LEGEND

PROPOSED CENTRELINE TRACK	
PROPOSED POINT OF SWITCH	0
PROPOSED TURNOUT UNIT	
EXISTING CENTRELINE TRACK	
EXISTING CULVERT	C=========
EXISTING OVERHEAD POWERLINE (APPROXIMATE LOCATION SHOWN)	P
EXISTING MANHOLE	Θ
EXISTING ELECTRICAL BOX	\boxtimes
EXISTING GROUND	
EXISTING AQUEDUCT	= $=$ $=$ $=$ $=$
PROPOSED FOULING POINT	I
EXISTING MILE POST	(M. 19)
GWWD ROW	
PROPOSED PROPERTY ACQUISITION	
PROPOSED CENTRELINE DITCH	
GEOTEXTILE SEPARATION FABRIC	
GRADING LIMIT	
500mm STRIPPING	· · ·
TEMPORARY EASEMENT AREA FOR ACCESS ROAD CONSTRUCTION	
EXISTING CONTOURS AND LABELS DISPLAYED AT 0.5m INTERVAL	90

		110
CT-0		
CT-C		
CT-1		
CT-2		
CT-3		
CT-4		

GENERAL NOTES

- 1. ALL DIMENSIONS ARE IN METERS UNLESS NOTED OTHERWISE.
- 2. SURVEY CONTROL IS BASED ON GEODETIC DATUM NAD83 (CSRS)- UTM ZONE 14U.
- 3. CONTRACTOR IS RESPONSIBLE FOR LOCATING AND PROTECTING ALL BURIED UTILITIES WHETHER IDENTIFIED OR NOT.
- 4. ALL EXISTING FENCES AFFECTED BY THE CONSTRUCTION ARE TO BE REBUILT BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER IN THE FIELD.
- 5. SOME LOCAL DITCH IMPROVEMENT MAY BE REQUIRED TO ENSURE DRAINAGE IS MAINTAINED AS DIRECTED BY THE ENGINEER IN FIELD.
- 6. ALL CONSTRUCTION EQUIPMENT OR MATERIAL SHALL BE KEPT TO THE NORTH SIDE OF THE MAIN LINE. NO WORK SHALL BE DONE SOUTH OF THE TRACKS IN PROXIMITY TO THE AQUEDUCT.
- STANDARDS SET FORTH IN AREMA MANUAL FOR RAILWAY ENGINEERING (2022).
- 8. UNLESS DIRECTED OTHERWISE, CONTRACTOR IS REQUIRED TO NOTIFY GWWD DISPATCH UPON ARRIVAL AND DEPARTURE FROM SITE BY PHONING 204-986-4175 OR 204-330-3397.
- FINALIZED. THE CONTRACTOR SHALL RESTORE ALL DAMAGED PROPERTY AND AGRICULTURAL LAND TO ITS ORIGINAL CONDITION WITHIN THE TEMPORARY EASEMENT SHOWN ON DWG. CT-101.

TRACK DESIGN NOTES

- 1. PROPOSED SIDING TRACK DESIGN SPEED = 10 MPH.
- 2. MAXIMUM DEGREE OF CURVE = 7° 30' 00"
- 3. RAIL CAR LENGTH = 60' (18.29m)
- 4. SIDING TRACK IS NON-SUPERELEVATED.
- 5. 14 ft. TRACK CENTRE AT THE FOULING POINT.

LIST OF PROJECT DRAWINGS

COVER SHEET **INDEX PAGE** GENERAL LAYOUT PROPOSED SIDING TRACK PLAN & PROFILE **TYPICAL SECTIONS & DETAILS** CROSS SECTIONS - STA. 0+000 TO STA. 0+140 CT-402 CROSS SECTIONS - STA. 0+150 TO STA. 0+261

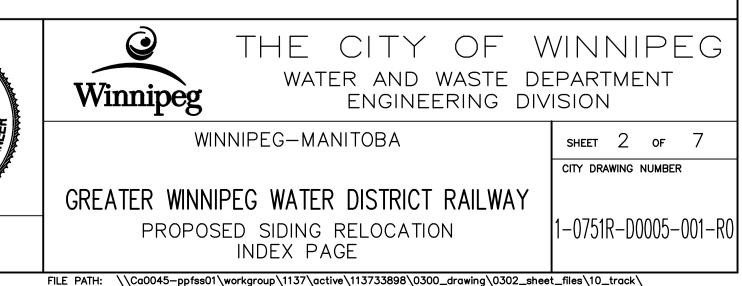
7. UNLESS NOTED OTHERWISE IN THE CONTRACT DOCUMENTS, ALL MATERIAL AND CONSTRUCTION SHALL CONFORM TO CN ENGINEERING SPECIFICATIONS FOR INDUSTRIAL TRACKS (NOVEMBER 2019) AND

9. ACCESS TO THE SITE IS PROVIDED THROUGH TEMPORARY CONSTRUCTION EASEMENTS ON PRIVATELY OWNED LAND, AND THROUGH LAND PURCHASE AGREEMENTS THAT HAVE NOT YET BEEN

B.M. ELEV.					Antec tage Avenue, Winnipeg MB Canada c.com	ENGINEER'S SEAL
				DESIGNED MY	CHECKED MY	TRANQUADA Member 35300
				DRAWN BY GR	APPROVED BY ERT	THE STOFESSION
				SCALE: HORIZONTAL N.T.S	RELEASED FOR CONSTRUCTION	CONSULTANT DRAWING NUMB
0	ISSUED FOR TENDER	23/05/05	M.Y.	VERTICAL		
NO.	REVISIONS	DATE	BY	DATE 2023 05 05	DATE	- CT-001
		·		PLOT DATE: 2023 05 05		BID OPPORTUNITY: 57-2023 CONTRACT NUMBER: X

ABBREVIATIONS

BVCE	BEGIN VERTICAL CURVE ELEVATION
BVCS	BEGIN VERTICAL CURVE STATION
CL	CENTERLINE
C.O.W.	CITY OF WINNIPEG
СТ	CURVE TO TANGENT
Dc	DEGREE OF CURVE
DS	DOUBLE SHOULDERED
ELEV	ELEVATION
EVCE	END VERTICAL CURVE ELEVATION
EVCS	END VERTICAL CURVE STATION
EX.	EXISTING
GRD.	GROUND
LH T.O.	LEFT HAND TURNOUT
LVC	LENGTH OF VERTICAL CURVE
L.S.T	LAST LONG TIE
Mi.	MILEAGE
ML	MAINLINE
N.T.S.	NOT TO SCALE
PI	POINT OF INTERSECTION
PRPSD.	PROPOSED
P.S.	POINT OF SWITCH
PVI	POINT OF VERTICAL INTERSECTION
RH T.O.	RIGHT HAND TURNOUT
RBM	RAIL BOUND MANGANESE
STA.	STATION
SUB.	SUBDIVISION
тс	TANGENT TO CURVE
Т.О.	TURNOUT
TOR	TOP OF RAIL
TYP.	TYPICAL



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