**

- 200** ACRYLIC STUCCO SYSTEM - ULTRA SMOOTH (STANDARD COLOUR: WHITE)  
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SOUTH ELEVATION

SCALE

AS NOTED

DESIGNER

CG

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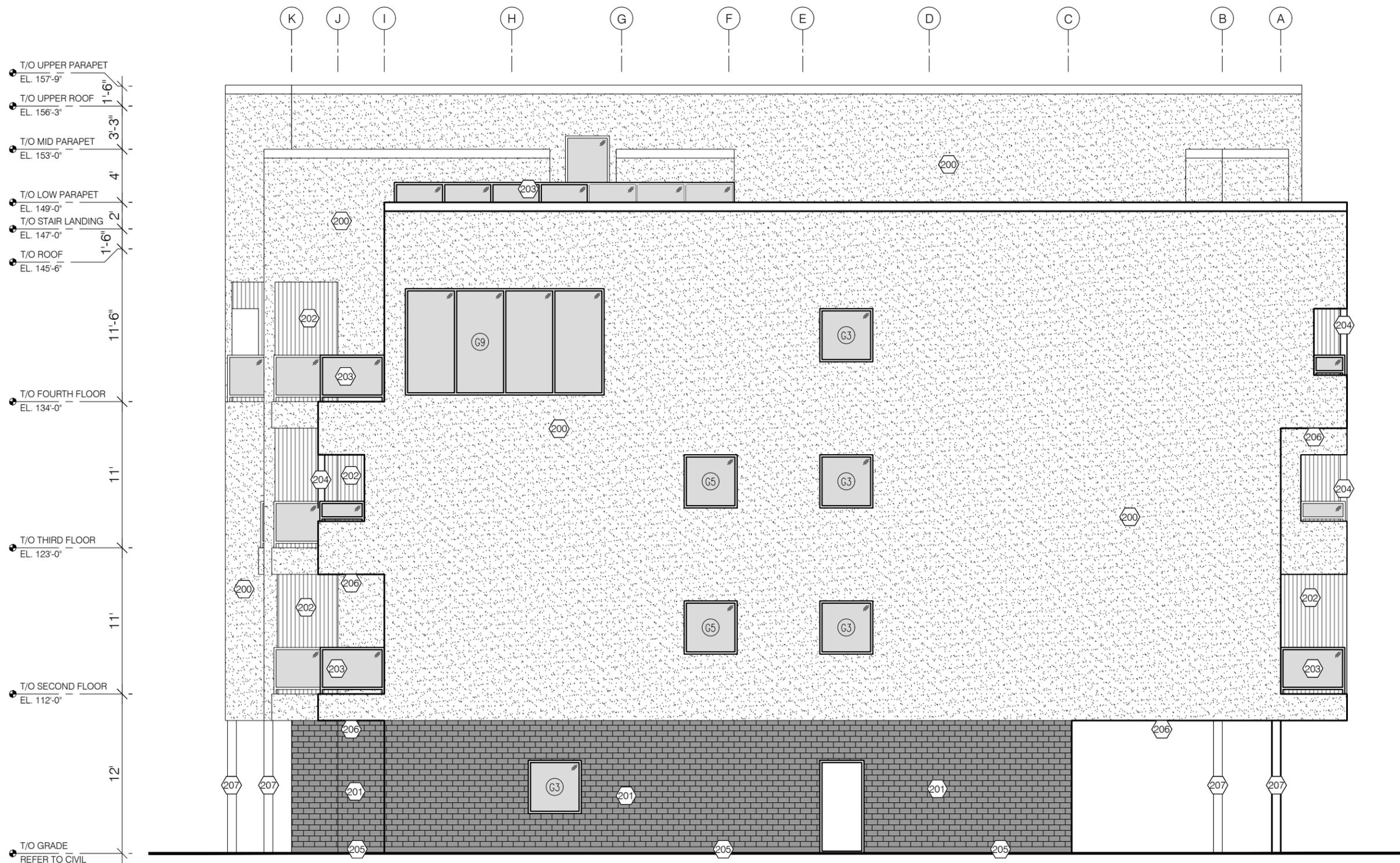
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TRANSITIONAL HOUSING

SHEET TITLE

EAST ELEVATION

SCALE

AS NOTED

DESIGNER

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JC / DD

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SHEET NUMBER

Z2.2R

2 architecture



1 EAST ELEVATION  
Z2.2R 1/8" = 1'-0"

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PROJECT

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TRANSITIONAL HOUSING

SHEET TITLE

NORTH ELEVATION

SCALE

AS NOTED

DESIGNER

CG

DRAWN BY

JC / DD

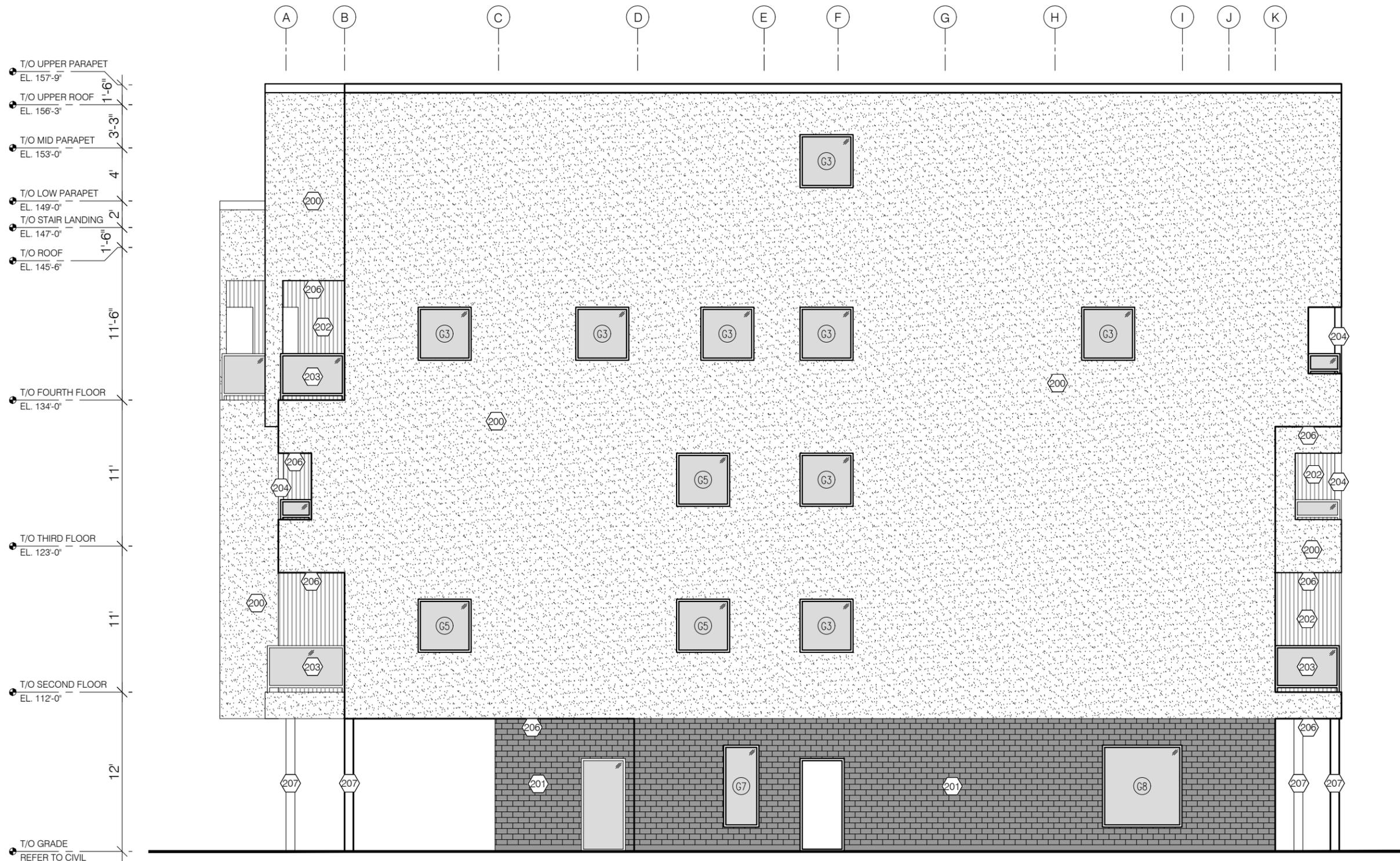
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SHEET NUMBER

Z2.3R

2 architecture



1 NORTH ELEVATION  
Z2.3R 1/8" = 1'-0"

ELEVATION KEYNOTES

- |  |  |
|--|--|
| <ul style="list-style-type: none"> <li>200 ACRYLIC STUCCO SYSTEM - ULTRA SMOOTH (STANDARD COLOUR: WHITE)<br/>G.C. TO PROVIDE SAMPLES FOR ARCHITECTURAL / OWNER APPROVAL BEFORE MATERIALS ARE ORDERED.</li> <li>201 ACRYLIC STUCCO SYSTEM - SMOOTH STENCILLED BRICK PATTERN - MONACO STUCCO (COLOURS: CHARCOAL &amp; GRAPHITE - TBC)<br/>G.C. TO PROVIDE SAMPLES FOR ARCHITECTURAL / OWNER APPROVAL BEFORE MATERIALS ARE ORDERED.</li> <li>202 FAUX WOOD COMPOSITE VERTICAL PLANK CLADDING<br/>CLADDING FINISH: LIGHT WOOD PATTERN (MAPLE/BIRCH/PINE)<br/>G.C. TO PROVIDE SAMPLES FOR ARCHITECTURAL / OWNER APPROVAL BEFORE MATERIALS ARE ORDERED.</li> </ul> | <ul style="list-style-type: none"> <li>203 42" HIGH PRE-ENGINEERED GUARD RAIL - TO BE POWDER COATED BLACK ALUMINUM POST GUARD RAIL SYSTEM WITH GLASS PANELS (OWNER TO CONFIRM)</li> <li>204 SIDING TRIM - BLACK (OWNER TO CONFIRM COLOUR)</li> <li>205 CEMENT PARGING OR PROTECTION BOARD</li> <li>206 SOFFIT ALUMINUM CLADDING TO MATCH FLASHING/TRIM - BLACK (OWNER TO CONFIRM COLOUR)<br/>POT LIGHTS TO BE PROVIDED IN SOFFIT</li> <li>207 COLUMNS FINISHED TO MATCH FLASHING/TRIM - BLACK (OWNER TO CONFIRM COLOUR)</li> </ul> |
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T/O UPPER PARAPET  
 EL. 157'-9"  
 T/O UPPER ROOF  
 EL. 156'-3"  
 T/O MID PARAPET  
 EL. 153'-0"  
 T/O LOW PARAPET  
 EL. 149'-0"  
 T/O STAIR LANDING  
 EL. 147'-0"  
 T/O ROOF  
 EL. 145'-6"  
 T/O FOURTH FLOOR  
 EL. 134'-0"  
 T/O THIRD FLOOR  
 EL. 123'-0"  
 T/O SECOND FLOOR  
 EL. 112'-0"  
 T/O GRADE  
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PROJECT  
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**1 WEST ELEVATION**  
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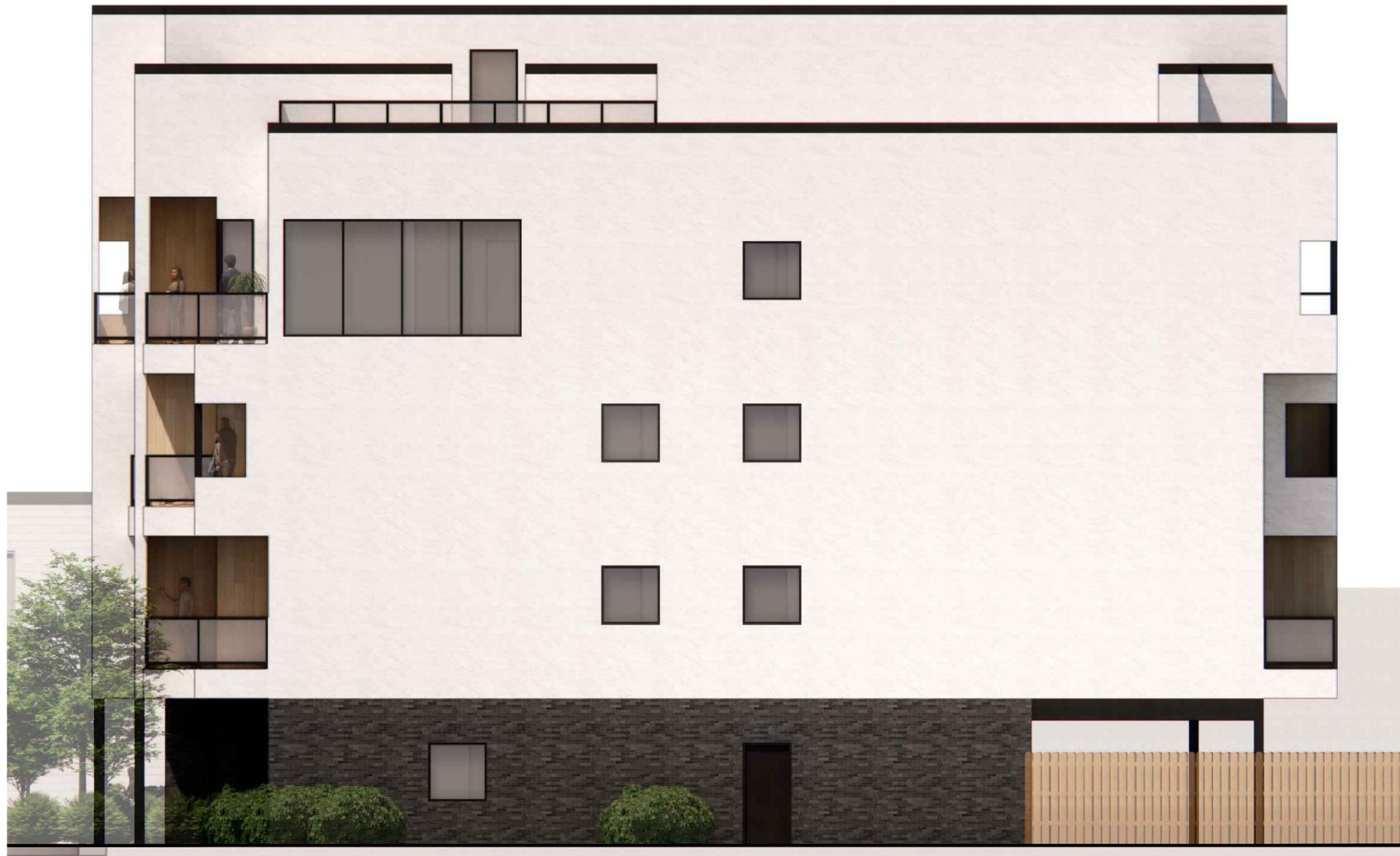
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2 architecture

K J I H G F E D C B A

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"THE DESIGN OF THE BUILDING SHALL INCLUDES AT LEAST TWO PROMINENT BUILDING FACADE MATERIALS. A PROMINENT BUILDING MATERIAL SHALL COVER AT MINIMUM 20% OF THE FRONT FACADE OF THE BUILDING"

"THE SURFACE AREA OF ANY FRONT FACADE FACING A PRIMARY STREET TO WHICH THE BUILDING IS ORIENTED SHALL PROVIDE A MINIMUM 20% AS WINDOW AREA..."

2 architecture



T/O UPPER PARAPET  
EL. 157'-9"

T/O UPPER ROOF  
EL. 156'-3"

T/O MID PARAPET  
EL. 153'-0"

T/O LOW PARAPET  
EL. 149'-0"

T/O STAIR LANDING  
EL. 147'-0"

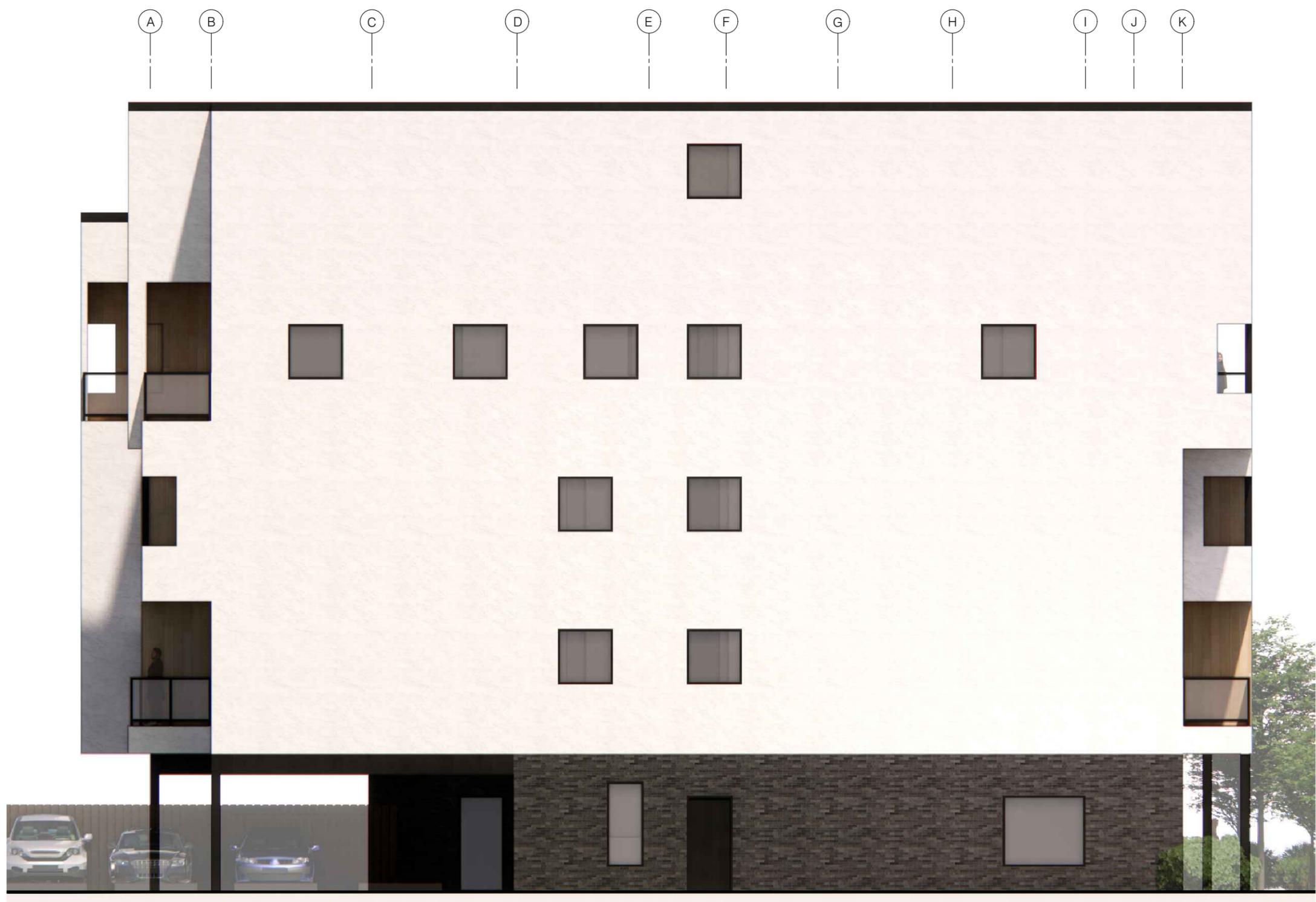
T/O ROOF  
EL. 145'-6"

T/O FOURTH FLOOR  
EL. 134'-0"

T/O THIRD FLOOR  
EL. 123'-0"

T/O SECOND FLOOR  
EL. 112'-0"

T/O GRADE  
REFER TO CIVIL



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ISSUED

- REISSUED FOR ZONING FEBRUARY 18, 2025
- REISSUED FOR ZONING FEBRUARY 5, 2025
- ISSUED FOR ZONING JANUARY 29, 2025
- ISSUED FOR CLASS B COSTING NOVEMBER 25, 2024
- ISSUED FOR ZONING REVIEW OCTOBER 24, 2024
- ISSUED FOR PLANNER REVIEW AUGUST 22, 2024
- ISSUED FOR CLASS C COSTING COORDINATION AUGUST 7, 2024
- CLIENT INITIATED CHANGE JULY 25, 2024
- REISSUED FOR CLIENT AND CONSULTANT REVIEW JULY 11, 2024
- REISSUED FOR CLIENT AND CONSULTANT REVIEW JULY 4, 2024
- ISSUED FOR CLIENT AND CONSULTANT REVIEW JUNE 13, 2024

ADDRESS  
455 ST. MARY'S RD  
WINNIPEG, MB

PROJECT  
MANITOBA INUIT ASSOCIATION  
TRANSITIONAL HOUSING

SHEET TITLE  
NORTH ELEVATION  
COLOURED

SCALE  
AS NOTED

DESIGNER  
CG

DRAWN BY  
JC / DD

DATE  
FEBRUARY 18, 2025

SHEET NUMBER  
**Z2.7R**

**1** NORTH ELEVATION  
Z2.7R 1/8" = 1'-0"

**ELEVATION KEYNOTES**

- 200 ACRYLIC STUCCO SYSTEM - ULTRA SMOOTH (STANDARD COLOUR: WHITE)  
G.C. TO PROVIDE SAMPLES FOR ARCHITECTURAL / OWNER APPROVAL BEFORE MATERIALS ARE ORDERED.
- 201 ACRYLIC STUCCO SYSTEM - SMOOTH STENCILLED BRICK PATTERN - MONACO STUCCO (COLOURS: CHARCOAL & GRAPHITE - TBC)  
G.C. TO PROVIDE SAMPLES FOR ARCHITECTURAL / OWNER APPROVAL BEFORE MATERIALS ARE ORDERED.
- 202 FAUX WOOD COMPOSITE VERTICAL PLANK CLADDING  
CLADDING FINISH: LIGHT WOOD PATTERN (MAPLE/BIRCH/PINE)  
G.C. TO PROVIDE SAMPLES FOR ARCHITECTURAL / OWNER APPROVAL BEFORE MATERIALS ARE ORDERED.
- 203 42" HIGH PRE-ENGINEERED GUARD RAIL - TO BE POWDER COATED BLACK ALUMINUM POST GUARD RAIL SYSTEM WITH GLASS PANELS (OWNER TO CONFIRM)
- 204 SIDING TRIM - BLACK (OWNER TO CONFIRM COLOUR)
- 205 CEMENT PARGING OR PROTECTION BOARD
- 206 SOFFIT ALUMINUM CLADDING TO MATCH FLASHING/TRIM - BLACK (OWNER TO CONFIRM COLOUR)  
POT LIGHTS TO BE PROVIDED IN SOFFIT
- 207 COLUMNS FINISHED TO MATCH FLASHING/TRIM - BLACK (OWNER TO CONFIRM COLOUR)

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2 architecture