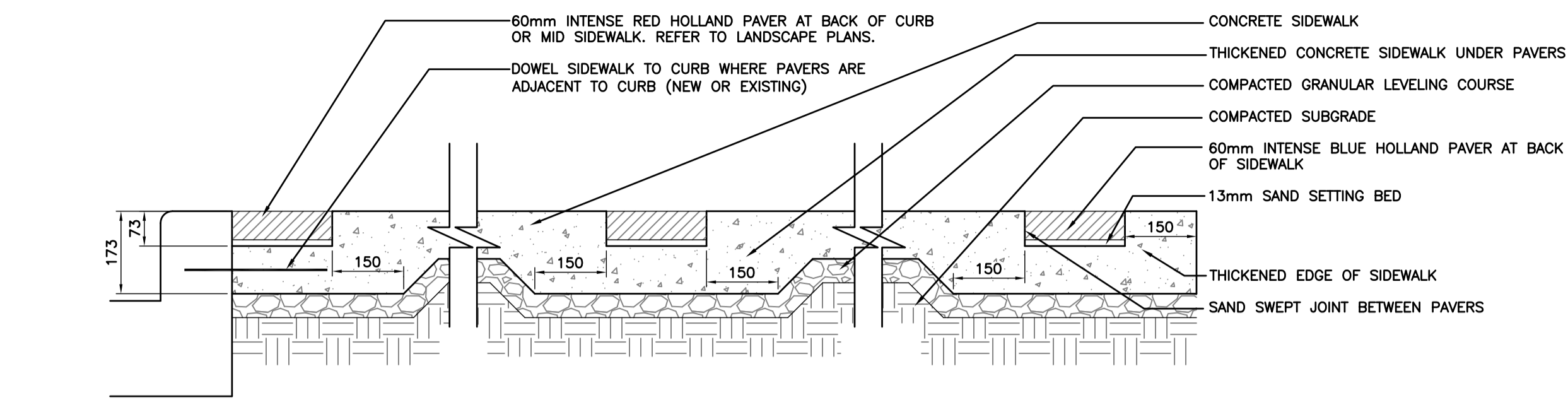
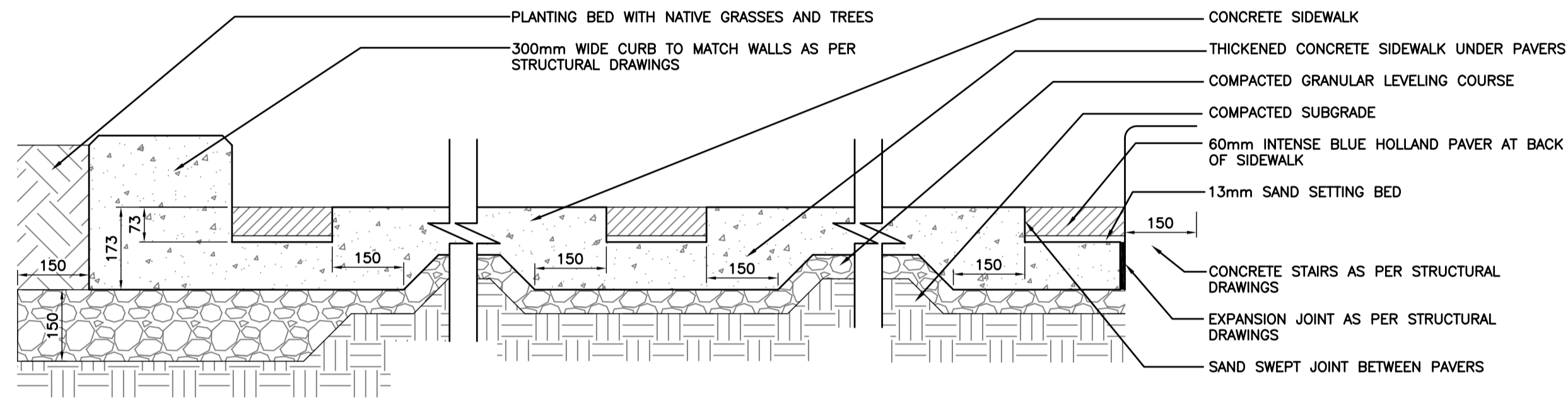


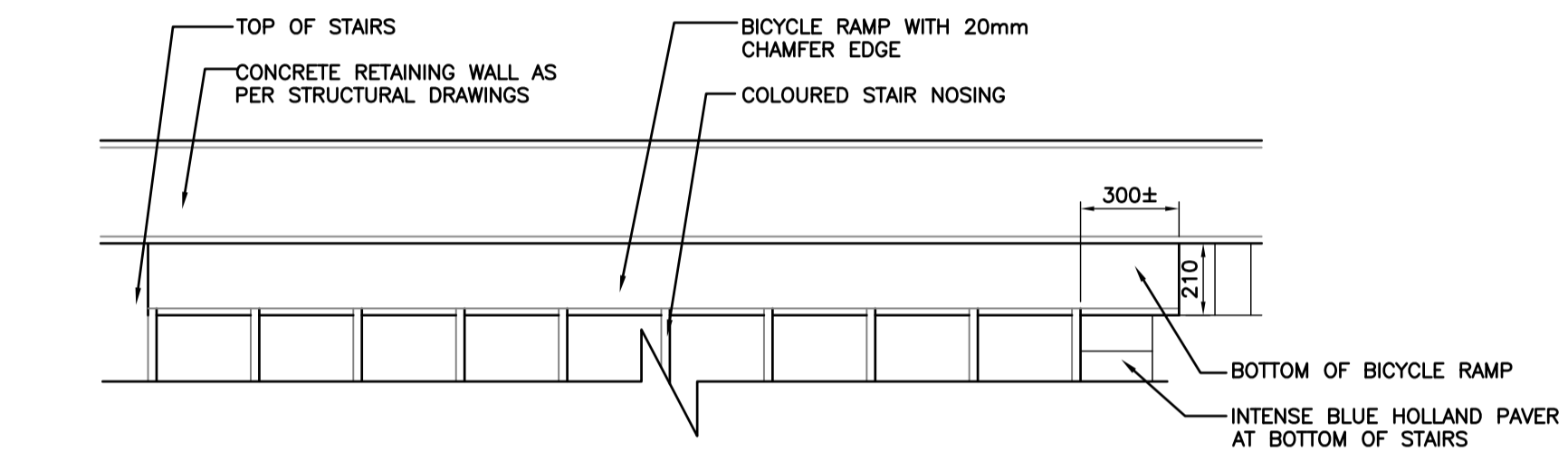
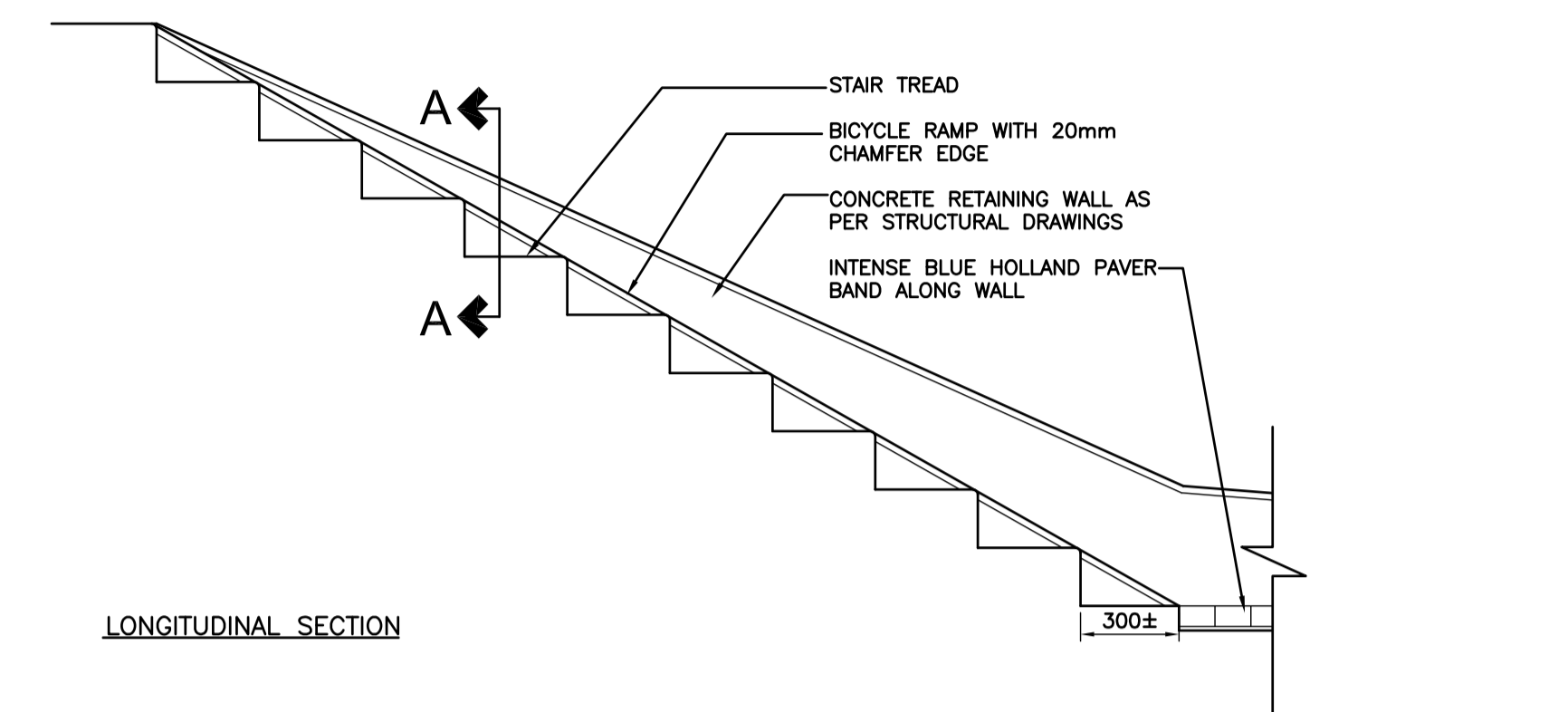
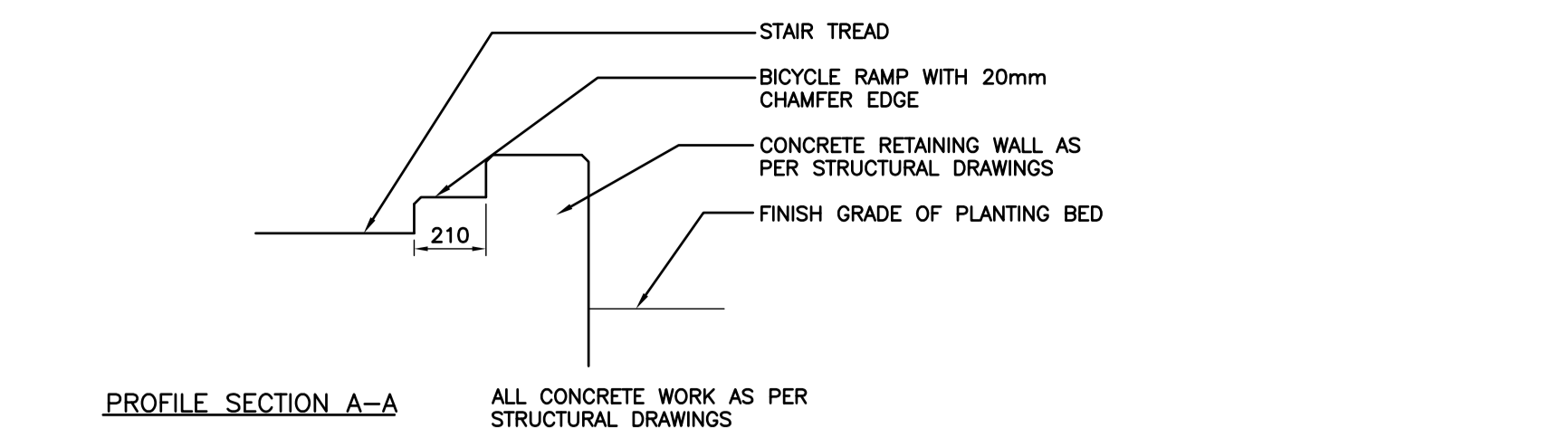
- NOTES**
- 1) SIDEWALK RAMP SURFACE SHALL BE AS INDICATED ON PLAN DRAWINGS
  - 2) 15M TIE BARS SHALL BE PLACED PRIOR TO THE PLACING OF ANY CONCRETE SIDEWALK. TIE BARS SHALL BE BENT AS REQUIRED AND PLACED SO AS TO ENSURE A MINIMUM CONCRETE COVER OF 40MM
  - 3) 15M TIE BARS SHALL BE DEFORMED AND CONFORM TO CSA STANDARD G30.12 GRADE 300, UNLESS THE BARS ARE TO BE BENT AND LATER STRAIGHTENED IN THE FIELD, IN WHICH CASE THEY SHALL CONFORM TO ASTM A307 GRADE A BOLT STEEL (227.53 MPa YIELD STRESS)
  - 4) MAXIMUM SLOPE OF FLARED SIDES SHALL BE 1:10 REFER TO CSA B651, ARTICLE 4.4.4.2



TYPICAL UNIT PAVER INSERT IN CITY SIDEWALK ②

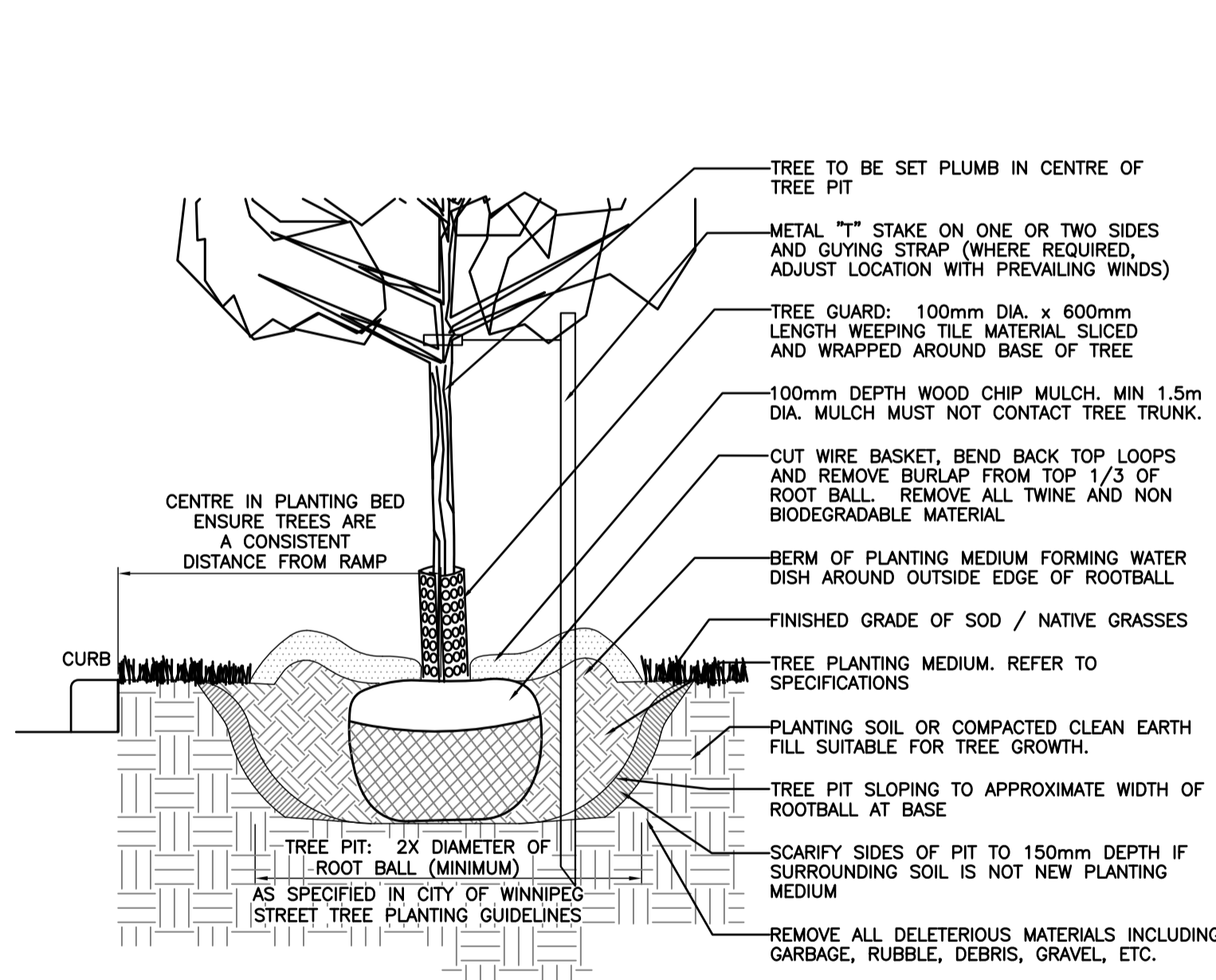


TYPICAL UNIT PAVER INSERT IN LOWER PLAZA ③

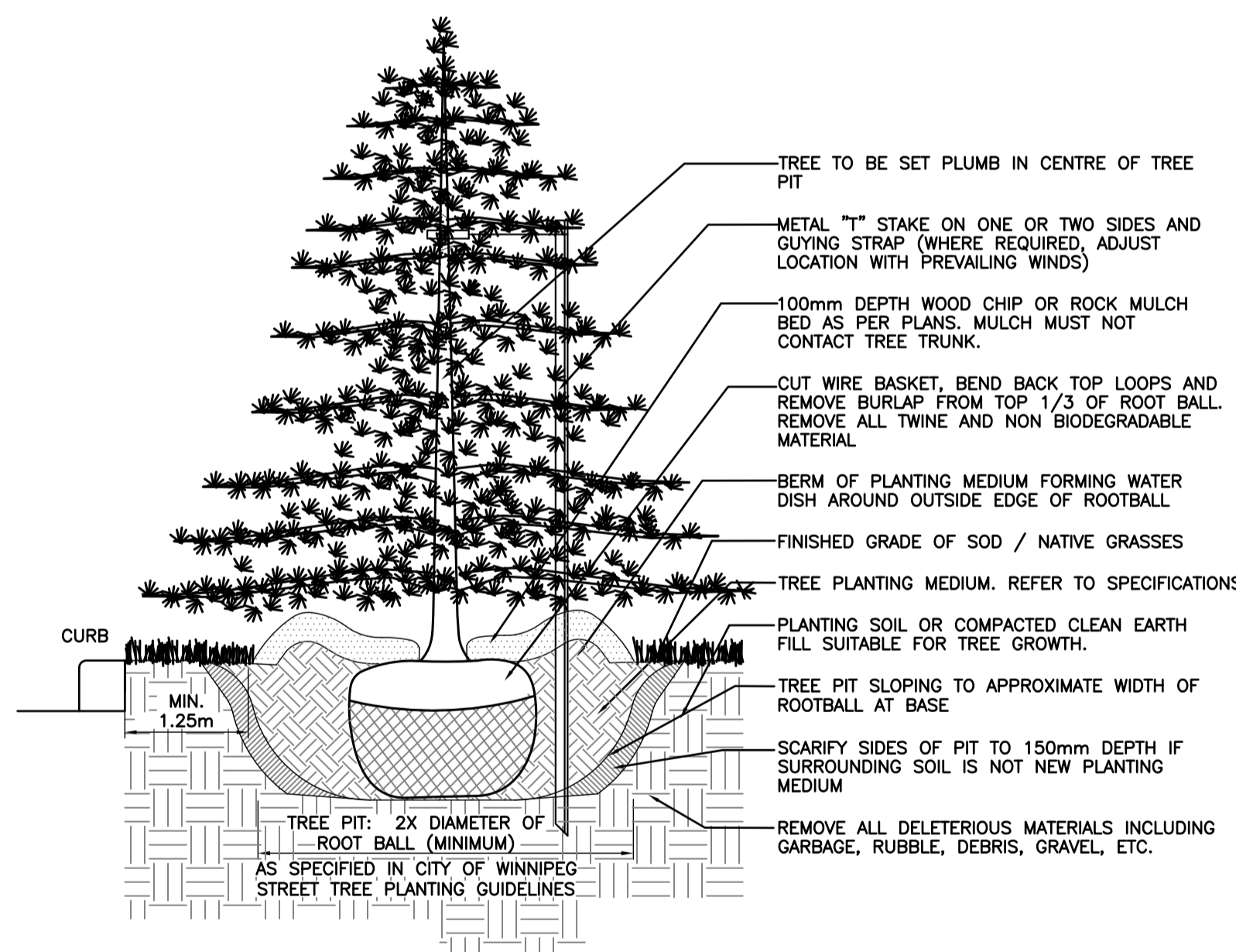


BICYCLE WHEEL RAMP ④

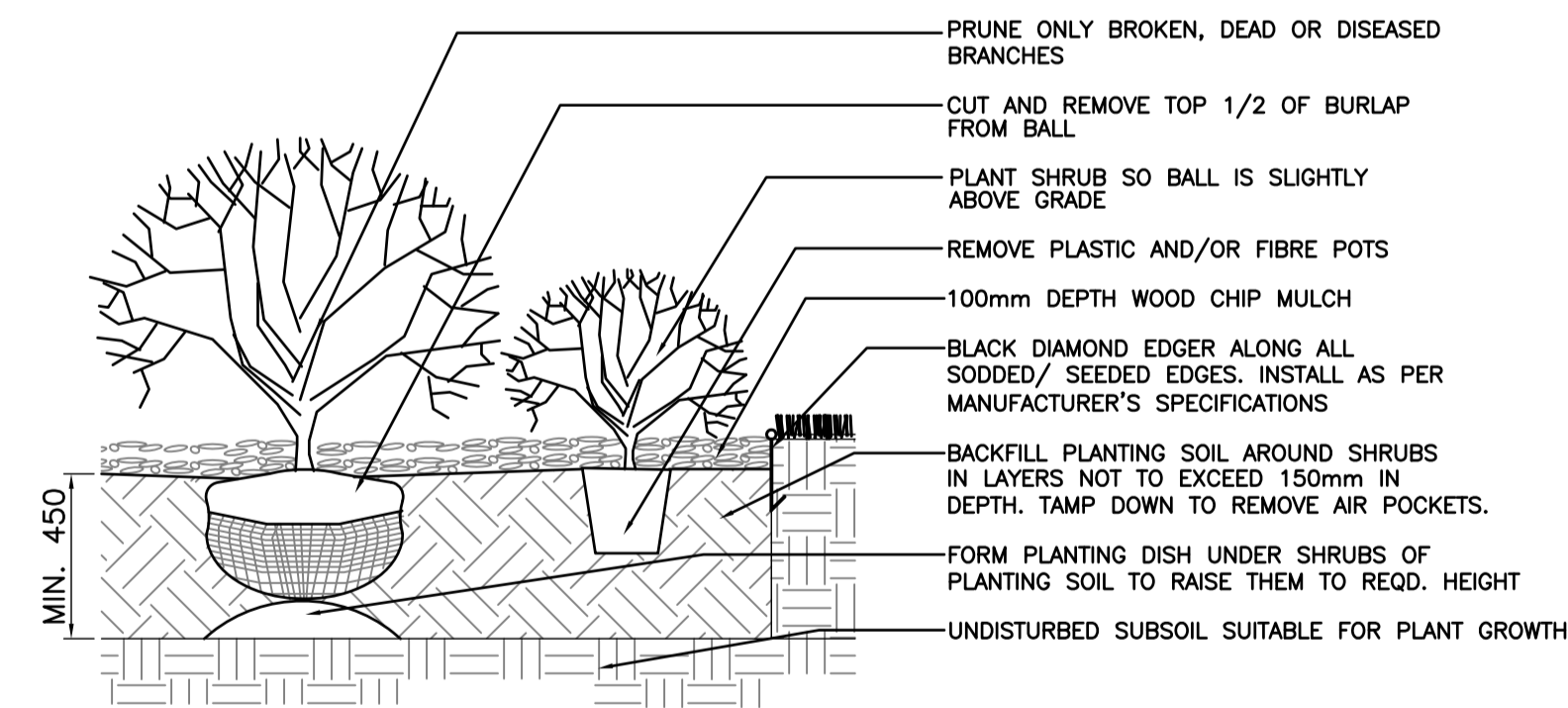
TYPICAL DETECTABLE PAVING IN SIDEWALK RAMP ①



TYPICAL DECIDUOUS TREE PLANTING ⑤



TYPICAL CONIFEROUS TREE PLANTING ⑥



TYPICAL SHRUB BED PLANTING / MULCH BED ⑦

X:\08-028 Southwestern BRT Corridor\dwg\2010\osborne station\wkg\dwg\DETAIL SHEET 2010-04-22.dwg

<b>LOCATION UNDERGROUND STRUCTURES</b> ORIGINAL SIGNED BY: X SUPV. U/G STRUCTURES COMMITTEE: X DATE: _____	<b>APPROVED STRUCTURES</b> ORIGINAL SIGNED BY: X DATE: _____	B.M. ELEV. _____ B.M. ELEV. _____	DESIGNED BY: SR/JW DRAWN BY: JW/OV CHECKED BY: JW/SR APPROVED BY: _____ HOR. SCALE: AS NOTED VERTICAL: _____ DATE: 2010-05-07	  RELEASED FOR CONSTRUCTION ORIGINAL SIGNED BY: RANDY FINGAS DATE: 2010/05/14	LANDSCAPE ARCHITECT'S SEAL  ORIGINAL SIGNED BY: JACKIE WILKIE JACQUELINE L. WILKIE member no. 75	<b>THE CITY OF WINNIPEG</b> <b>TRANSIT DEPARTMENT</b> SOUTHWEST RAPID TRANSIT CORRIDOR - STAGE 1 OSBORNE STATION & ASSOCIATED WORKS LANDSCAPE DETAILS	CITY DRAWING NUMBER: B237-10-118 SHEET OF: 118 OF 121 CONSULTANT DRAWING NUMBER: C5-L-5007-T
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**NOTE:**  
 LOCATION OF UNDERGROUND STRUCTURES AS SHOWN ARE BASED ON THE BEST INFORMATION AVAILABLE, BUT NO GUARANTEE IS GIVEN THAT ALL EXISTING UTILITIES ARE SHOWN OR THAT THE GIVEN LOCATIONS ARE EXACT. CONFIRMATION OF EXISTENCE AND EXACT LOCATION OF ALL SERVICES MUST BE OBTAINED FROM THE INDIVIDUAL UTILITIES BEFORE PROCEEDING WITH CONSTRUCTION.

NO.	REVISIONS	DATE	BY
1	ISSUED FOR TENDER	10/05/14	JW