

Broadway Streetscape Vision

Submitted to
CITY OF WINNIPEG
510 Main Street
Winnipeg, MB R3B 1B9

Submitted by
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Figure 1. Manitoba Legislature looking north. Photo: by Mahesh Gupta via Unsplash.

Winnipeg is located in Treaty One Territory, the home and traditional lands of the Anishinaabe (Ojibwe), Ininew (Cree), and Dakota peoples, and in the National Homeland of the Red River Métis. Our drinking water comes from Shoal Lake 40 First Nation, in Treaty Three Territory.

We respect the Treaties that were made on these territories, we acknowledge the harms of the past and of the present, and we dedicate ourselves to moving forward in partnership with Indigenous communities in a spirit of reconciliation and collaboration.

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Figure 2. Broadway in 1920. Photo: City of Winnipeg Archives.

1.0 Introduction

1.1 Project Scope and Goals

This document presents a Vision for revitalizing Broadway's streetscape, building on the Broadway Tree Planting Plan completed for the City of Winnipeg in 2022, the concurrent street renewal project led by AECOM, and the Downtown Winnipeg BIZ's site furniture plan that is currently being implemented. It identifies opportunities for upgrades to paving, planting, and other elements in keeping with the framework and streams identified in the Downtown Recovery Strategy, with a focus on the median as the area with the greatest need, and the most potential for transformative enhancements.

This Vision provides guidance on how all streetscape elements on the median and boulevards, policies, and programs can work together to reinforce the character and significance of one of Winnipeg's most iconic streets. Those elements include tree planting, site furnishings, planters, paving, accessibility features, supplementary lighting, and interpretive and public art installations. It will also consider how programming such as street festivals and food trucks, Cool Gardens installations, historic tours and more can support and be supported by enhancement initiatives. Implementation steps and budget pricing are also provided for all key recommendations.

The goal of the Vision is to provide a framework for future capital planning and decision-making to improve the coherence and impact of investments made along Broadway, re-imagining this visionary 19th Century corridor through a 21st Century lens.

1.2 Concurrent Projects

Broadway Future Tree Strategy

The Broadway Future Tree Strategy was completed in 2022 and describes a program of tree replacement with enhanced planting conditions for the iconic elm allée. It was prepared in collaboration with the City Forestry Branch and provides block-by-block planting plans and notes for installation that correspond with the updated City tree planting standards, which were developed in parallel with the plan.

Broadway Site Furnishings

Between April and July 2022, HTFC worked with the Downtown Winnipeg BIZ to select a new coordinated suite of site furnishings for Broadway to replace the existing benches, waste bins and bike racks which were at the end of their service life. Nominated furnishings were evaluated against performance specifications specific to each furnishing type, related to durability, accessibility, aesthetics, operation, ergonomics, sustainability, cost, future supply reliability, and ease of maintenance and repair. This work was fast-tracked to suit the funding timeframe, so it was completed ahead of the Vision. Installations were completed in May, 2023, and will be adjusted as required to accommodate AECOM's second phase of sidewalk renewals in 2024.

COVID-19 Economic Response and Recovery Plan (The Plan)

On November 25, 2021, Council unanimously approved the COVID-19 Economic Response and Recovery Plan (The Plan).

The Plan supports the Downtown Recovery Strategy (DRS) which is a three-year strategy developed by downtown stakeholder organizations and City staff with input from community members to support the recovery of the downtown from the COVID-19 pandemic.

The intent is to invest in places and spaces to attract and sustain more people downtown. Ten million dollars of gas tax funds was dedicated to ten (10) downtown projects, including \$600,000 for Broadway. Collectively the proposed investments in the plan are designed to achieve the following:

- Improve accessibility and connectivity between districts.
- Increase tourism and local visitation downtown.
- Achieve greater social equity in the downtown.
- Enhance existing commercial clusters.
- Create and sustain downtown businesses.
- Grow the downtown residential population.
- Improve perceptions of downtown as clean and cared for.
- Increase the integration of public art downtown.
- Ensure Indigenous considerations and stories are prominent downtown.
- Broadway Road Renewal (City of Winnipeg)

2019-2024 Downtown Streets Renewal Project

Through this project, which is led by AECOM and supported by the Government of Canada New Building Canada Fund and the Province of Manitoba, Broadway will see major rehabilitation work done on numerous streets to improve road surface conditions. New concrete sidewalks will be installed along with new trees and soils cells. The project will not have permanent implications for parking, loading, intersection geometries or routing. Installation of new site furnishings has been coordinated with the sidewalk renewal phasing.

Tribute to Shoal Lake 40 First Nation

During the summer of 2023 the City of Winnipeg and Downtown Winnipeg BIZ unveiled a tribute to the community of Shoal Lake 40 First Nation during a ceremony at the newly refurbished Broadway Centennial Fountain, located on the boulevard along Broadway at Donald Street. The tribute features a large copper plaque, inscribed with a message of gratitude and support for the community of Shoal Lake 40 First Nation. The newly installed tribute to Shoal Lake 40 First Nation and water fountain refurbishment is part of the Broadway Revitalization Project. The Broadway Revitalization Project is largely funded through the Canada Community Revitalization Fund, awarded by the Government of Canada to Downtown Winnipeg BIZ.

2.0 Brief History of Broadway

Broadway has been one of Winnipeg's most prominent streets since settlement in the 19th Century. Running west-east between Osborne Street and Main Street it is bookended by the architectural landmarks of the Manitoba Legislative Building and Union Station.

However, its most iconic feature is arguably the wide, central median. Winnipeg's Park Board began establishing boulevards and parks at the turn of the 20th Century, which included an allée of elms down Broadway's median and boulevards, flanking the streetcar line. This strong, green, central axis was reinforced by the Legislature Building opened in 1920 and Union Station in 1911, complementing the formal and luxurious residential district.

The early 20th Century saw Winnipeg's wealthy begin to move away from the downtown which led to an increase in commercial and multi-family investment. The street became a porous edge between the commercial downtown and residential neighbourhoods south of the Assiniboine River. A push was made in the mid-20th Century to make Broadway the "Wall Street of the West" and many institutions and corporations established head offices on the street.

Still a busy public space pre-COVID-19, the street has become less frequented post-pandemic and public safety, turnover of businesses, and lack of foot traffic are some of the main concerns held by business owners and other interested parties. Additionally, the once healthy elms are now ailing – providing an opportunity to replant and reinvigorate the median as the beating heart of Broadway.

Broadway played a role in reconciliation efforts between the Francophone community centred in Saint Boniface and the rest of Winnipeg. During the city's early years, Broadway connected directly with Provencher Boulevard via the Saint-Boniface-Broadway bridge, until 1908 when business, political, and cultural rivalries severed the tie and Union Station was constructed as a terminus to the boulevard (see Figures 3 and 4). The construction of Esplanade Riel in 2003 re-establishes that link and alignment and celebrates the collective strength of our diverse heritages.

Before it was Broadway, this area was a hub of trade for Indigenous peoples for thousands of years. In the late 18th Century, European movement into the region brought smallpox which devastated the approx. 5000-member Indigenous community who lived at the intersection of the Assiniboine and Red Rivers. Archaeologists have found evidence of mass graves throughout downtown Winnipeg, particularly near Upper Fort Garry, The Forks, and Main and Broadway (see Appendix 4). In 2018, the Assembly of Manitoba Chiefs suggested memorial statues, plaques, and other public place signifiers as a way to honour the lives lost and educate the public. Future public art installations within the Broadway corridor in the vicinity of known burial sites should consider this in the brief to artists

Broadway has a rich and varied history which can be exemplified through thoughtful design and intentional enhancements.



Figure 3. 1881 map of Winnipeg showing Broadway and Provencher still linked by bridge. Photo: Manitoba Historical Society.

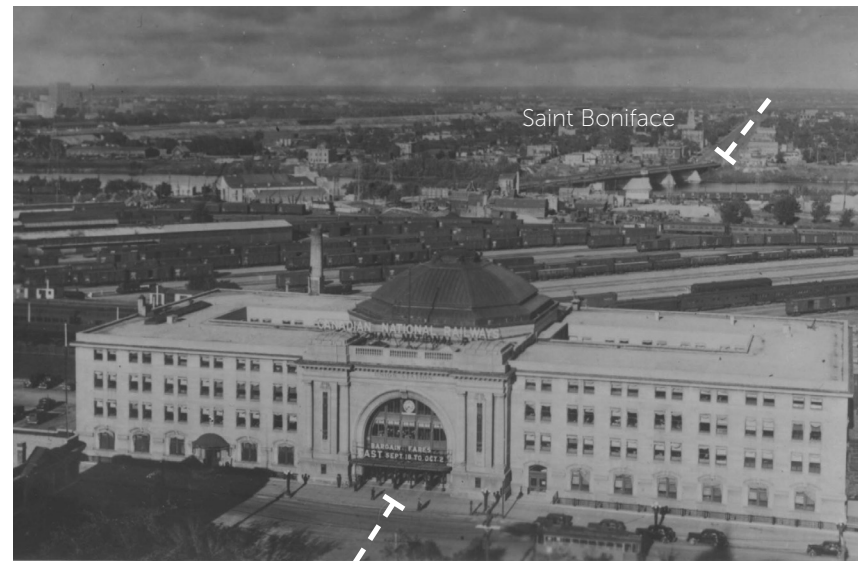


Figure 4. Union Station in 1940 (looking east to Saint Boniface). Photo: City of Winnipeg Archives.



Figure 5. Broadway's elm-lined boulevards in 1910. Photo: Winnipeg Free Press.



Figure 6. Manitoba Legislature Building in 1940 (with Broadway in the foreground). Photo: City of Winnipeg Archives.

3.0 What We Heard

3.1 Background

An engagement process was devised to help build the inventory and assessment of the current condition of the streetscape on Broadway and to identify future opportunities for enhancements, programming, and site furnishings. Working with the City of Winnipeg, the Downtown Winnipeg Biz, AECOM, and Blueprint, the stakeholders to participate in the engagement process were identified. These participants consisted of businesses, residents, and property owners in the area that are representative of Broadway's public. Due to the limited scope for the Downtown Road Renewal work for Broadway, it did not include any public engagement activities. Therefore, the focus group sessions were an opportunity to provide the stakeholders with an update regarding the City's Downtown Road Renewal work scheduled for Broadway.

A total of two (2) focus group sessions were held to build a common understanding of what streetscape elements work together to define Broadway and collect feedback on the priorities for locations and types of streetscape improvements. The goal of these sessions was to work directly with the stakeholders to ensure their concerns and aspirations were understood and considered. Each focus group session was 90 minutes in length and consisted of a virtual presentation with activities. The activities promoted joint problem-solving and creative thinking regarding the type of streetscape improvements that should be included such as site furnishings, public art, wayfinding, and landscaping, to help to inform the overall vision for Broadway. For those that could not participate in a focus group session, follow-up phone interviews were conducted to collect feedback.

Additional working meetings were scheduled with representatives from City departments, including Planning, Parks, Downtown Winnipeg BIZ, Forestry, Active Transportation, Streets Maintenance, Heritage, and the Winnipeg Arts Council. During these meetings, issues and opportunities were identified around topics of green infrastructure, pedestrian access to medians, coordination with road renewals, and plans for public art and events.

3.2 Business Owner's Views

Words used to describe Broadway:

Elegant, trees, iconic, promenade, historic, gateway, empty

Opportunities:

Existing iconic architecture and streetscape (wide boulevards and medians) should be retained and built upon to create connections to other Winnipeg landmarks like the Forks, art galleries, and True North Square.

Concerns:

It is currently car-centric, creating a perceived safety issue for pedestrians and cyclists.

Preferences:

Emphasize Broadway as a gateway destination with improved/increased:

- Trees
- Lighting
- Public art as wayfinding
- Waste receptacles
- Public washroom

3.3 City of Winnipeg's Views

Similar sentiments were expressed and specific insight was received regarding maintenance. Snow clearing around planters and site furnishings, and median tree health were the main concerns expressed in relation to increasing seating nodes and public art in the median.

3.4 Summary

Throughout the engagement meetings, food trucks and median programming came up as both a concern and a preference. Food trucks are desired to increase liveliness on the street but are a concern for restaurateurs. Median programming is also thought to bring more visitors to the area however there is a concern for street trees health and human safety regarding traffic.

The primary preferences across the public engagement sessions are:

- Increased trees and park-like green space
- Welcoming lighting
- Maintain iconic wide boulevards, medians, and historic building façades
- Activation of the medians through seating and art
- Revive Broadway's identity as both a destination and a gateway to the cultural downtown core

Specific items to note:

- Areas needing most attention: Memorial to Donald.
- AECOM narrowing crossings and bumpouts where possible on adjoining streets (not along Broadway), adding heated bus shelters.
- Rapid Transit design 2025, implementation 2027, will heavily impact Main + Broadway intersection.
- No new median plantings to occur until after roadworks complete, 2025
- Call for artist submissions Winter 2023/24 for installation Spring 2025



Figure 7. Broadway's median seating nodes being enjoyed in 1970. Photo: City of Winnipeg Archives.

4.0 Analysis

Site analysis of Broadway included a walking tour taking photographs of existing conditions and listing existing fixtures and features, as well as completing wind and shade analysis using 3D modelling software. The results are demonstrated in the various graphics that follow. The analysis findings, in conjunction with the engagement sessions, have informed the Vision and next steps for Broadway.

4.1 Wind + Shade

As with any urban environment, wind and shade are heavily influenced by the surrounding buildings. Broadway's wide boulevards and generous building setbacks offer some places of refuge that can be capitalized on with strategic planning. Shade studies for the summer solstice and spring equinox (Figures 8 + 9) highlight areas that workers downtown are more likely to take their lunch. Once wind patterns are overlaid, see Figure 11, warm areas in spring are highlighted that are both free of shade and protected from the prevailing winds. The two areas specified on Figure 11 (red dots) are already being used: the westerly spot is a raised seating area and the easterly spot was a Downtown Winnipeg BIZ pop-up office space last year. Taking existing site conditions into consideration mean that its worthwhile investing in seating nodes and waste receptacles in these areas as they are more likely to be used, encouraging a lively streetscape.

4.2 Existing Site Conditions & Programming

Visual analysis confirmed much of the information received during the engagement sessions: that median tree health was poor, concrete sidewalks would be better moving forward than extensive pavers, and that more bike parking and bench seating was needed.

Many of the engagement session attendees said that Broadway is less busy and that they wanted to see more people return to the area. Identifying under-utilized parking lots as potential streetlife generators either for temporary pop-ups (food, co-working, parties) or as permanent interventions (installation art or memorials) expands the usable street space and encourages people into the area.

Analysis mapping shows that Broadway is indeed a strong link between many landmarks and downtown destinations such as the Forks and the Winnipeg Art Gallery, with pedestrian friendly access routes to both the Assiniboine and Red Rivers. Figure 12 situates Broadway as a nexus in Central Winnipeg leading to the busy downtown and quieter residential areas, that has strong streetlife potential.



Figure 8. Summer solstice (December 22) shade study at 12 pm. Blue indicates shady spots, opportunities to encourage outdoor lunches on boulevard.



Figure 9. Spring equinox (March 20) shade study at 12 pm. Orange indicates sunny spots, opportunities to encourage outdoor lunches on boulevards.

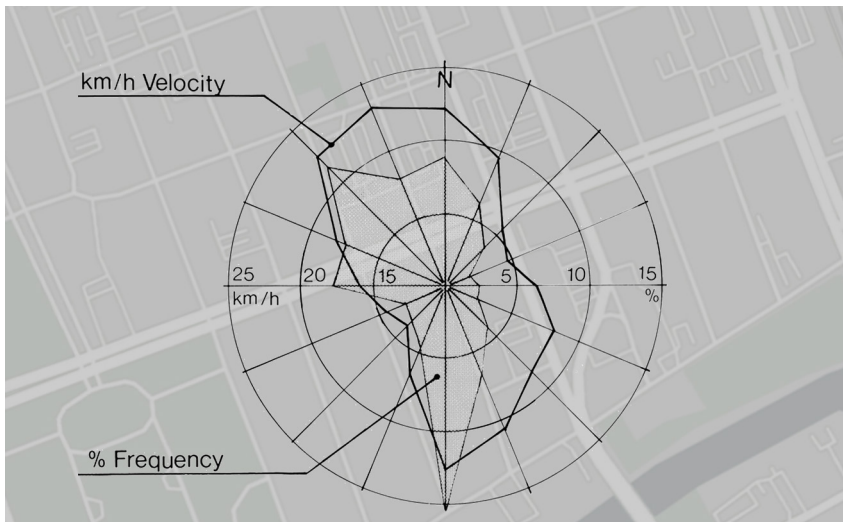


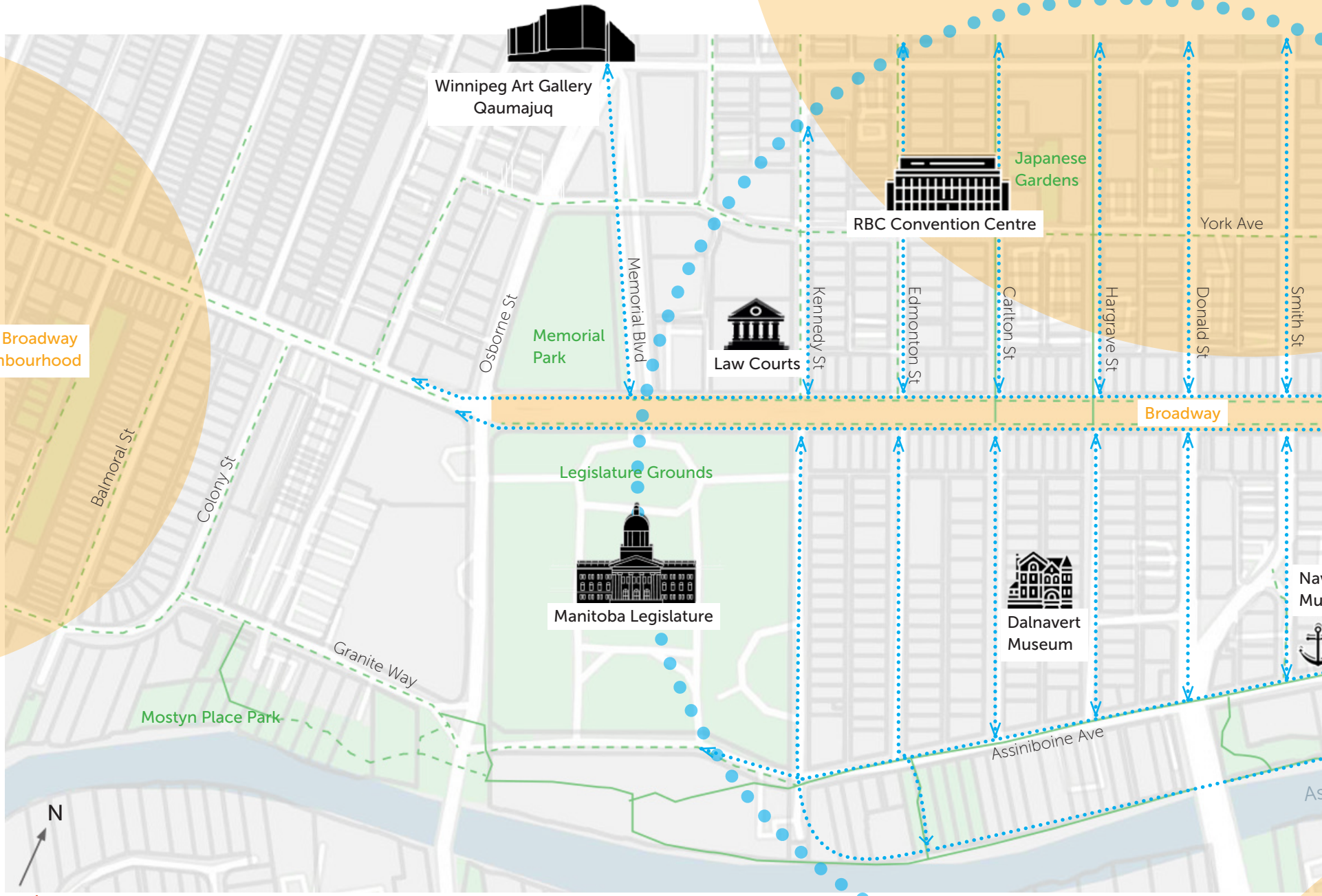
Figure 10. Winnipeg winter (October to April) windrose indicating wind direction, velocity, and frequency. The winds are predominately from the south and north west.



Figure 11. Wind movement with spring equinox shadow overlay. Blue lines indicate predictable wind paths, red circles indicate protected areas.

West Broadway
Neighbourhood

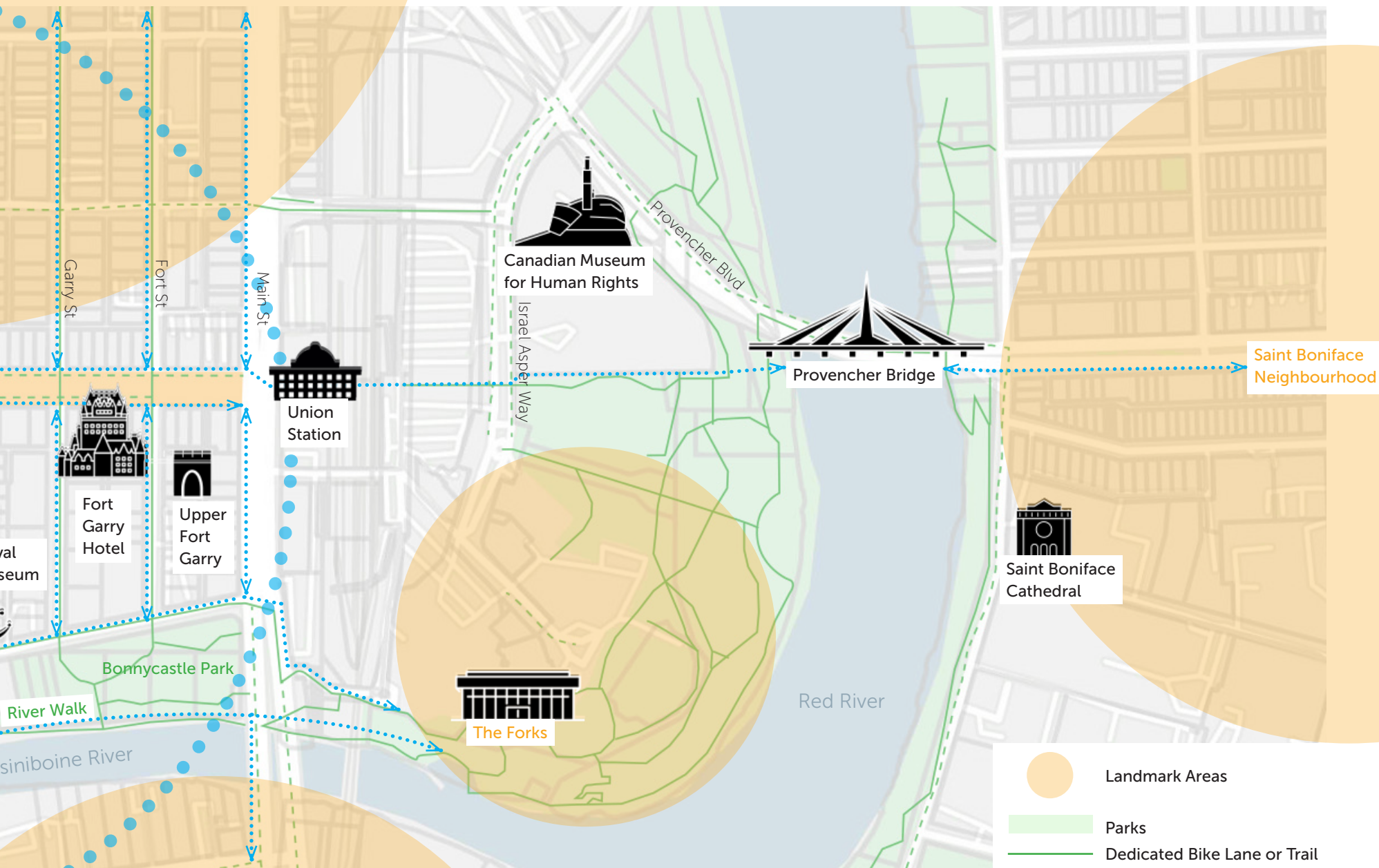
Sports, Hospitality,
Entertainment District



15 min walk

Figure 12. Context map highlighting landmark buildings, landmark areas, parks, connectivity networks and walkability.

and
(SHED)



Saint Boniface
Neighbourhood

Provencher Bridge

Canadian Museum
for Human Rights

Union
Station

Fort
Garry
Hotel

Upper
Fort
Garry

Saint Boniface
Cathedral

The Forks

Red River

- Landmark Areas
- Parks
- Dedicated Bike Lane or Trail
- Bike-Friendly Lane
- Pedestrian Connections

Osborne Neighbourhood

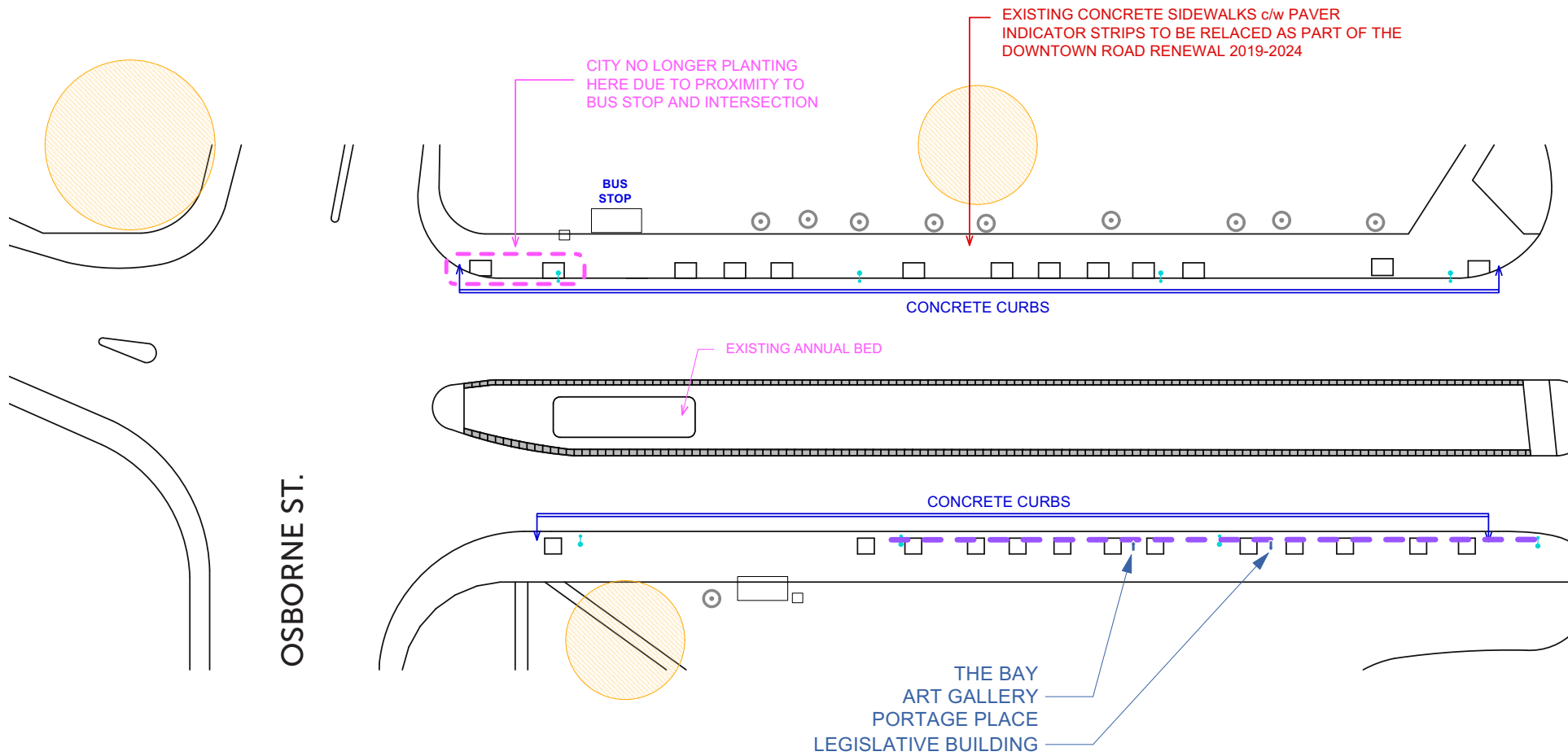
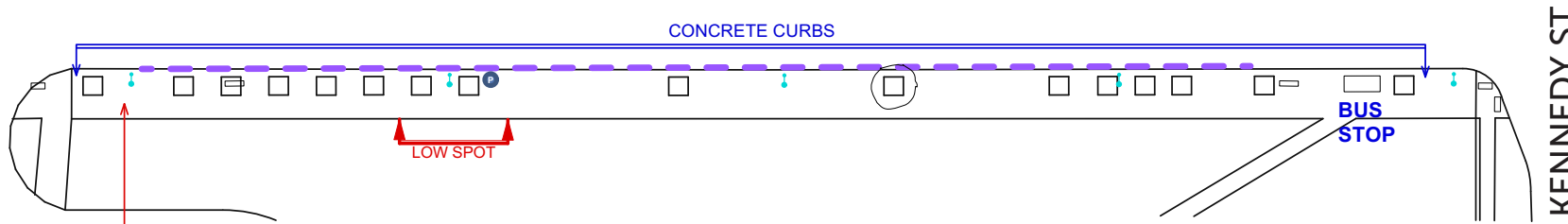
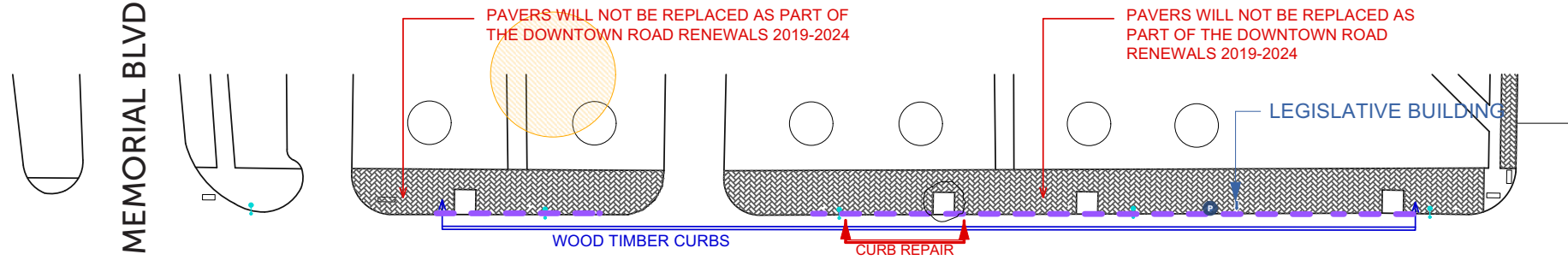


Figure 13. Osborne St to Kennedy St - Existing Site Furnishings and Streetlife Generators.

MEMORIAL BLVD.

MEMORIAL BLVD.



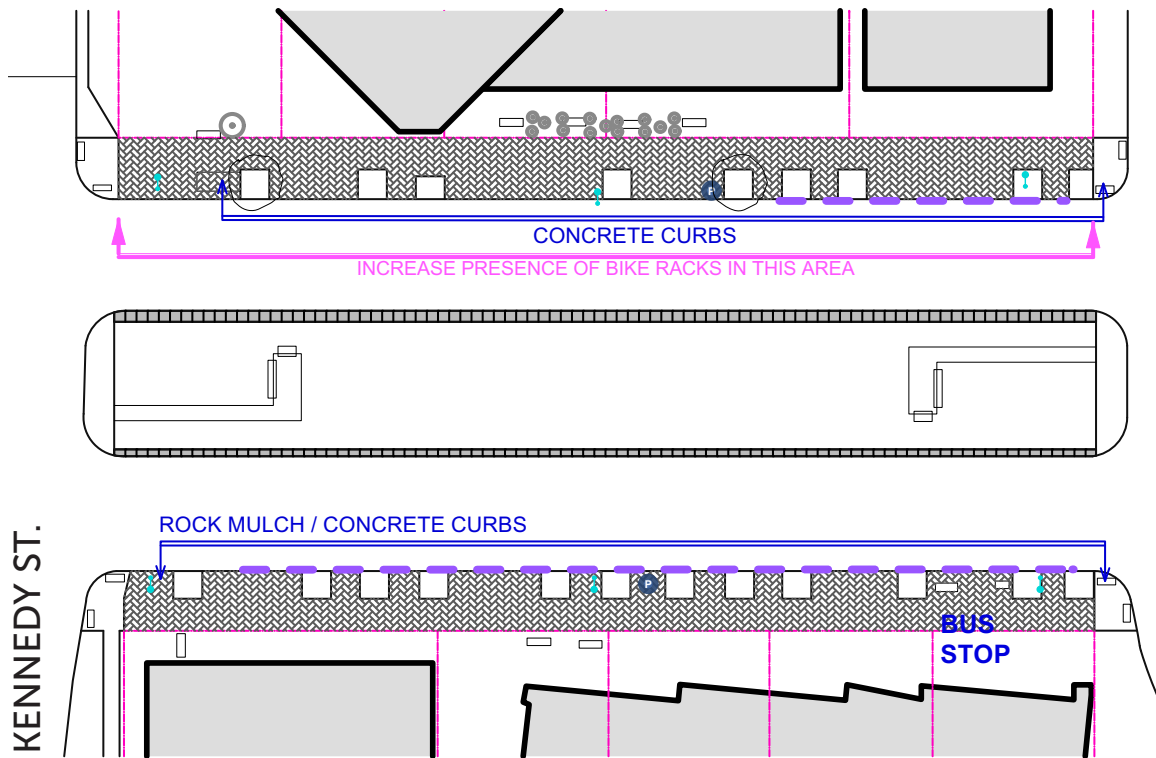
KENNEDY ST.

BROADWAY STREETLIFE GENERATORS:

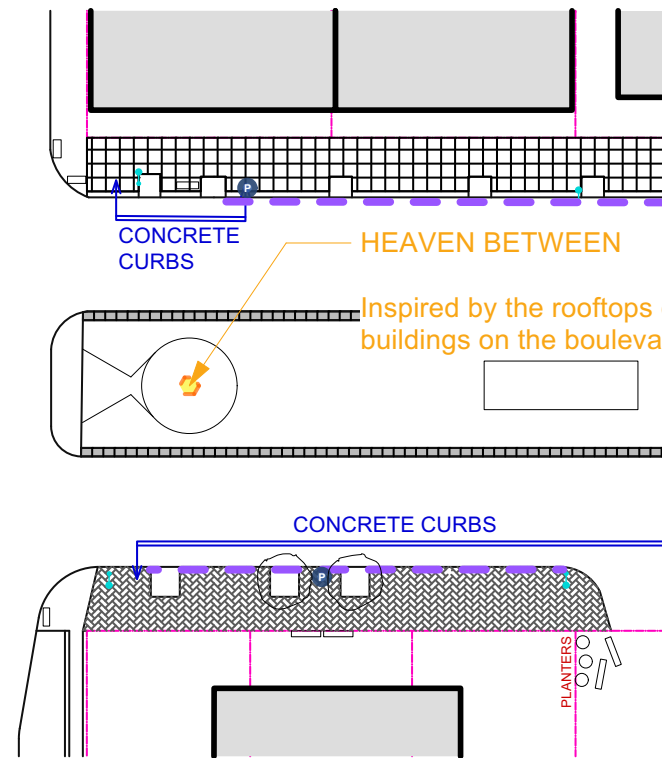
- LANDMARKS
- STAY PLACES I.E. HOTELS (EXISTING & FUTURE)
- RESIDENTIAL (EXISTING & FUTURE)

EXISTING FURNISHINGS:

- PROPERTY LINES
- CONCRETE UNISTONE PAVERS
- CONCRETE BROADWAY PAVERS
- TINTED CONCRETE MEDIAN PADS
- FUTURE STREET LIGHTS
- EXISTING TREE
- EXISTING PARKING METRES
- EXISTING PUBLIC ART
- EXISTING CITY OF WINNIPEG WAYFINDING SIGNAGE DIRECTION
- ON-STREET 2-HOUR PARKING

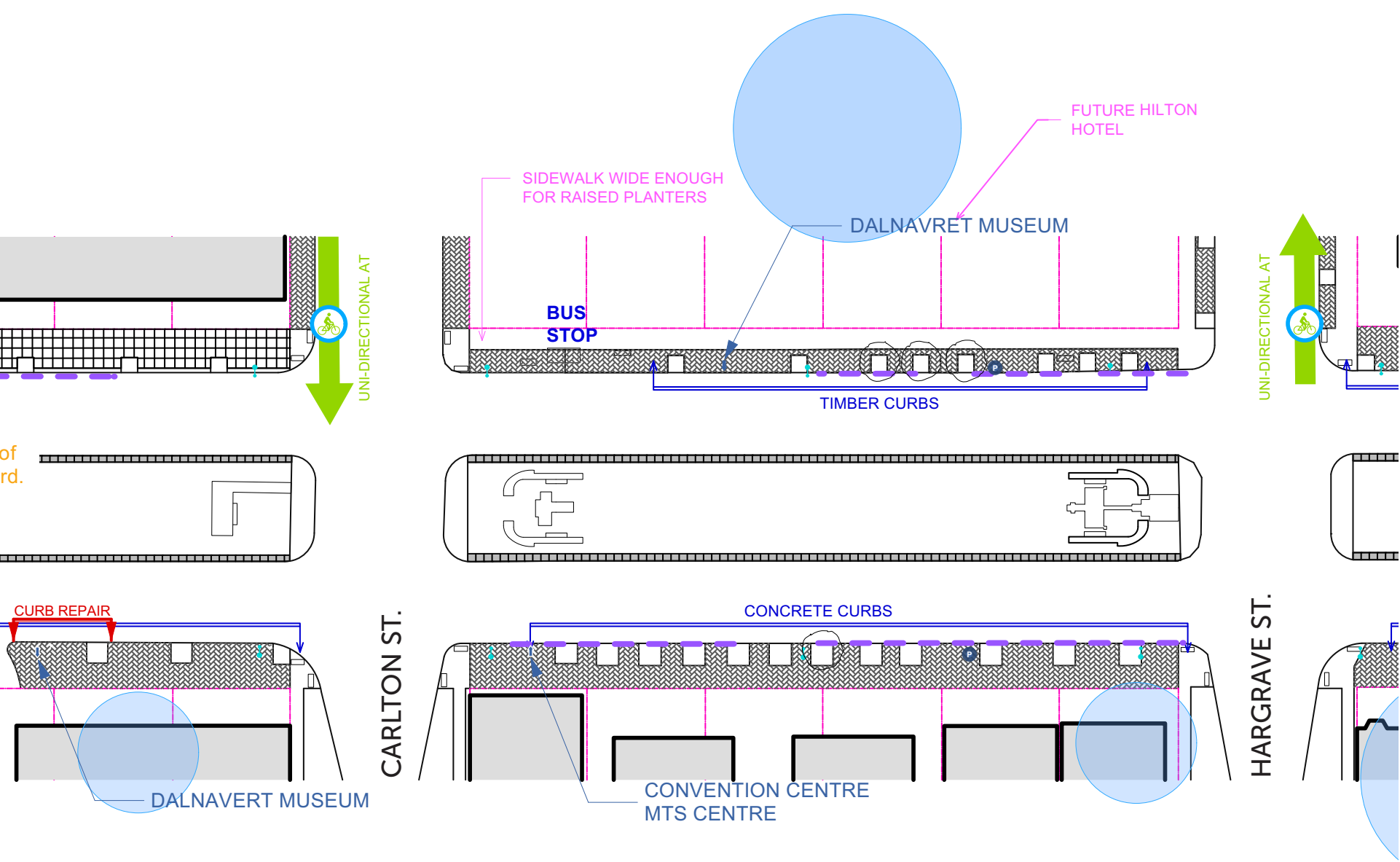


KENNEDY ST.






EDMONTON ST.

Figure 14. Kennedy St to Hargrave St - Existing Site Furnishings and Streetlife Generators.



BROADWAY STREETLIFE GENERATORS:

-  LANDMARKS
-  STAY PLACES I.E. HOTELS (EXISTING & FUTURE)
-  RESIDENTIAL (EXISTING & FUTURE)

EXISTING FURNISHINGS:

-  PROPERTY LINES
-  CONCRETE UNISTONE PAVERS
-  CONCRETE BROADWAY PAVERS
-  TINTED CONCRETE MEDIAN PADS
-  FUTURE STREET LIGHTS
-  EXISTING TREE
-  EXISTING PARKING METRES
-  EXISTING PUBLIC ART
-  EXISTING CITY OF WINNIPEG WAYFINDING SIGNAGE DIRECTION
-  ON-STREET 2-HOUR PARKING

Examples of Existing Site Conditions



Figure 15. Pavers in poor condition.



Figure 16. Lack of bike parking.



Figure 17. *Heaven Between*, a permanent light-based public art at a seating node.



Figure 18. Newer bench seating.



Figure 19. Desire line (informal path) along median.



Figure 20. Poor sidewalk condition.



Figure 21. Wayfinding signs.



Figure 22. Older bench seating.

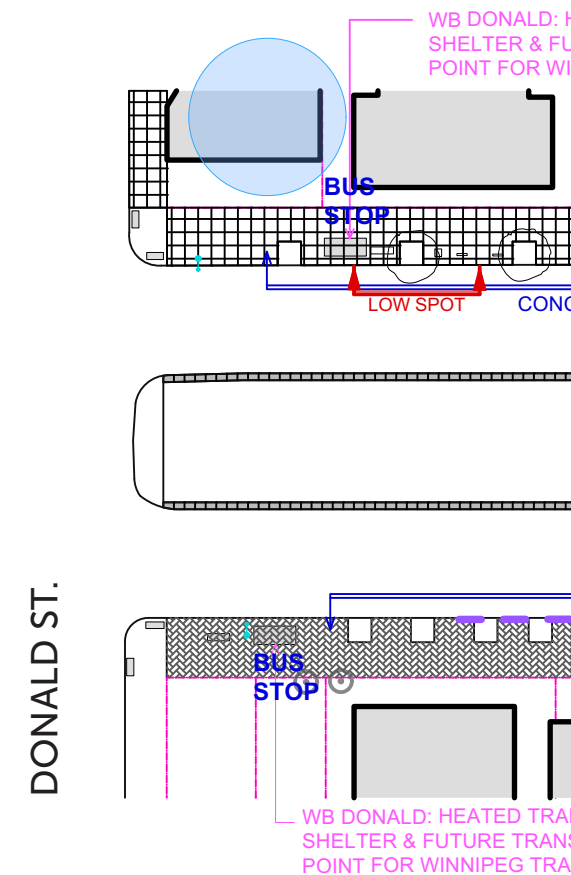
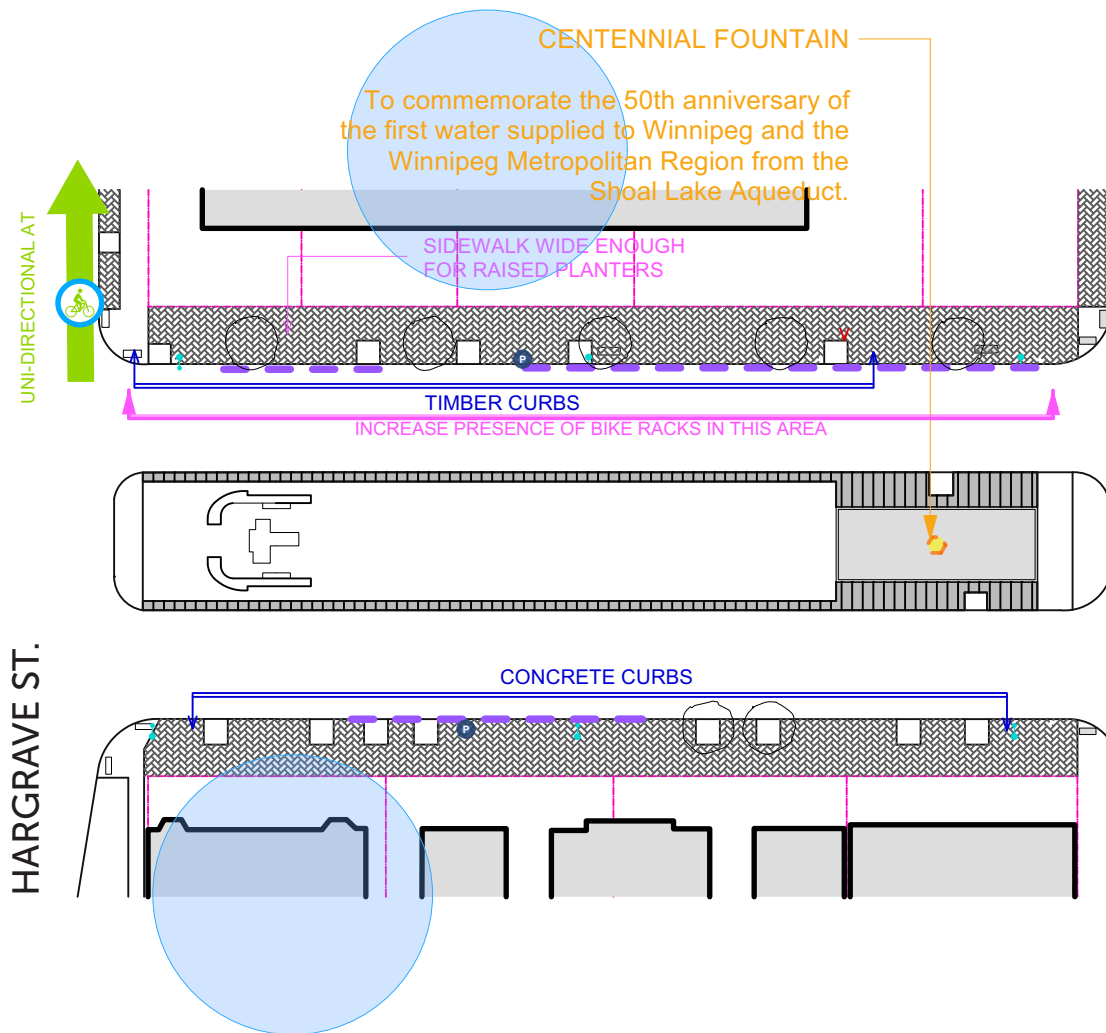
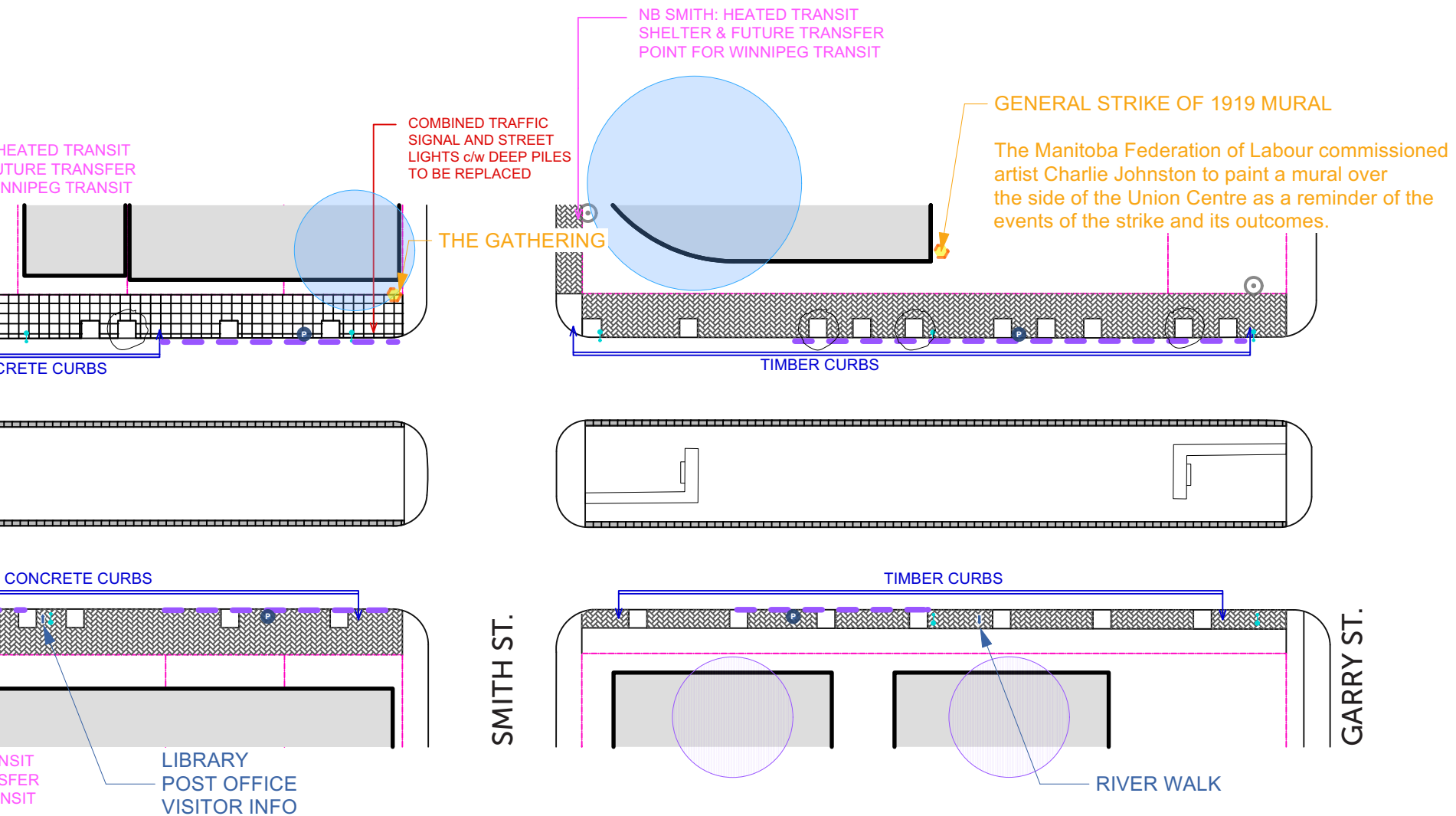


Figure 23. Hargrave St to Garry St - Existing Site Furnishings and Streetlife Generators.



BROADWAY STREETLIFE GENERATORS:

- LANDMARKS
- STAY PLACES I.E. HOTELS (EXISTING & FUTURE)
- RESIDENTIAL (EXISTING & FUTURE)

EXISTING FURNISHINGS:

- PROPERTY LINES
- CONCRETE UNISTONE PAVERS
- CONCRETE BROADWAY PAVERS
- TINTED CONCRETE MEDIAN PADS
- FUTURE STREET LIGHTS
- EXISTING TREE
- EXISTING PARKING METRES
- EXISTING PUBLIC ART
- EXISTING CITY OF WINNIPEG WAYFINDING SIGNAGE DIRECTION
- ON-STREET 2-HOUR PARKING

Examples of Existing Programming



Figure 24. Downtown Winnipeg BIZ free outdoor work space on Broadway.



Figure 25. Free-to-use pop-up workstation (Black & Gold Photography).



Figure 26. Bookable seats and casual seating (Black & Gold Photography).



Figure 27. ManyFest along Broadway (Tourism Winnipeg).



Figure 28. Bears on Broadway.



Figure 29. Cool Gardens installation along Broadway (Canadian Architect).



Figure 30. Art murals along Broadway (The Murals of Winnipeg).



Figure 31. Festival du Voyageur snow sculptures (Denis Vrignon Tessler).

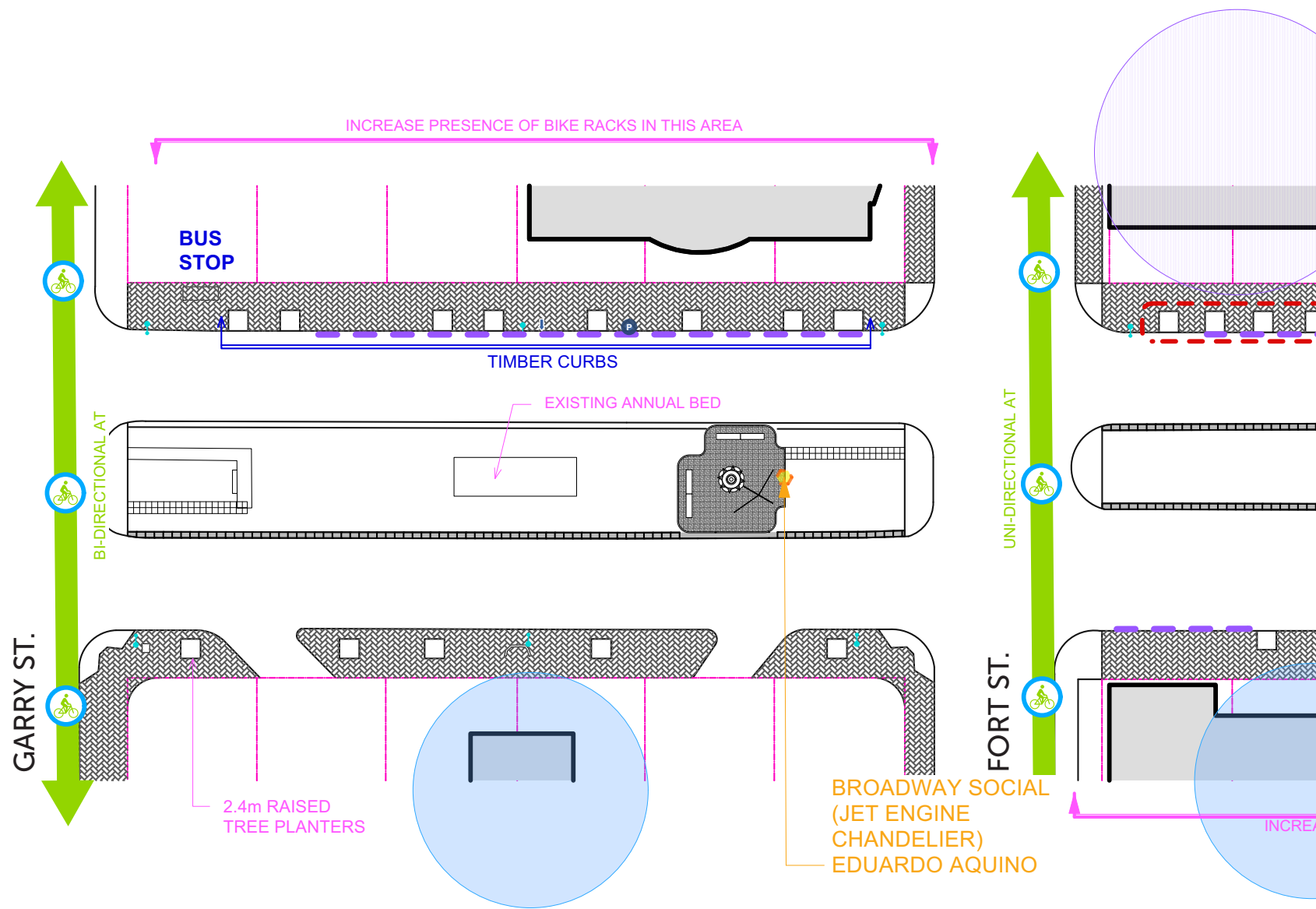
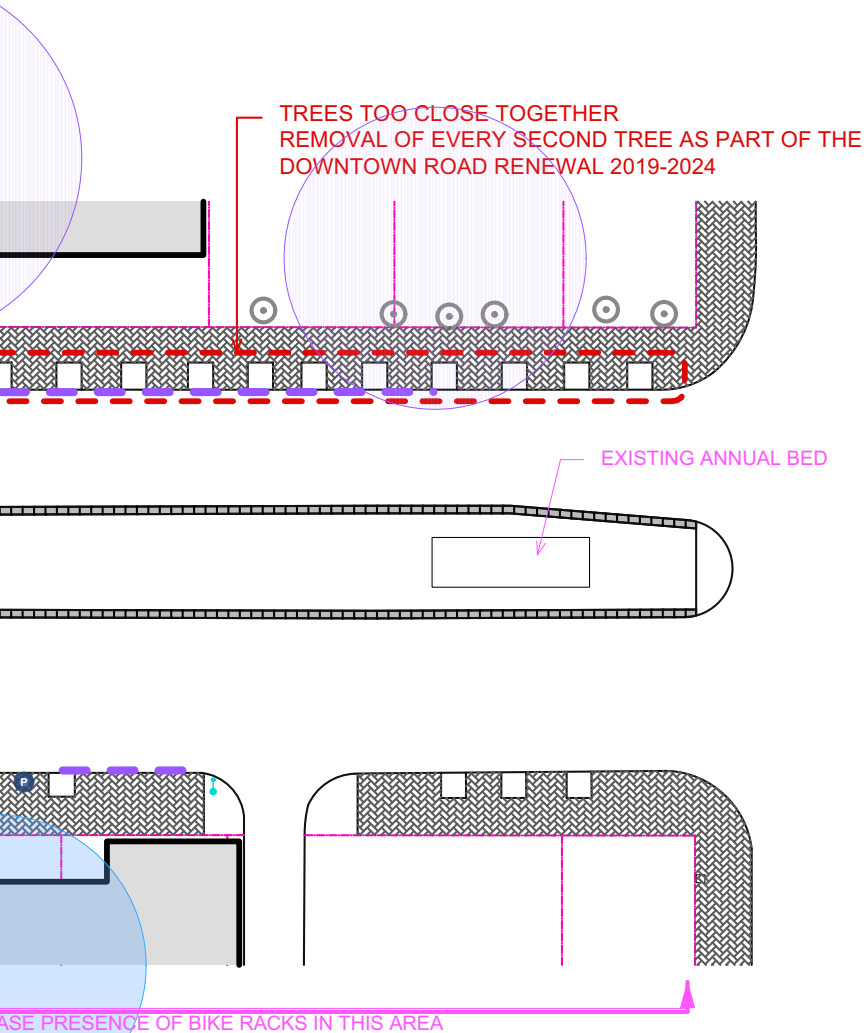
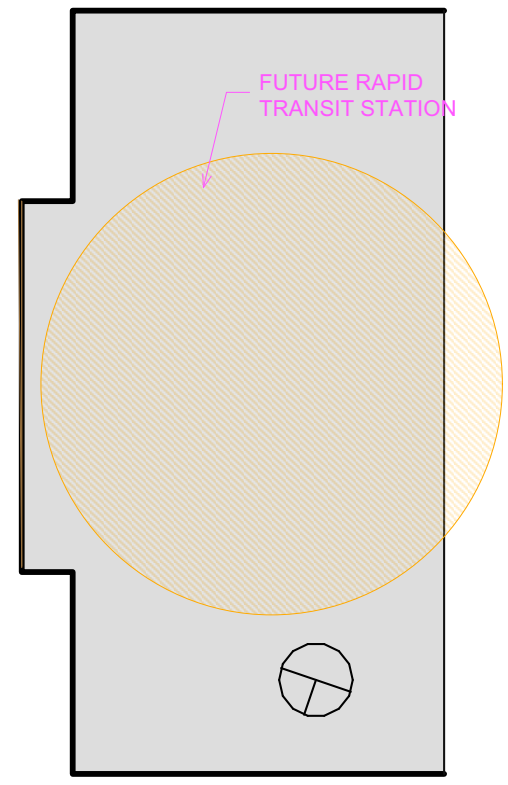


Figure 32. Garry St to Main St - Existing Site Furnishings and Streetlife Generators.



MAIN ST.



BROADWAY STREETLIFE GENERATORS:

- LANDMARKS
- STAY PLACES I.E. HOTELS (EXISTING & FUTURE)
- RESIDENTIAL (EXISTING & FUTURE)

EXISTING FURNISHINGS:

- PROPERTY LINES
- CONCRETE UNISTONE PAVERS
- CONCRETE BROADWAY PAVERS
- TINTED CONCRETE MEDIAN PADS
- FUTURE STREET LIGHTS

- EXISTING TREE
- EXISTING PARKING METRES
- EXISTING PUBLIC ART
- EXISTING CITY OF WINNIPEG WAYFINDING SIGNAGE DIRECTION
- ON-STREET 2-HOUR PARKING

5.0 Vision

5.1 Overview

The design responds to the primary preferences identified during the engagement sessions:

- Increased trees and park-like green space
- Welcoming lighting
- Maintain iconic wide boulevards, medians, and historic building façades
- Activation of the medians through seating and art
- Revive Broadway's identity as both a destination and a gateway to the cultural downtown core

Both the site analysis and engagement feedback emphasized the importance of Broadway maintaining its iconic tree canopy. Therefore, more trees have been added in conjunction with soil cells and appropriate placements (further from the road) will entrust this green corridor for future generations.

To add to pedestrian comfort and extend the hours of use, aimable cutoff light fixtures integrated into the street light standards are proposed at each intersection with a seating node. Intersections without seating nodes have areas assigned for future public art installations, with the understanding that many of these installations will incorporate lighting, which will add to the vibrant streetscape.

Analysis showed a lack of bike racks, therefore more have been allocated for, keeping in mind the increase in active transportation infrastructure with the Downtown Bike Strategy. With support from the City of Winnipeg, the

Downtown Winnipeg BIZ will be undertaking a short-term bike parking strategy to highlight best practices and refining bike rack placement.

A lack of benches and spaces for outdoor lunches has been rectified through the addition of thoughtful seating nodes in some of the medians. Per City of Winnipeg Parks and Forestry department requests, care has been taken to situate seating nodes away from tree roots and major plantings to allow for ease of maintenance. Additionally, as Broadway shifts towards more residential spaces, the use of the streetscape will extend beyond typical business hours. The additional seating has been integrated to accommodate the change of use from commercial to residential.

Many participants during engagement sessions said they felt Broadway was a gateway destination and that this quality should be enhanced, therefore the east entrance at Main St has a full planting bed and a designated art location to make it clear entry point. Paving bands at the west and east entrances to Broadway (Osborne St and Main St) are distinct and usher visitors into this mixed-use district.

The median will host four major public art pieces that are 'occupiable': pieces that can be inhabited as social spaces like the three existing ones (Heaven Between, Centennial Fountain, and Broadway Social). The location of the fourth is tentatively proposed at the Memorial/Kennedy median, to be confirmed with the Winnipeg Arts Council. In addition, space for a seasonal snow sculpture at the Main Street end of the median, and two sculptural obelisks that should be subject to a design competition or public art commission, mark the limits of the corridor. These future public art installations within the Broadway corridor in the vicinity of known burial sites should consider this in the brief to artists.



Figure 33. Rendering of median seating area showing reconfigured limestone walls, naturalized plantings, raised annual planters and accessible seating.

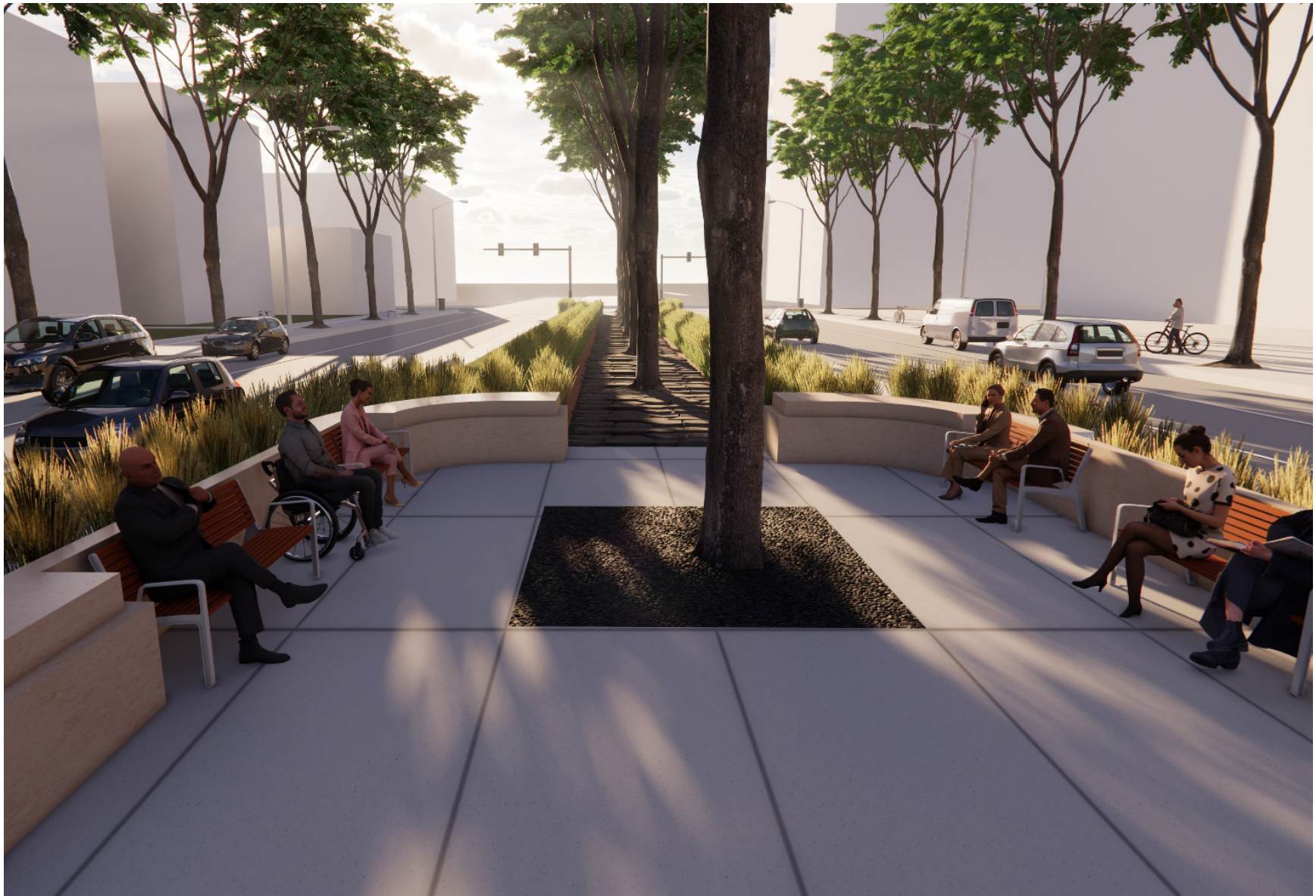


Figure 34. Rendering of median node highlighting slate slab treatment at the base of restored tree canopy.



Figure 35. Rendering of custom lighting.

5.2 Materials Palette



Figure 36. Modified street light with adjustable fixtures on existing base



Figure 37. Waste receptacles



Figure 38. Backed benches for seating nodes



Figure 39. Entry obelisk



Figure 40. Crosswalk mural art



Figure 41. Limestone seating node walls



Figure 42. Shrub bed mulch, warm brown in colour



Figure 43. Slate flagstone underneath median trees (Coen + Partners)



Figure 44. Slate flagstone and the interaction with trees (Coen + Partners)



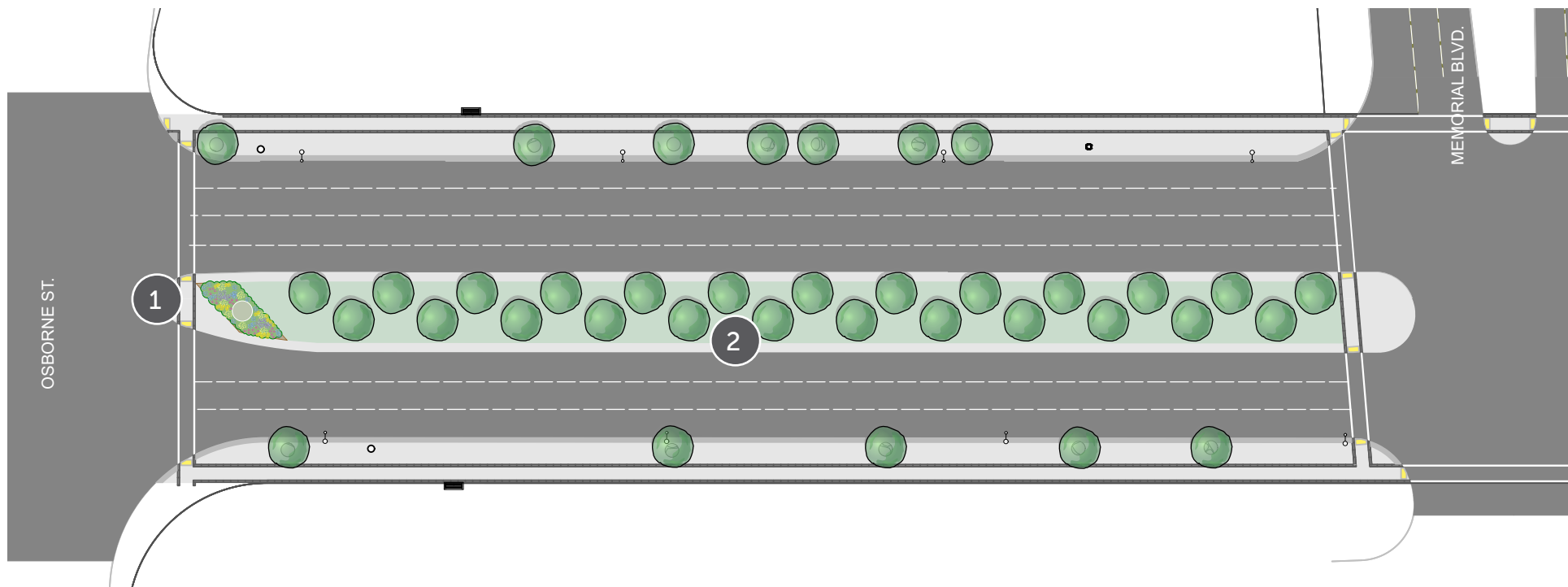
Figure 45. Median and "avenue" planting of Elms



Figure 46. Perennial grasses with upright spreading, similar to Karl Foerster

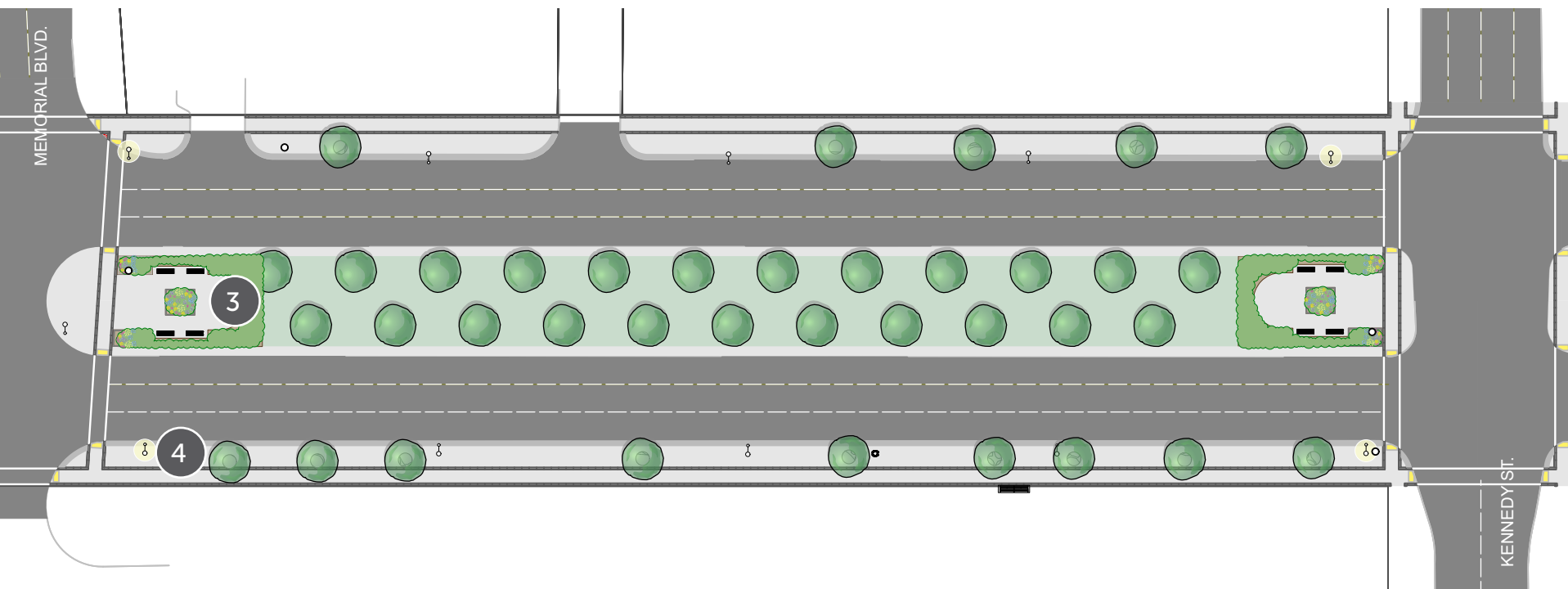


Figure 47. Flowering annuals

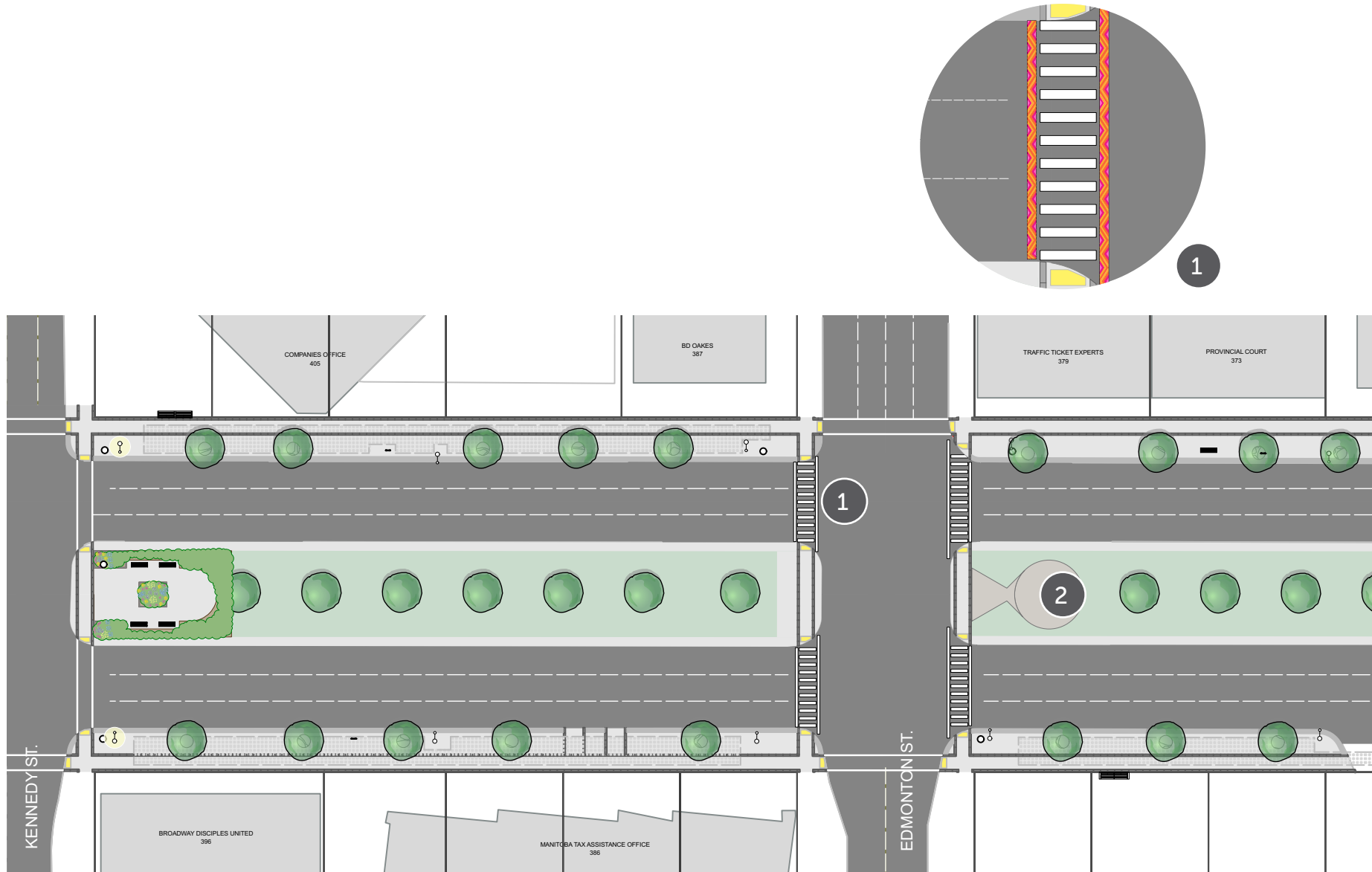


- 1 At either end of Broadway (Osborne and Broadway, Main and Broadway) a signifying marker such as an obelisk is proposed to denote the entrance to this historic street.
- 2 Tree placement determined by the Broadway Future Tree Strategy. Trees to include tree root trenches.

Figure 48. 1:650 scale plan of proposed design for Osborne St to Kennedy St

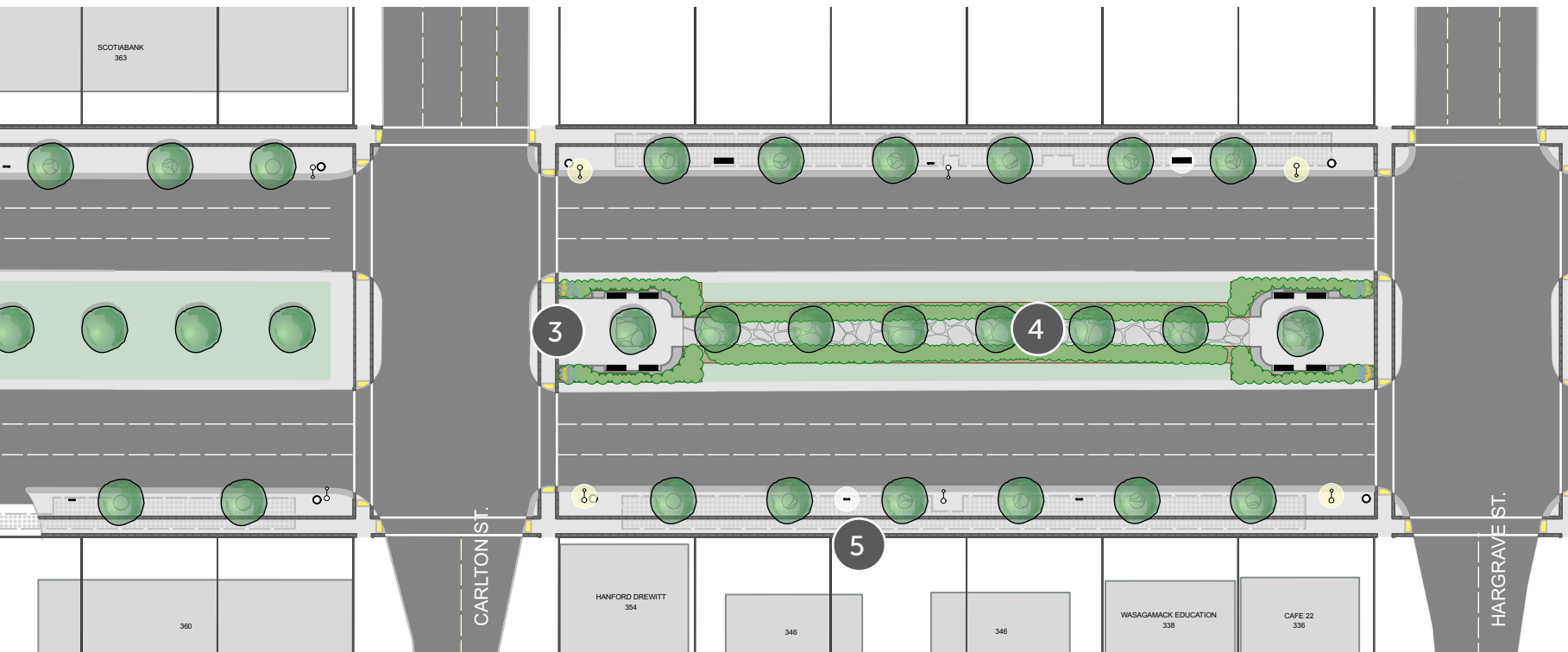


- 3 Seating nodes at either end of the median have raised planting beds with perennial grasses and flowering annuals near the end. Additionally, raised annual beds are located at the centre of the seating nodes. The annuals can be easily swapped out to allow for future public art installations. Accessible seating nodes include benches with backrest and armrests.
- 4 LED (RGB) pole mount area lighting attached to existing Hydro pole to provide lighting for median seating nodes.

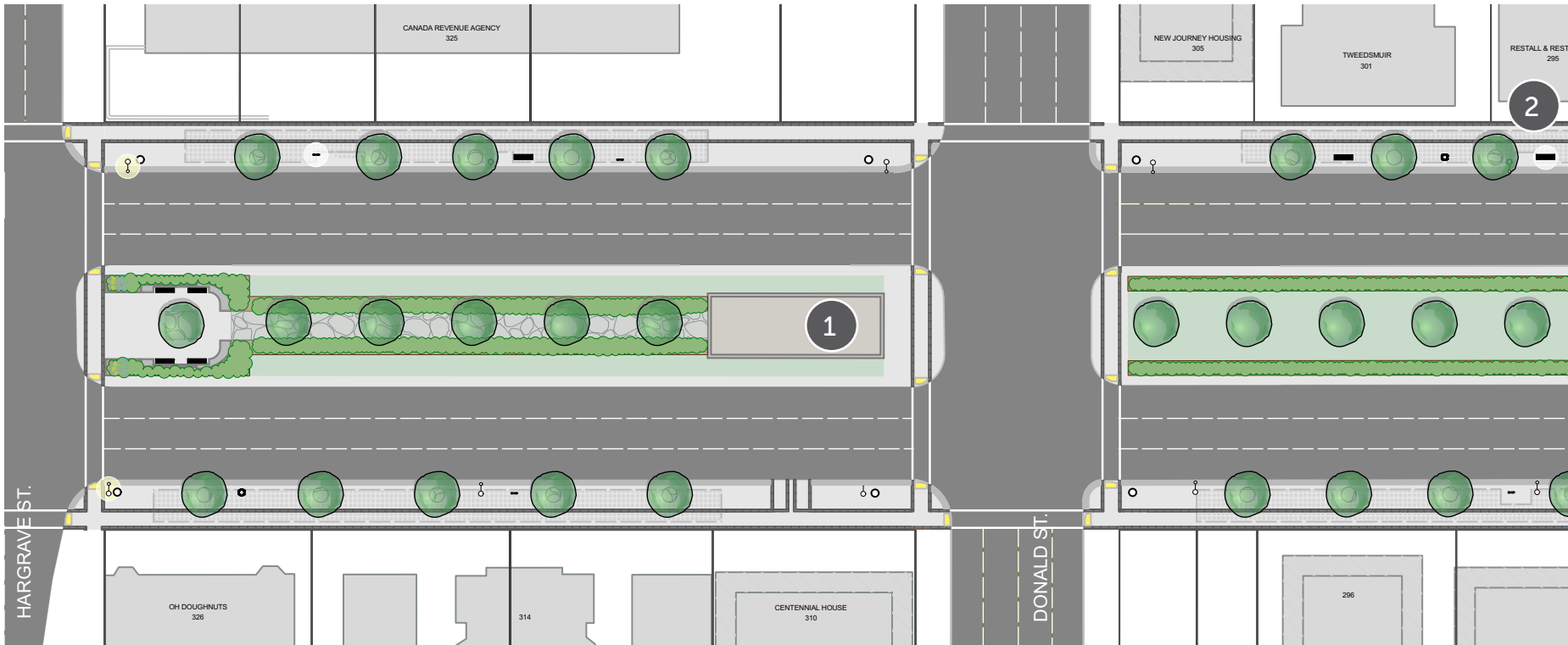


- 1 Future crosswalk mural art within the two longitudinal transverse lines for events and programming.
- 2 Existing public art (*Heaven Between*).

Figure 49. 1:650 scale plan of proposed design for Kennedy St to Hargrave St

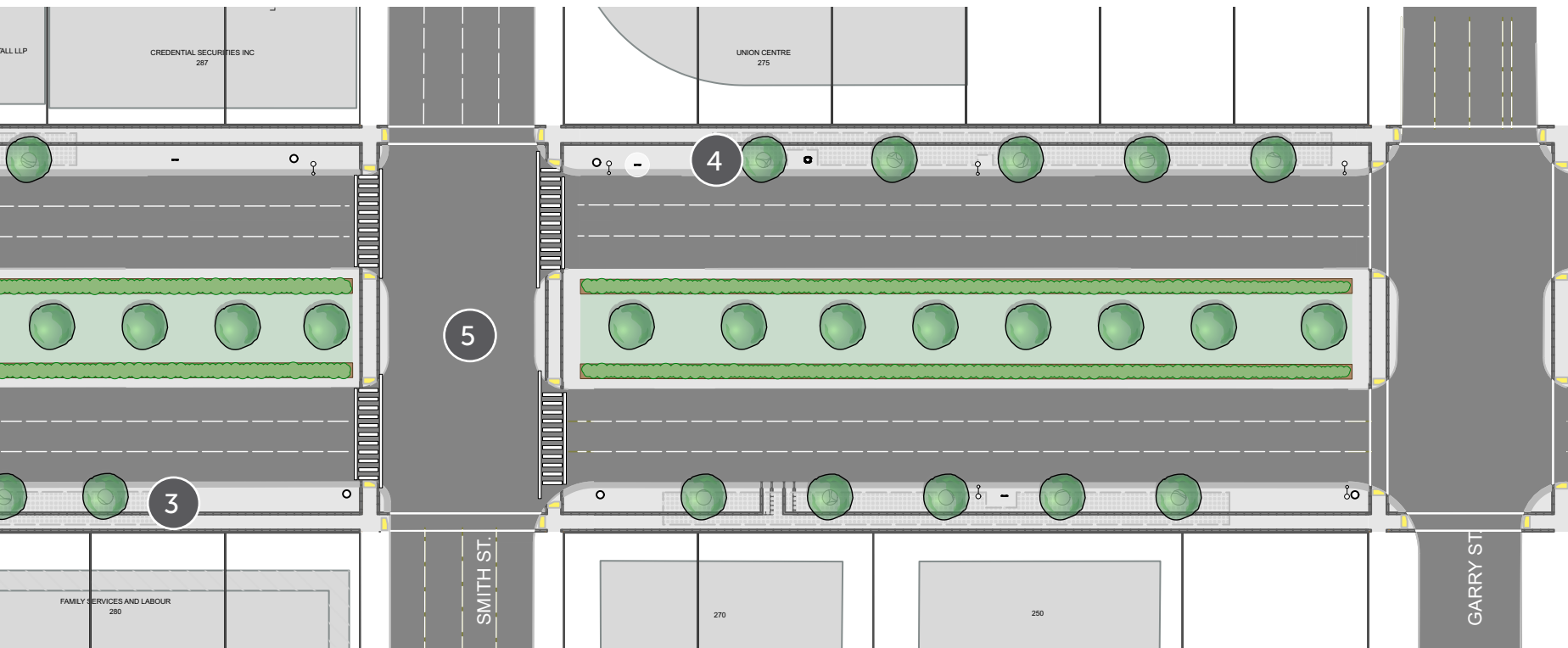


- 3 Seating nodes with tree instead of perennial planting bed.
- 4 Naturalized planting strip with two rows of grasses and slate flagstone underneath median trees.
- 5 Additional bike racks.

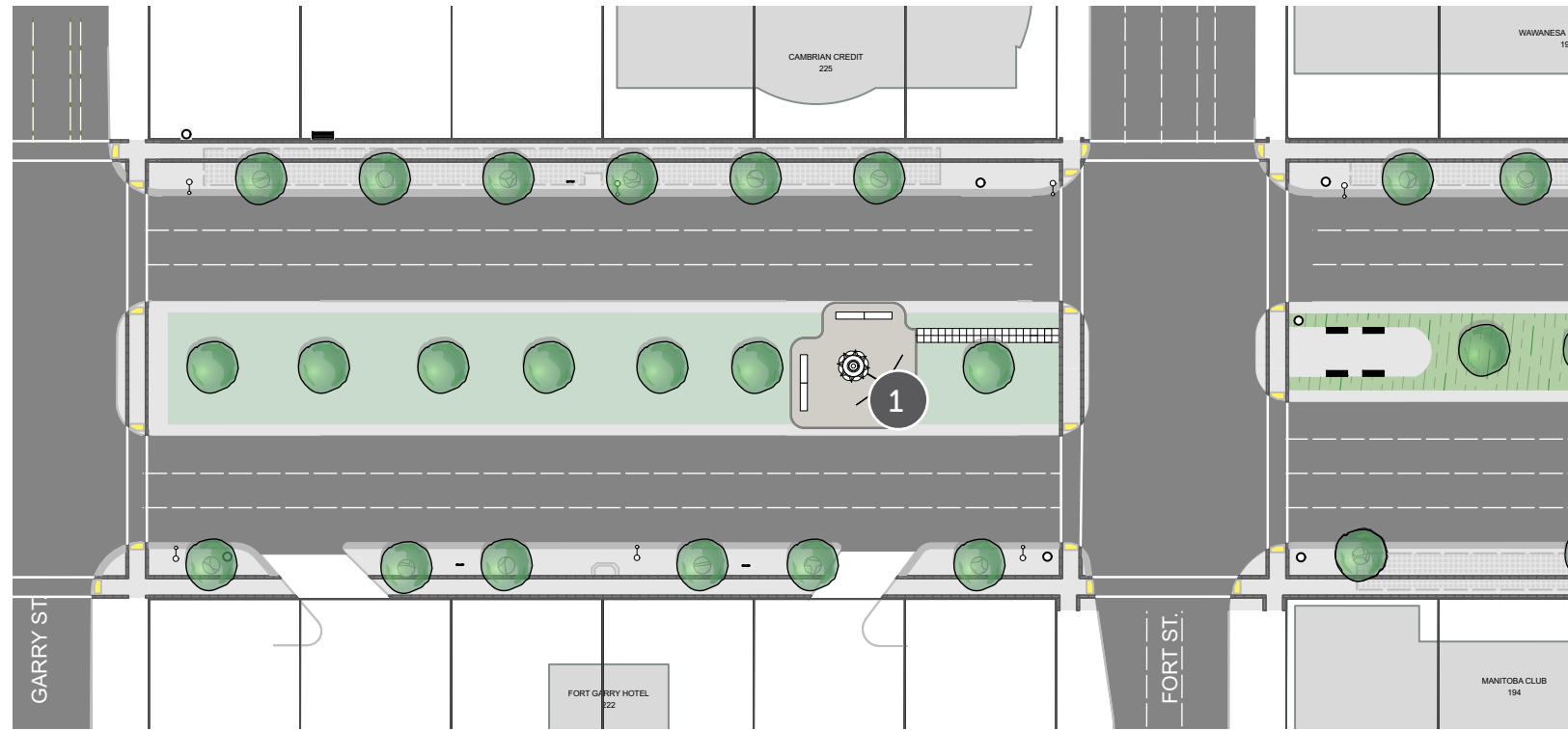


- 1 Existing Centennial Fountain with Honouring Shoal Lake 40 landscaping.
- 2 Additional benches.
- 3 Soil cells as part of 2019-2024 Downtown Streets Renewal Project.

Figure 50. 1:650 scale plan of proposed design for Hargrave St to Garry St



- 4 Boulevard tree wells to be a minimum of 1500mmØ with granite mulch.
- 5 Future crosswalk mural art within the two longitudinal transverse lines for events and programming.



- 1 Existing public art (*Broadway Social*).
- 2 Naturalized planting bed of grasses to celebrate and invite into Broadway.

Figure 51. 1:650 scale plan of proposed design for Garry St to Main St



- 3 Entry obelisk is proposed to denote the entrance to this historic street with space for annuals.
- 4 Space allocated for Festival du Voyageur ice sculpture and future public art.

6.0 Costs

6.1 Class '4' Cost Estimate

Estimated construction costs were prepared by HTFC Planning & Design to a Class '4' cost classification based on the consultants' conceptual work on the streetscape design vision. Prices are defined and classified based on the stage of a project's development and the level of information available at the time of estimate.

6.2 Exclusions

The Cost Estimate does not include the Forestry compensation fee for the removal of trees along Broadway. Additionally, the estimate does not include the new or upgraded transit stations, as this will be part of The Rapid Transit Downtown Corridors project, which could also have the capacity to include the recommended unit pavers in the boulevard amenity zone to define the entries of Broadway at Osborne and Main Street. The street furnishings included in the cost estimate represent supplementary fixtures as highlighted in the Vision; furnishings purchased through the Broadway Site Furnishings are treated separately and excluded from this estimate. Upgrades to site electrical are excluded from this cost estimate, but should

be considered for future public art and lighting along the median. Public art coordination, artist fees and consultation fees is not included, unless otherwise noted.

Other exclusions:

- GST
- Financing costs
- Winter construction premiums
- Third party commissioning costs
- Premium for phased construction

Based on these limits and exclusions noted above, a summary of construction costs is provided in Figure 50.

	Description	Qty.	Unit	Unit Price	Cost	Total
A.	Demolition & Site Preparation					
	1 Clearing & Grubbing & Rough Grading	6802	sq.m	\$ 40	\$ 272,080	
B.	Site Furnishings					
	1 MMcite Miela Backless Bench (Standard)	2	each	\$ 1,937	\$ 3,874	
	2 MMcite Miela Backed Bench (Standard)	28	each	\$ 2,394	\$ 67,032	
	3 Landscapeforms Poe Litter Receptacle	4	each	\$ 3,400	\$ 13,600	
	4 Maglin Single Hoop Bike Rack	4	each	\$ 595	\$ 2,380	
	5 Granite Slate	290	sq.m.	\$ 400	\$ 116,000	
C.	Site Lighting/Electrical					
	1 Modified Street Light with Aimable Fixtures on Existing Base	12	each	\$ 7,500	\$ 90,000	
D.	Pedestrian Facilities & Landscaping					
	1 Crossings Line Painting (Mural)	4	allow	\$ 2,500	\$ 10,000	
	2 CIP Concrete (Seating Areas)	635	sq.m.	\$ 200	\$ 127,000	
	3 CIP Concrete Curbs (Curb & Gutter - Median at 1m width)	2240	l.m.	\$ 330	\$ 739,200	
	4 Mulch Beds (100mm depth planting medium & 75mm depth woodchip mulch)	660	sq.m.	\$ 40	\$ 26,400	
	5 Granite Mulch (75mm depth, 19mm (3/4") clean washed granite mulch)	318	sq.m.	\$ 60	\$ 19,080	
	6 Deciduous Trees	122	each	\$ 800	\$ 97,600	
	7 Sod Restoration	260	sq.m.	\$ 20	\$ 5,200	
	8 Perennials (Flowering & grasses)	1500	each	\$ 75	\$ 112,500	
	9 Native Grass Seed Mix (Short grass mix)	475	sq.m.	\$ 30	\$ 14,250	
E.	Limestone Seating Area Walls					
	1 Demolition & Reconstruction of Existing	6	allow	\$ 20,000	\$ 120,000	
	2 Supply and Installation of Limestone Wall Extensions	6	tonne	\$ 2,500	\$ 15,000	
F.	Signage					
	1 Entry Obelisk	2	allow	\$ 80,000	\$ 160,000	
				subtotal	\$ 2,011,196	
				gc overhead and profit 10%	\$ 201,120	
				design fees 7%	\$ 140,784	
				contingency 20%	\$ 402,239	
				Total	\$ 2,755,339	
				Development Budget Envelope	\$ 2,760,000	

Figure 52. Class '4' Cost Estimate

	Description	Qty.	Unit	Unit Price	Cost	Total
A.	Demolition & Site Preparation					
	1 Clearing & Grubbing & Rough Grading	140	sq.m	\$ 40	\$ 5,600	
B.	Site Furnishings					
	1 MMcite Miela Backed Bench (Standard)	4	each	\$ 2,394	\$ 9,576	
C.	Limestone Seating Area Walls					
	1 Demolition & Reconstruction of Existing	2	allow	\$ 20,000	\$ 40,000	
	2 Supply and Installation of Limestone Wall Extensions	2	tonne	\$ 2,500	\$ 5,000	
D.	Pedestrian Facilities & Landscaping					
	1 CIP Concrete (Seating Areas)	80	sq.m.	\$ 200	\$ 16,000	
	2 CIP Concrete Curb (1m width)	200	lm.	\$ 150	\$ 30,000	
	3 Mulch Beds (c/w 100mm depth planting medium & 75mm depth woodchip mulch)	40	sq.m.	\$ 40	\$ 1,600	
	4 Granite Mulch (75mm depth, 19mm (3/4") clean washed granite mulch)	10	sq.m.	\$ 60	\$ 600	
	5 Perennials (Flowering & grasses)	100	each	\$ 75	\$ 7,500	
	6 Deciduous Trees	1	each	\$ 800	\$ 800	
	7 Sod Restoration	10	sq.m.	\$ 20	\$ 200	
					subtotal	\$ 116,876
					gc overhead and profit 10%	\$ 11,688
					design fees 7%	\$ 8,181
					contingency 20%	\$ 23,375
					Total	\$ 160,120
					Development Budget Envelope	\$ 160,000

Figure 53. Seating Area Option A - Cost Breakdown

	Description	Qty.	Unit	Unit Price	Cost	Total
A.	Demolition & Site Preparation					
	1 Clearing & Grubbing & Rough Grading	155	sq.m	\$ 40	\$ 6,200	
B.	Site Furnishings					
	1 MMcite Miela Backed Bench (Standard)	4	each	\$ 2,394	\$ 9,576	
	2 Landscapeforms Poe Litter Receptacle	1	each	\$ 3,400	\$ 3,400	
C.	Pedestrian Facilities & Landscaping					
	1 CIP Concrete (Seating Areas)	92	sq.m.	\$ 200	\$ 18,400	
	2 CIP Concrete Curb (1m width)	200	l.m.	\$ 150	\$ 30,000	
	Mulch Beds (c/w 100 mm depth planting medium & 75mm					
	3 depth woodchip mulch)	72	sq.m.	\$ 40	\$ 2,880	
	4 Perennials (Flowering & grasses)	125	each	\$ 75	\$ 9,375	
	5 Sod Restoration	10	sq.m.	\$ 20	\$ 200	
					subtotal	\$ 80,031
					gc overhead and profit 10%	\$ 8,003
					design fees 7%	\$ 5,602
					contingency 20%	\$ 16,006
					Total	\$ 109,642
					Development Budget Envelope	\$ 110,000

Figure 54. Seating Area Option B - Cost Breakdown

7.0 Next Steps

Implementing the strategy will require coordination between a number of City departments and agencies, including Forestry, Property and Planning and Development, Public Works, Parks, and the Winnipeg Arts Council, along with the Downtown Winnipeg BIZ. A project committee could be struck with representatives from each to ensure that decisions are synchronized and broadly supported, and all groups are communicating regularly and effectively.

The recommended order of priorities for implementation are:

1. Supplemental site furnishings
2. Tree planting
3. Median curb reconstruction
4. Median landscape redevelopment – soft landscaping and hard landscaping can be separate phases
5. Lighting upgrades
6. Gateway obelisks
7. Public art installations
8. Programming

As development unfolds within the right-of-way and on adjacent properties, some strategic directions may require adjustment or overhaul due to changes in context and funding. The Planning Department and BIZ should internally review this plan at least every two years to ensure it remains relevant and aligned with available resources.

Appendices

Appendix 1: Focus Group Presentation

Broadway Streetscape Vision

Focus group session
March 2023



Agenda

1. Welcome & introductions
2. Meeting objectives
3. Project background
4. Activities
5. Next steps



2



Welcome & introductions

Marsha Christiuk, City of Winnipeg

Jori Pincock, Downtown Biz

Thomas Findlay, AECOM

Glen Manning, HTFC Planning & Design

Michaela Peyson, HTFC Planning & Design

3



Meeting objectives

In this session we aim to:



Build a common understanding of what streetscape elements work together to define Broadway



Collect feedback on your priorities for locations and types of streetscape improvements



Ensure you can ask questions and provide feedback

4



Project background

Project overview

Project background

The Broadway Streetscape Vision seeks to identify opportunities for how all streetscape elements along Broadway (from Osborne Street to Main Street) can work together to reinforce the character and significance of one of Winnipeg's most iconic streets.

The project will also consider and build upon:

- Broadway Future Tree Strategy (City of Winnipeg)
- Broadway Site Furnishings (Downtown BIZ)
- 2023 Broadway Eastbound Road Renewal (City of Winnipeg)

6



Project overview

Project background

The identified opportunities and placemaking elements will align with the framework and streams identified in the *Downtown Recovery Strategy*.

Winnipeg's downtown recovery strategy framework



Questions?

Definitions

Project background

Streetscape:

- The area between the building frontages and the curb (i.e., the sidewalk where most elements are located)
- The median is also part of the streetscape

Streetscape elements can include:

- Site furnishings (benches, waste receptacles)
- Landscaping (trees, planters)
- Lighting
- Wayfinding
- Public art
- Placemaking to support programming



Broadway looking east

Streetscape elements

Project background



Wayfinding



Public art



Permanent lighting



Green infrastructure



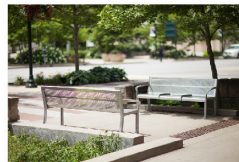
Crosswalk art / decorative crosswalks



Patio space for restaurants

Streetscape elements

Project background



Seating



Plantings



Spaces for gathering / hosting events



Seasonal lighting



Bike racks



Paving

Streetscape programming

Project background



Food trucks



Street festivals



Temporary art installations



Historic tours

Activities

Activity

Discussion & questions

- 1 If you could use one word to describe the character of Broadway, what would that word be? *Open discussion*

14



Activity

Discussion & questions

- 2 When you think of Broadway's streetscape, are there specific areas along Broadway where we should focus efforts to make the biggest impact? *Open discussion*

- Osborne to Memorial
- Memorial to Kennedy
- Kennedy to Carlton
- Carlton to Donald
- Donald to Garry
- Garry to Main

15



Activity

Discussion & questions

- 3 From your perspective, what challenges do you currently see in the public realm along Broadway? *Open discussion*

- Not enough places to sit
- Not enough lighting
- Not enough wayfinding
- Unsafe crossings
- Inconsistent sidewalks
- Discontinuous bike lanes
- Not enough shade
- Not enough secure bike racks
- Other

16



Activity

Discussion & questions

4

What are the **top three** aspects that you think would help make Broadway more welcoming to people and encourage them to spend more time in the area? *Open discussion*

- Accessible and inclusive wayfinding
- Public art (local artists, design competitions, etc.)
- Unique lighting to beautify public spaces year-round
- Connected and accessible path system that supports a wide range of activities (i.e., cycling, walking, mobility aids)
- Enhancing pedestrian access to the median
- Prioritize tree planting
- Add more site furnishings (i.e., benches, waste receptacles, bike racks)
- Add plantings to enhance vibrancy along the median and boulevards
- Create spaces for gathering or to host events

17



Activity

Discussion & questions

5

What aspects do you perceive to be the key motivators in generating everyday life on the street along Broadway? *Open discussion*

- Food trucks
- Temporary/ pop-up art installations i.e., Cool Gardens
- Historic tours / art walks
- Shopping / restaurants
- Other

18



Activity

Discussion & questions

6

How important do you feel the following elements are in the future design of this space? *Open discussion*

- Bike racks
- Public art
- Seating
- Wayfinding/interpretive signage/banners
- Gateway markers, such as signage and plantings to define key entry areas
- Permanent lighting
- Seasonal or temporary lighting
- Landscaping (trees, planters)
- Furnishings
- Seasonal weather protection
- Decorative crosswalks
- Patio space for restaurants

19



Activity

Discussion & questions

7

Are there additional elements you feel are important to include along Broadway that we did not capture? *Open discussion/Miro board*

20



Next steps

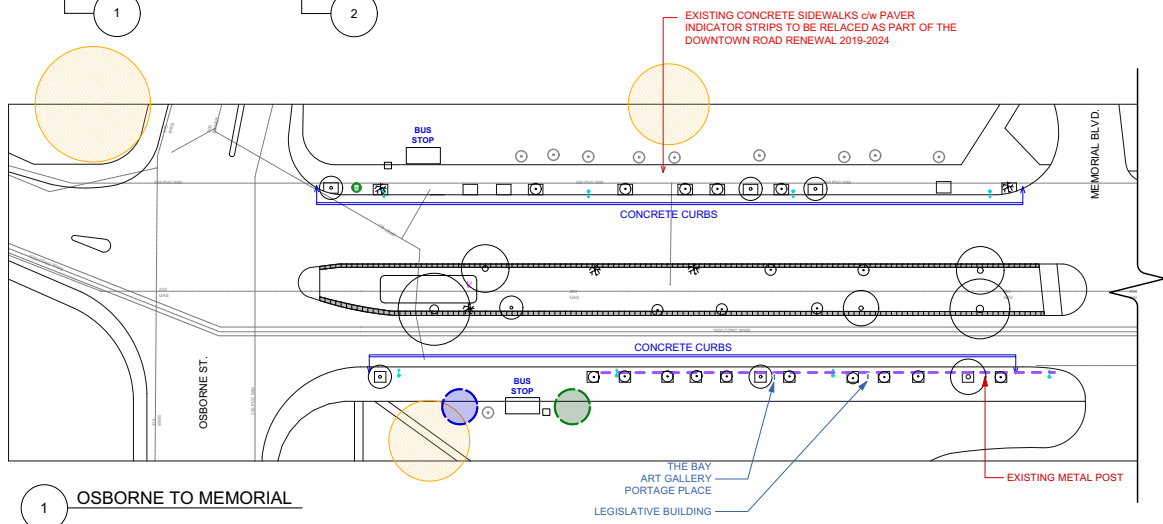
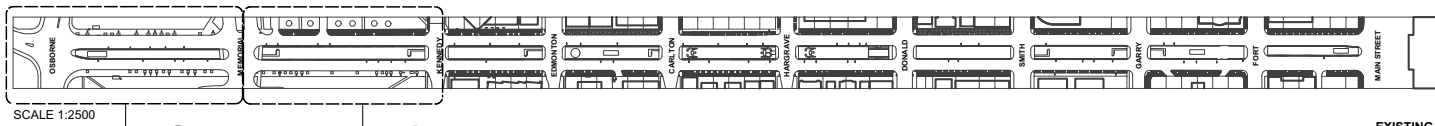
Broadway Streetscape Vision

Thank you

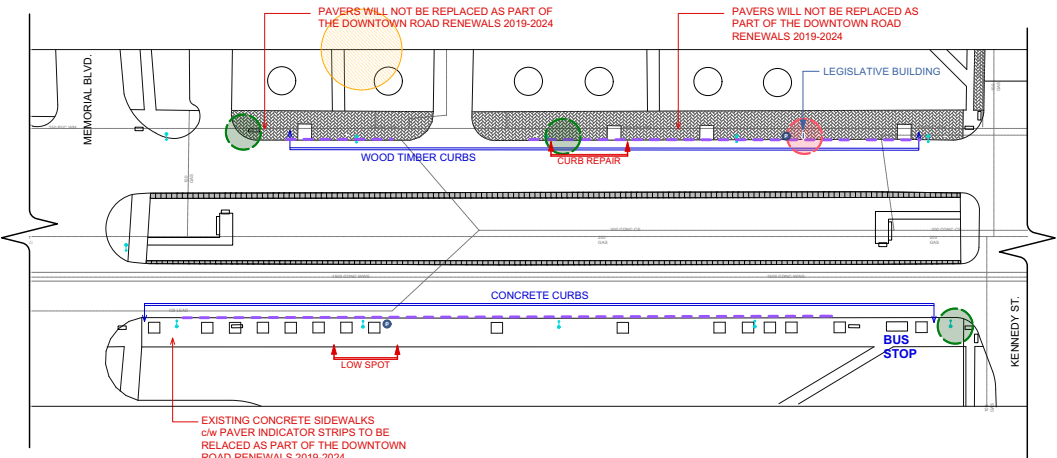


Appendix 2: Focus Group & Interview Records of Discussion

Appendix 3: Detailed existing conditions maps



1 OSBORNE TO MEMORIAL



2 MEMORIAL TO KENNEDY

EXISTING FURNISHINGS:

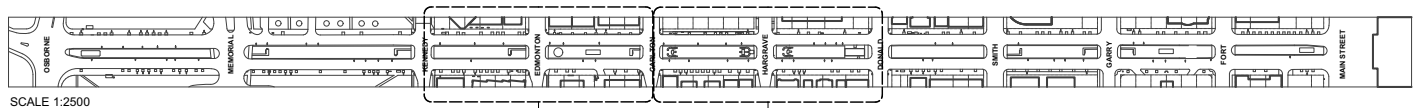
- PROPERTY LINES
- CONCRETE UNISTONE PAVERS
- CONCRETE BROADWAY PAVERS
- TINTED CONCRETE MEDIAN PADS
- FUTURE STREET LIGHTS
- EXISTING TREE
- EXISTING PARKING METRES
- EXISTING PUBLIC ART
- EXISTING TAKE PRIDE WINNIPEG WASTE RECEPTACLE
- EXISTING CITY OF WINNIPEG WAYFINDING SIGNAGE DIRECTION
- EXISTING CIGARETTE BUTT RECEPTACLE
- ON-STREET 2-HOUR PARKING

BROADWAY STREETLIFE GENERATORS:

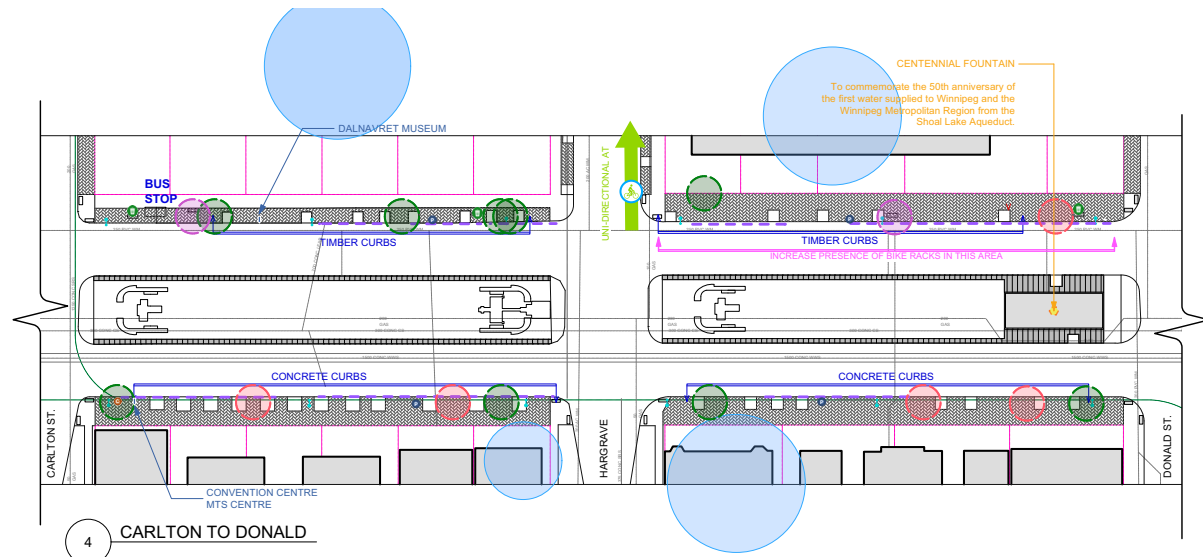
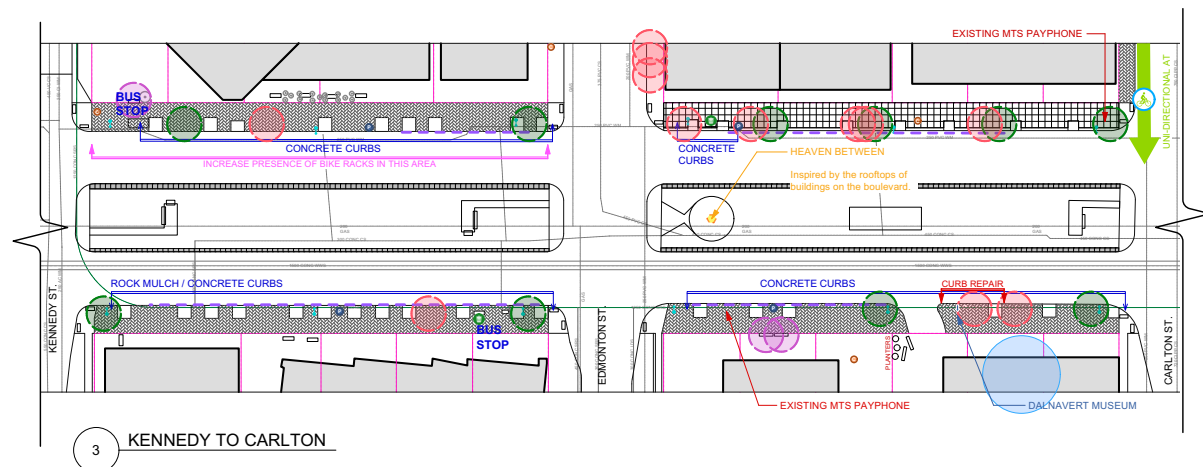
- LANDMARKS
- STAY PLACES I.E. HOTELS (EXISTING & FUTURE)
- RESIDENTIAL (EXISTING & FUTURE)

BROADWAY STREET FURNISHINGS REMOVAL:

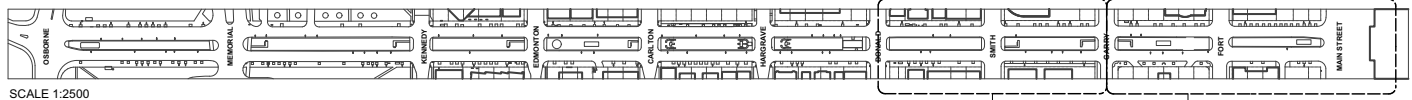
- EXISTING WASTE RECEPTACLE
- EXISTING BACKLESS BENCH
- EXISTING BACKED BENCH
- EXISTING BIKE RACK



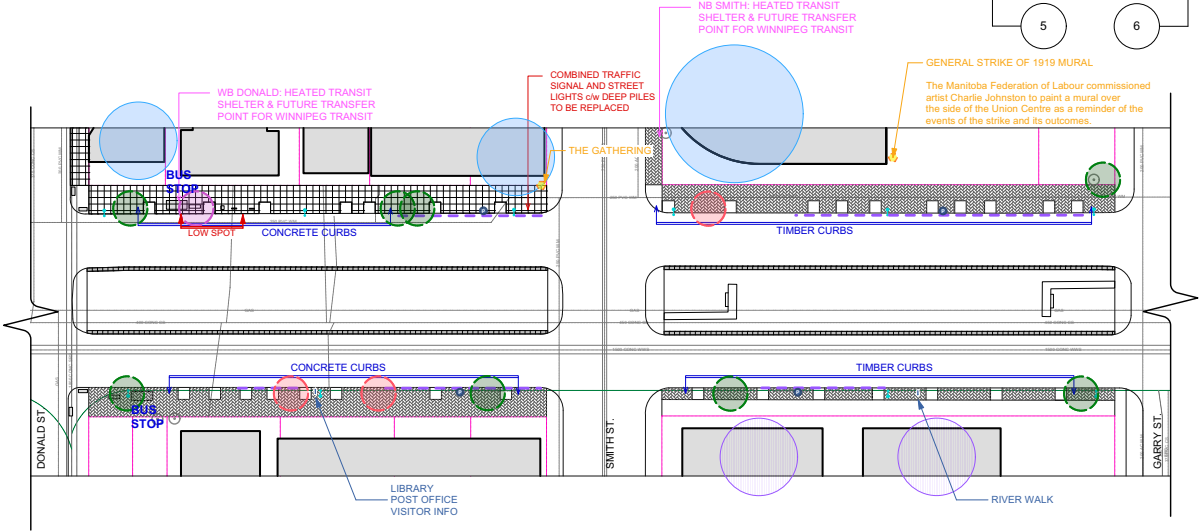
SCALE 1:2500



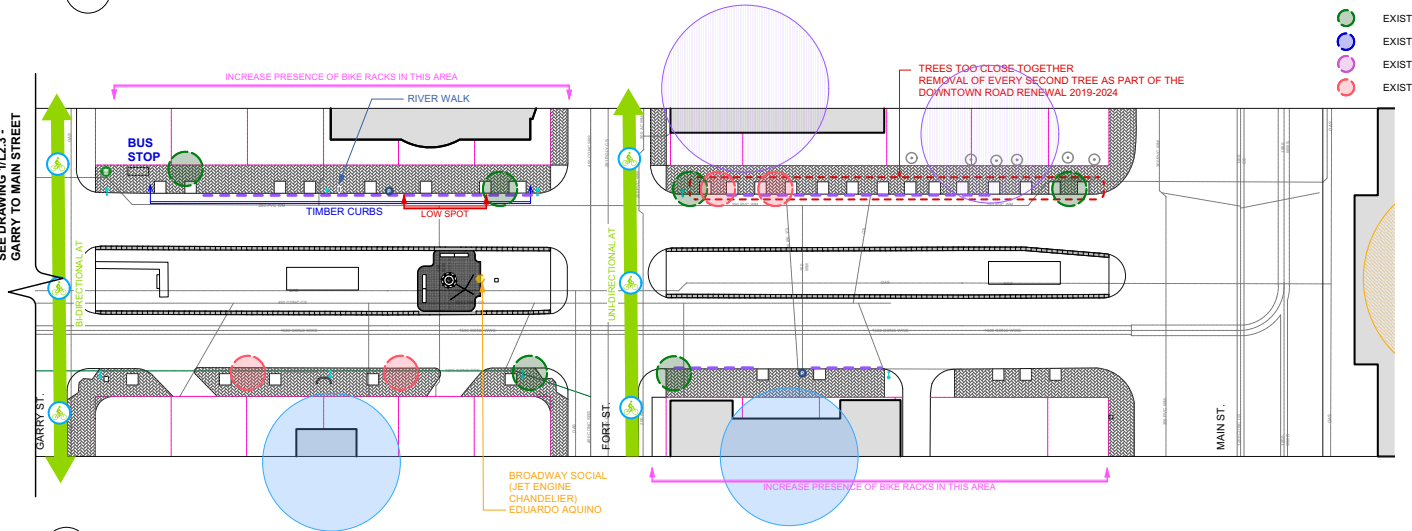
- EXISTING FURNISHINGS:**
- PROPERTY LINES
 - CONCRETE
 - UNISTONE PAVERS
 - CONCRETE BROADWAY PAVERS
 - TINTED CONCRETE MEDIAN PADS
 - FUTURE STREET LIGHTS
 - EXISTING TREE
 - EXISTING PARKING METRES
 - EXISTING PUBLIC ART
 - EXISTING TAKE PRIDE WINNIPEG WASTE RECEPTACLE
 - EXISTING CITY OF WINNIPEG WAYFINDING SIGNAGE DIRECTION
 - EXISTING CIGARETTE BUTT RECEPTACLE
 - ON-STREET 2-HOUR PARKING
- BROADWAY STREETLIFE GENERATORS:**
- LANDMARKS
 - STAY PLACES I.E. HOTELS (EXISTING & FUTURE)
 - RESIDENTIAL (EXISTING & FUTURE)
- BROADWAY STREET FURNISHINGS REMOVAL:**
- EXISTING WASTE RECEPTACLE
 - EXISTING BACKLESS BENCH
 - EXISTING BACKED BENCH
 - EXISTING BIKE RACK



SCALE 1:2500



5 DONALD TO GARRY



6 GARRY TO MAIN STREET

EXISTING FURNISHINGS:

- PROPERTY LINES
- ▨ CONCRETE UNISTONE PAVERS
- ▨ CONCRETE BROADWAY PAVERS
- ▨ TINTED CONCRETE MEDIAN PADS
- ! FUTURE STREET LIGHTS
- ⊙ EXISTING TREE
- P EXISTING PARKING METRES
- ⚡ EXISTING PUBLIC ART
- ♻️ EXISTING TAKE PRIDE WINNIPEG WASTE RECEPTACLE
- ⚡ EXISTING CITY OF WINNIPEG WAYFINDING SIGNAGE DIRECTION
- ⊙ EXISTING CIGARETTE BUTT RECEPTACLE
- - - ON-STREET 2-HOUR PARKING

BROADWAY STREETLIFE GENERATORS:

- ⊙ LANDMARKS
- ⊙ STAY PLACES I.E. HOTELS (EXISTING & FUTURE)
- ⊙ RESIDENTIAL (EXISTING & FUTURE)

BROADWAY STREET FURNISHINGS REMOVAL:

- ⊙ EXISTING WASTE RECEPTACLE
- ⊙ EXISTING BACKLESS BENCH
- ⊙ EXISTING BACKED BENCH
- ⊙ EXISTING BIKE RACK

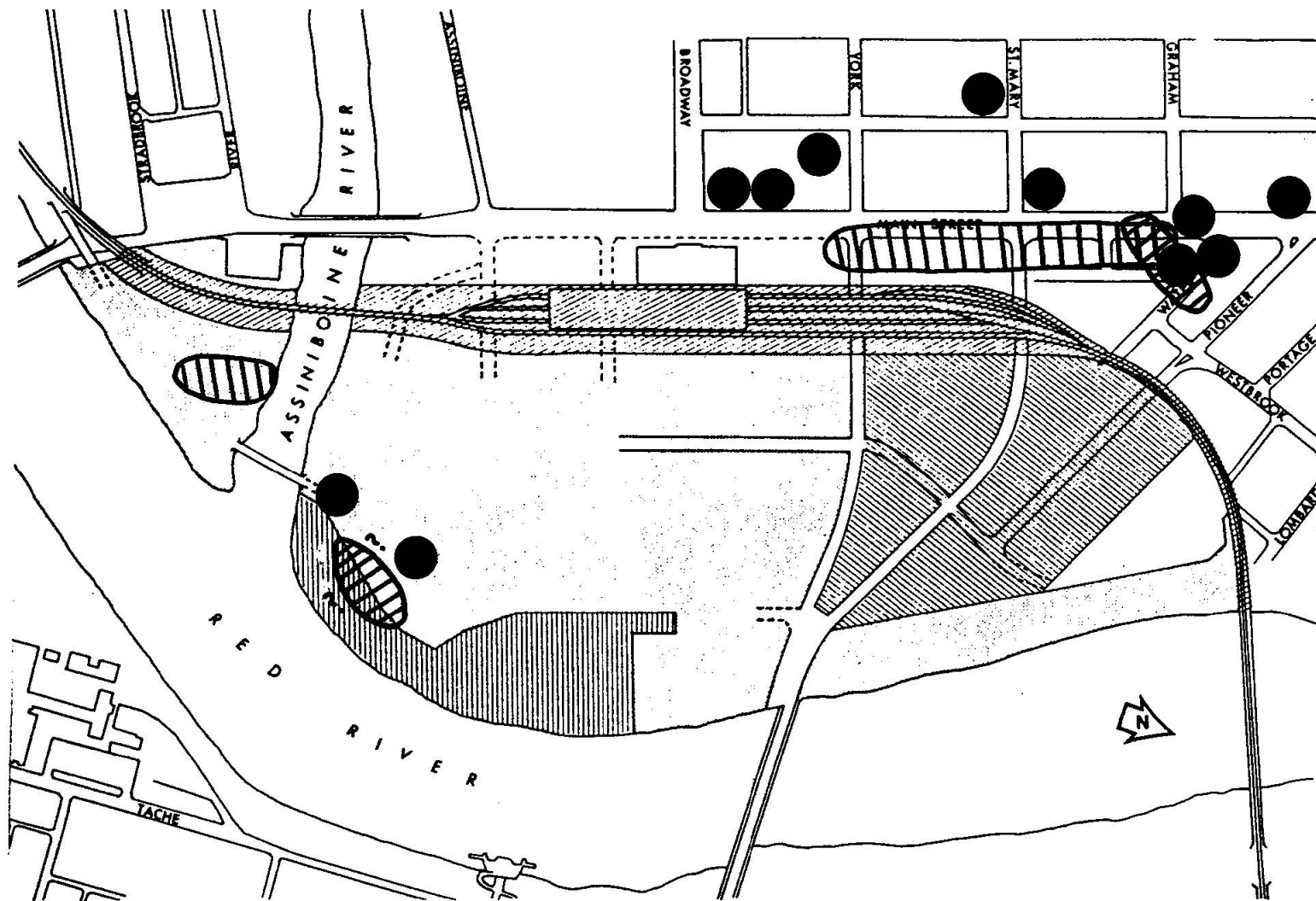
SEE DRAWING 1/L2.3 - GARRY TO MAIN STREET

BIDIRECTIONAL

UNIDIRECTIONAL

Appendix 4: Indigenous burial ground map and sources

- Map showing burial grounds document source: https://www.theforks.com/uploads/bibliographies/1988-Forks_Archaeological_Impact_Assessment_and_Mngm_Plan.pdf
- Assembly of Manitoba Chiefs comments on downtown Winnipeg burial grounds: <https://manitobachiefs.com/assembly-of-manitoba-chiefs-shocked-that-downtown-winnipeg-is-a-first-nations-burial-site/>



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FIGURE 6: RECORDED AND POTENTIAL BURIAL LOCATIONS

- -- Recorded locations of burials
- ▨ -- Potential burial locations