



**THE CITY OF WINNIPEG**  
**PUBLIC WORKS DEPARTMENT**  
**ENGINEERING DIVISION**



**LOCATION PLAN**

**ARLINGTON BRIDGE**  
**PHASE 4 - DECOMMISSIONING**

**TENDER No. 746-2025**



704-INF-MBI03007.01-DWG-S1001



ACCEPTED BY:

BRIDGE PROJECTS ENGINEER

CITY DRAWING NUMBER:

DATE

SHEET 1

DRAWING LIST

SHEET NUMBER	DRAWING DESCRIPTION
1	COVER SHEET
2	DRAWING LIST AND GENERAL NOTES
3	OVERALL SITE PLAN
4	PARTIAL SITE PLAN
5	PARTIAL SITE PLAN
6	PARTIAL SITE PLAN
7	GENERAL ARRANGEMENT - EXISTING
8	SOUTH APPROACH ACCESS PLAN - REMOVAL FROM GRADE OPTION
9	SOUTH APPROACH SPANS 1-12 & 13 - REMOVAL FROM GRADE OPTION
10	SPAN 14 ACCESS PLAN - SPMT AND GANTRY REMOVAL OPTION
11	SPAN 14 - SPMT AND GANTRY REMOVAL OPTION
12	SPAN 15 ACCESS PLAN - SPMT AND GANTRY REMOVAL OPTION
13	SPAN 15 - SPMT AND GANTRY REMOVAL OPTION
14	SPAN 16 ACCESS PLAN - SPMT AND GANTRY REMOVAL OPTION
15	SPAN 16 - SPMT AND GANTRY REMOVAL OPTION
16	SPAN 17 ACCESS PLAN - SPMT AND GANTRY REMOVAL OPTION
17	SPAN 17 - SPMT AND GANTRY REMOVAL OPTION
18	SPAN 18 ACCESS PLAN - SPMT AND GANTRY REMOVAL OPTION
19	SPAN 18 - SPMT AND GANTRY REMOVAL OPTION
20	PIER 1 DEMOLITION
21	PIER 2 DEMOLITION
22	PIER 3 DEMOLITION
23	PIER 4 DEMOLITION
24	PIER 5 DEMOLITION
25	SPAN 19 ACCESS PLAN - SPMT REMOVAL OPTION
26	SPAN 19 - SPMT REMOVAL OPTION
27	SPAN 20 ACCESS PLAN - SPMT REMOVAL OPTION
28	SPAN 20 - SPMT REMOVAL OPTION
29	SPAN 21 ACCESS PLAN - SPMT REMOVAL OPTION
30	SPAN 21 - SPMT REMOVAL OPTION
31	SPAN 19, 20 AND 21 CROSS SECTIONS - SPMT REMOVAL OPTION
32	SPAN 18 ACCESS PLAN - BLAST AND DROP REMOVAL OPTION
33	SPAN 18 - BLAST AND DROP REMOVAL OPTION
34	SPAN 19 ACCESS PLAN - BLAST AND DROP REMOVAL OPTION
35	SPAN 19 - BLAST AND DROP REMOVAL OPTION
36	SPAN 20 ACCESS PLAN - BLAST AND DROP REMOVAL OPTION
37	SPAN 20 - BLAST AND DROP REMOVAL OPTION
38	SPAN 21 ACCESS PLAN - BLAST AND DROP REMOVAL OPTION
39	SPAN 21 - BLAST AND DROP REMOVAL OPTION
40	SPANS 22-27 - CRANE REMOVAL OPTION
41	NORTH APPROACH SPANS 28-37 - REMOVAL FROM GRADE OPTION
42	TYPICAL PROTECTION DETAILS

GENERAL NOTES:

1. THESE DRAWINGS SHALL BE READ IN CONJUNCTION WITH THE CONTRACT SPECIFICATIONS.
2. THE GEOMETRY, LAYOUT, AND ANY DETAILS DEPICTING THE EXISTING STRUCTURE ARE BASED ON EXISTING DRAWINGS AND LIMITED FIELD SURVEY DATA. THE CONTRACTOR SHALL REFER TO RECORD DRAWINGS FOR ADDITIONAL STRUCTURE INFORMATION. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO FIELD VERIFY ALL NECESSARY DIMENSIONS SUCH THAT THE WORK CAN BE CONSTRUCTED AS SHOWN ON THESE DRAWINGS. THE CONTRACTOR SHALL REPORT ANY DISCREPANCIES TO THE CONTRACT ADMINISTRATOR PRIOR TO DECOMMISSIONING.
3. METHODS AND OPTIONS SHOWN ON THESE DRAWINGS ARE CONCEPTUAL AND ARE NOT FINALIZED.
4. DO NOT SCALE OFF DRAWINGS.

DECOMMISSIONING OPTIONS:

OPTION A -

PART A: 2026 WORKS

1. REMOVE SOUTH APPROACH SPANS (1-13) FROM GRADE BY CONVENTIONAL DEMOLITION METHODS.
2. REMOVE SOUTH APPROACH SUBSTRUCTURE UNITS AND ABUTMENT AS SHOWN ON THE DRAWINGS.
3. REMOVE PRATT SPANS (14-18) BY SELF PROPELLED MODULAR TRANSPORTERS (SPMT) GANTRY METHOD.
4. REMOVE PIERS 1-5 AS SHOWN ON THE DRAWINGS.

PART B: 2027 WORKS

1. REMOVE CAMELBACK SPANS (19-21) VIA SELF PROPELLED MODULAR TRANSPORTERS (SPMT) METHOD.
2. REMOVE NORTH SPANS (22-27) BY CRANE POSITIONED ON JARVIS AVENUE.
3. REMOVE NORTH APPROACH SPANS (28-37) FROM GRADE BY CONVENTIONAL DEMOLITION METHODS.
4. REMOVE NORTH APPROACH SUBSTRUCTURE UNITS AND ABUTMENT AS SHOWN ON THE DRAWINGS.

OPTION B -

PART A: 2026 WORKS

1. REMOVE SOUTH APPROACH SPANS (1-13) FROM GRADE BY CONVENTIONAL DEMOLITION METHODS.
2. REMOVE SOUTH APPROACH SUBSTRUCTURE UNITS AND ABUTMENT AS SHOWN ON THE DRAWINGS.
3. REMOVE PRATT SPANS (14-17) BY SELF PROPELLED MODULAR TRANSPORTERS (SPMT) GANTRY METHOD.
4. REMOVE PIERS 1-5 AS SHOWN ON THE DRAWINGS.

PART B: 2027 WORKS

1. REMOVE PRATT TRUSS SPAN 18 BY BLAST AND DROP METHOD.
2. REMOVE CAMELBACK SPANS (19-21) BY BLAST AND DROP METHOD.
3. REMOVE NORTH SPANS (22-27) BY CRANE POSITIONED ON JARVIS AVENUE.
4. REMOVE NORTH APPROACH SPANS (28-37) FROM GRADE BY CONVENTIONAL DEMOLITION METHODS.
5. REMOVE NORTH APPROACH SUBSTRUCTURE UNITS AND ABUTMENT AS SHOWN ON THE DRAWINGS.

DECOMMISSIONING NOTES:

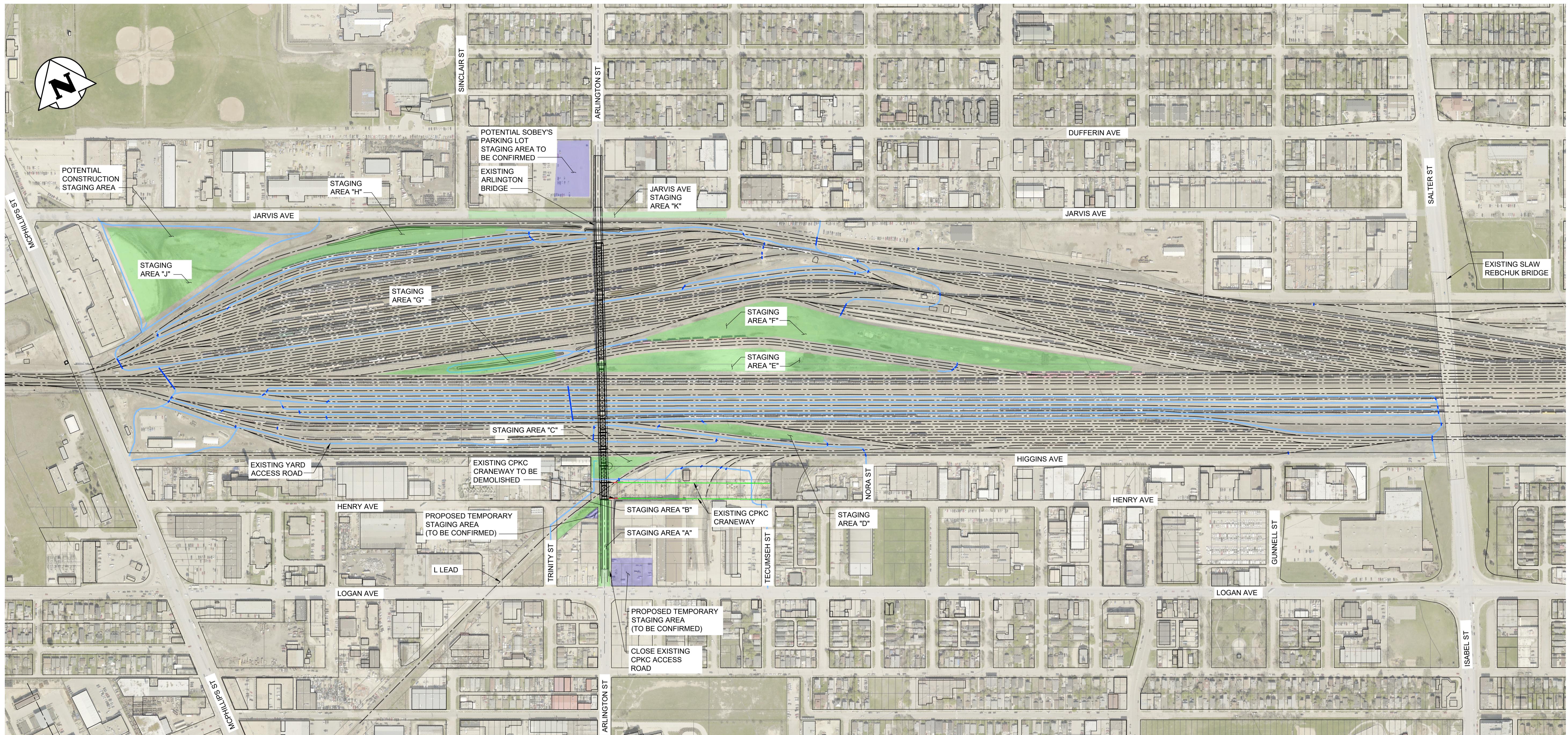
1. SUBSTRUCTURE UNITS TO BE REMOVED SHALL BE REMOVED 1m BELOW GRADE. CRUSHED 20mm DOWN LIMESTONE TO BE PLACED IN EXCAVATION, COMPACTED IN 300mm LIFTS.
2. ALL DECOMMISSIONING MATERIALS TO BE REMOVED OFF SITE AND DISPOSED OF PROPERLY.
3. ARLINGTON RAMP ROADWAY SHALL BE REMOVED AS PER NOTED LIMITS.
4. CPKC WILL PREPARE SPMT PATHWAYS WITHIN THE YARD AND WILL INSTALL CROSSING PLANKS AS SHOWN WITHIN THIS DRAWING SET. LIMITS OF PLANKING/SPMT PATHWAYS TO BE DETERMINED BY CONTRACTOR.
5. CONTRACTOR TO DESIGN SPMT PATHWAYS AND MATERIALS REQUIREMENTS.
6. CONTRACTOR RESPONSIBLE FOR CONFIRMING UNDERGROUND UTILITIES.
7. CPKC WILL PLACE BLAST AND DROP PROTECTION MATERIALS WITHIN THE YARD.
8. CONTRACTOR TO DESIGN BLAST AND DROP PROTECTION SYSTEM. CPKC TO PLACE BALLAST WITHIN YARD.
9. CONTRACTOR RESPONSIBLE FOR ALL PRELIMINARY ENGINEERING TO LIFT AND MOVE TRUSS SPANS INCLUDING ANY REQUIRED STRENGTHENING AND TEMPORARY BRACING.
10. CITY OF WINNIPEG TO REMOVE ALUMINUM RAILING SYSTEM PRIOR TO DECOMMISSIONING.
11. REMAINING PIERS ARE PROPOSED TO BE REMOVED DURING RECONSTRUCTION OF THE REPLACEMENT BRIDGE, BY OTHERS.
12. SIZE OF SUBSTRUCTURE UNITS BELOW GRADE MAY VARY FROM RECORD DRAWINGS.
13. NEEDLES AND OTHER HAZARDS OBSERVED ON BRIDGE AND SHALL BE REMOVED PRIOR TO DECOMMISSIONING FOR SAFETY REASONS.
14. VEHICLES USE OF THE EXISTING BRIDGE LIMITED TO PASSENGER VEHICLES AND QUARTER TON TRUCKS. LARGE VEHICLES OR EQUIPMENT WILL NEED TO BE REVIEWED BY THE CONTRACT ADMINISTRATOR FOR ACCEPTANCE.
15. LIMITED ORIGINAL DRAWINGS OF THE EXISTING STRUCTURE AND PREVIOUS REPAIRS WILL BE MADE AVAILABLE TO THE CONTRACTOR.
16. LOAD RATING ANALYSIS FINITE ELEMENT MODEL WILL BE MADE AVAILABLE TO THE CONTRACTOR. USE OF LOAD RATING MODEL AT CONTRACTOR'S RISK.
17. RECENT INSPECTION REPORTS AND OTHER IMPORTANT DOCUMENTS WILL BE MADE AVAILABLE TO THE CONTRACTOR.
18. STATED MASS FOR EACH SPAN INCLUDES THE CONCRETE DECK AND SIDEWALK.
19. MASS STATED IS APPROXIMATE.
20. CONTRACTOR LOAD RATING ANALYSIS TO BE IN ACCORDANCE TO CSA CHBDC 2025.
21. TRUSS ASSESSMENT FOR SPMT MOVE TO BE IN ACCORDANCE WITH FEDERAL HIGHWAY ADMINISTRATION MANUAL TO USE OF SELF PROPELLED MODULAR TRANSPORTERS (SPMT) TO REMOVE AND REPLACE BRIDGES JUNE 2007.
22. SPMT UNITS AND CONFIGURATIONS SHOWN ARE CONCEPTUAL. CONTRACTOR TO DEVELOP SPMT CONCEPT AND LIFTING APPARATUS CAPABLE OF SAFELY LIFTING AND MOVING THE TRUSS SPANS.

ISSUED FOR  
INFORMATION  
ONLY  
NOT TO BE  
USED FOR  
CONSTRUCTION

 **TETRA TECH**  
ENGINEERS  
GEOSCIENTISTS  
MANITOBA  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

B.M. ELEV.		 <b>TETRA TECH</b>		PRELIMINARY DRAWING	
DESIGNED BY	ML	REVIEWED BY	MJB	NOT TO BE USED FOR CONSTRUCTION	CONSULTANT DRAWING NO.
DRAWN BY	EV	APPROVED BY	MJB		704-INF.MB10304301-S0002
SCALE:	AS NOTED	ACCEPTED BY	DATE		
0 ISSUED FOR TENDER	25.12.01	MJB			
NO. REVISIONS	DATE	BY	DATE	2025.12.01	

THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT		CITY DRAWING NUMBER
ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING		SHEET 2 OF 42
DRAWING LIST AND GENERAL NOTES		2



LEGEND:

- EXISTING YARD ACCESS ROADS
- EXISTING AT GRADE CROSSING
- CONSTRUCTION STAGING AREA
- POTENTIAL STAGING AREA TO BE CONFIRMED WITH PROPERTY OWNER

NOTES:

- WORK AREAS AND WORK PLAN FOR EACH ITEM IS DESCRIBED WITHIN THIS DRAWING SET

ISSUED FOR  
INFORMATION  
ONLY  
NOT TO BE  
USED FOR  
CONSTRUCTION

**ENGINEERS  
GEOSCIENTISTS**  
MANITOBA  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

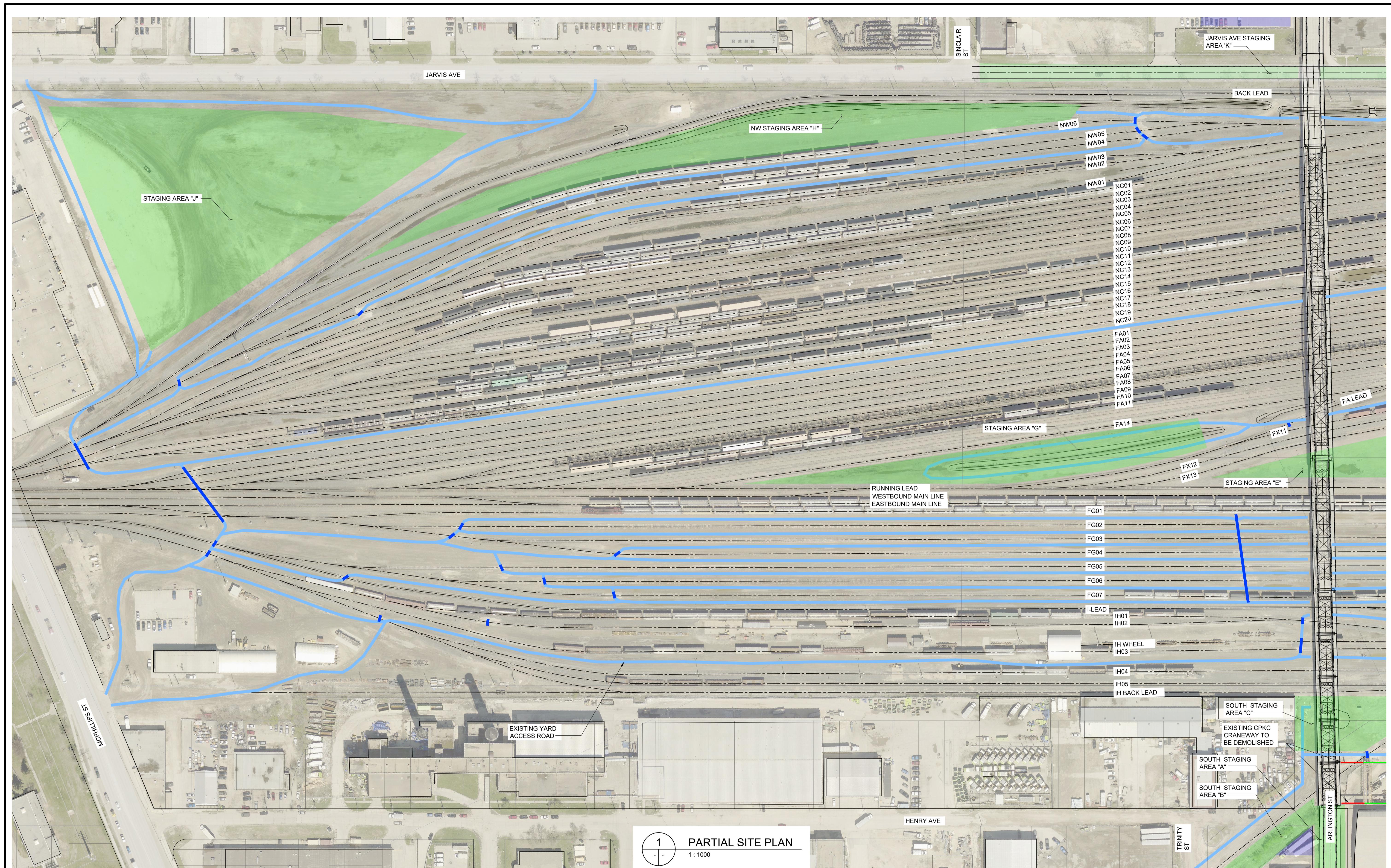
1  
1 : 3000  
OVERALL SITE PLAN

B.M. ELEV.		TETRA TECH	
DESIGNED BY	ML	REVIEWED BY	MJB
DRAWN BY	EV	APPROVED BY	MJB
SCALE:	AS NOTED	ACCEPTED BY	DATE
0 ISSUED FOR TENDER	25.12.01	MJB	
NO. REVISIONS	DATE	BY	DATE 2025.12.01

PRELIMINARY  
DRAWING  
NOT TO BE  
USED FOR  
CONSTRUCTION  
CONSULTANT DRAWING NO.  
704-INF.MB10304301-S0003

**Winnipeg**  
THE CITY OF WINNIPEG  
PUBLIC WORKS DEPARTMENT  
ARLINGTON BRIDGE  
PHASE 4 - DECOMMISSIONING  
OVERALL SITE PLAN

CITY DRAWING NUMBER  
SHEET 3 OF 42  
3



B.M.  
ELEV.



**TETRA TECH**

**PRELIMINARY DRAWING**

NOT TO BE USED FOR CONSTRUCTION

CONSULTANT DRAWING NO.

704-INF.MB10304301-S0004



**THE CITY OF WINNIPEG**  
PUBLIC WORKS DEPARTMENT

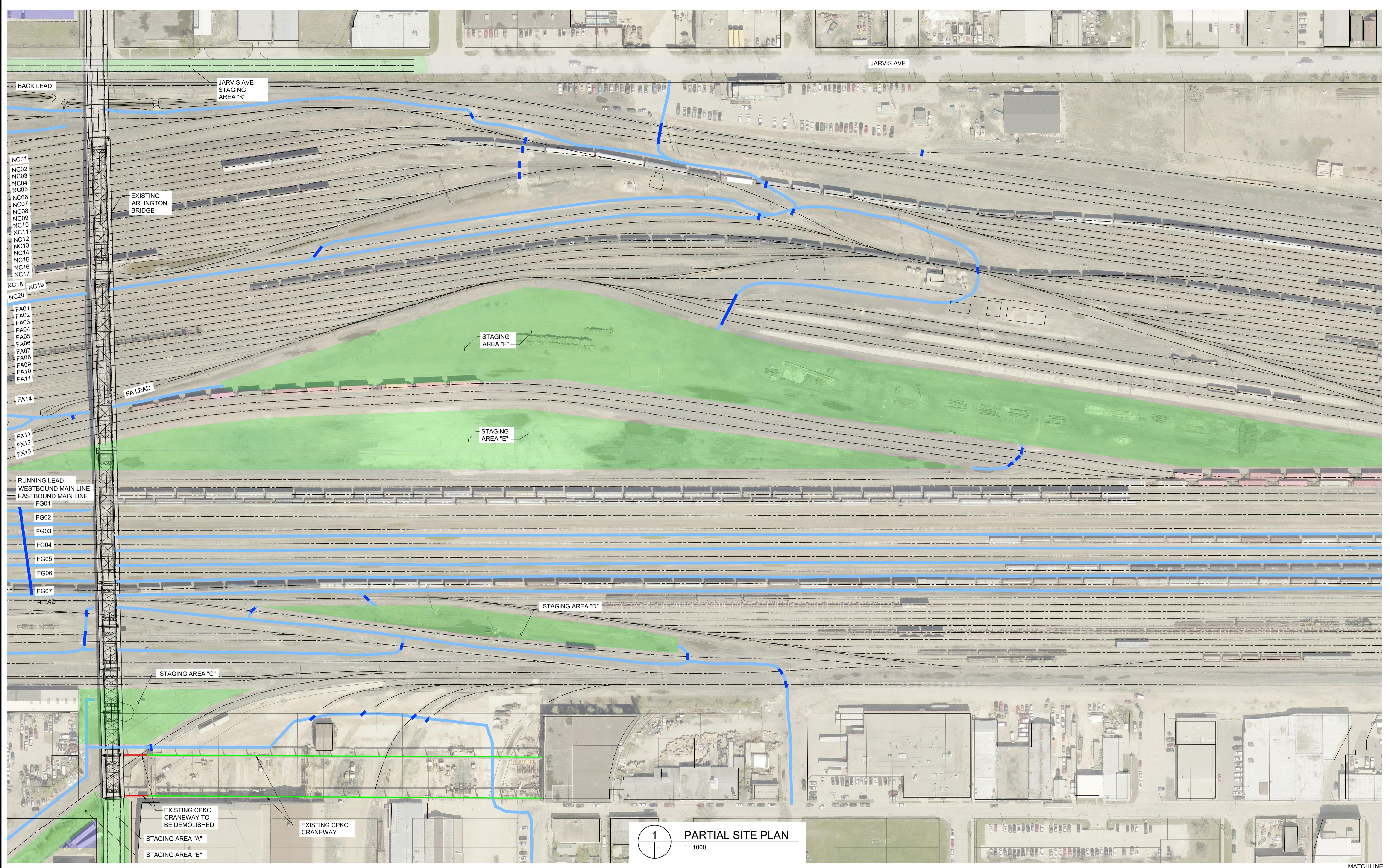
ARLINGTON BRIDGE  
PHASE 4 - DECOMMISSIONING

CITY DRAWING NUMBER

4 OF 42

PARTIAL SITE PLAN

4



LEGEND:

- EXISTING YARD ACCESS ROADS
- EXISTING AT GRADE CROSSING
- CONSTRUCTION STAGING AREA
- POTENTIAL STAGING AREA TO BE CONFIRMED WITH PROPERTY OWNER

ISSUED FOR INFORMATION ONLY  
NOT TO BE USED FOR CONSTRUCTION

**ENGINEERS GEOSCIENTISTS MANITOBA**  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

B.M. ELEV.	
0 ISSUED FOR TENDER	25.12.01 MJB
NO. REVISIONS	DATE BY DATE 2025.12.01

**TETRA TECH**

PRELIMINARY DRAWING  
NOT TO BE USED FOR CONSTRUCTION  
CONSULTANT DRAWING NO.  
704-INF.MB10304301-S0005

**Winnipeg**

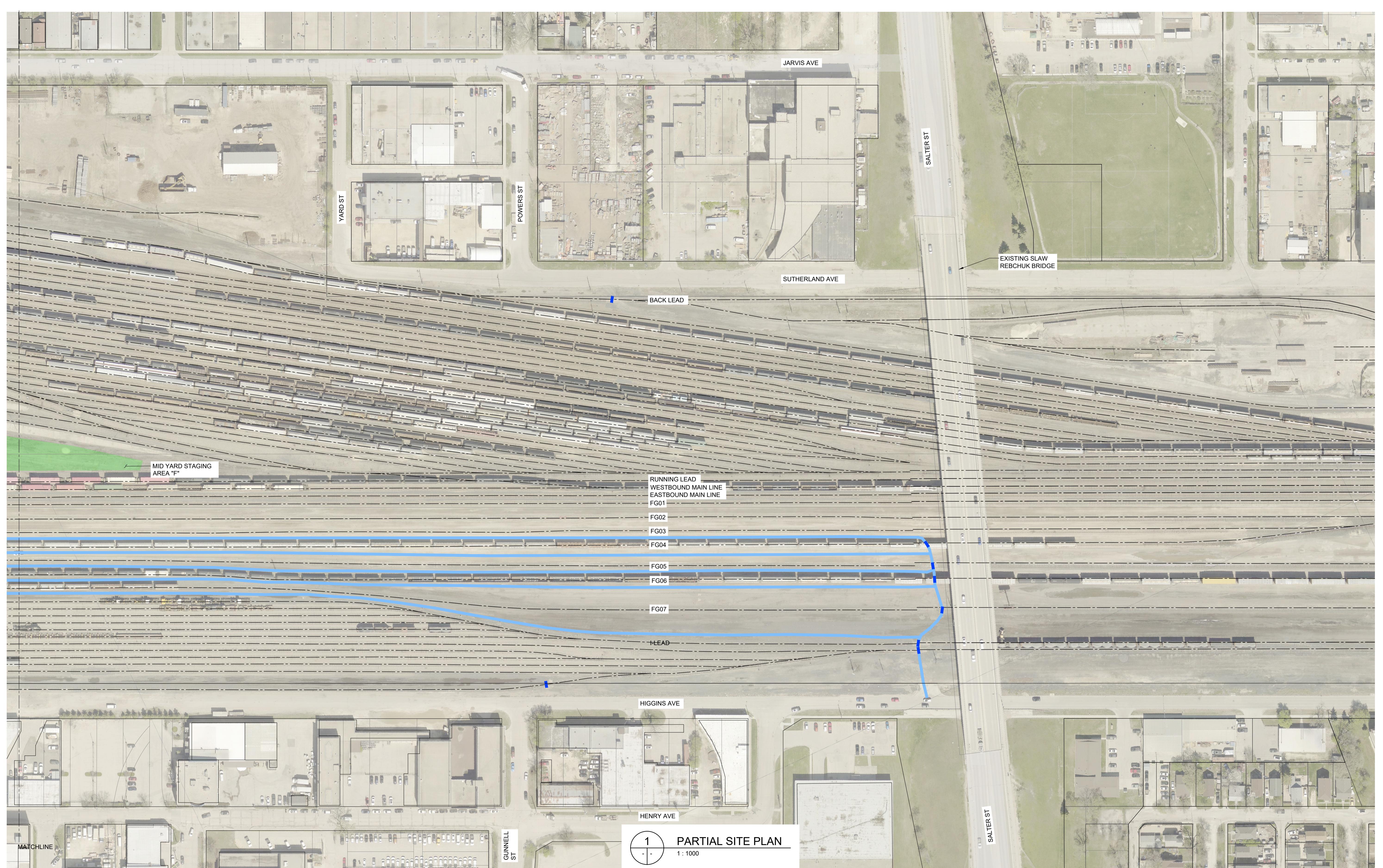
**THE CITY OF WINNIPEG**  
PUBLIC WORKS DEPARTMENT

ARLINGTON BRIDGE  
PHASE 4 - DECOMMISSIONING

CITY DRAWING NUMBER  
SHEET 5 OF 42

PARTIAL SITE PLAN

5



## LEGEND

- EXISTING YARD ACCESS ROADS
- EXISTING AT GRADE CROSSING
- CONSTRUCTION STAGING AREA

ISSUED FOR  
**INFORMATION  
ONLY**

NOT TO BE  
USED FOR  
CONSTRUCTION



# PARTIAL SITE PLAN

B.M. ELEV.			
0	ISSUED FOR TENDER	25.12.01	MJB
NO.	REVISIONS	DATE	BY



**TETRA TECH**

# PRELIMINARY DRAWING

The logo for Winnipeg, featuring the word "Winnipeg" in a bold, sans-serif font. Above the letter "i", there is a stylized graphic element consisting of a circle with a dot inside, connected by a curved line to a horizontal line that extends to the right.

# THE CITY OF WINNIPEG

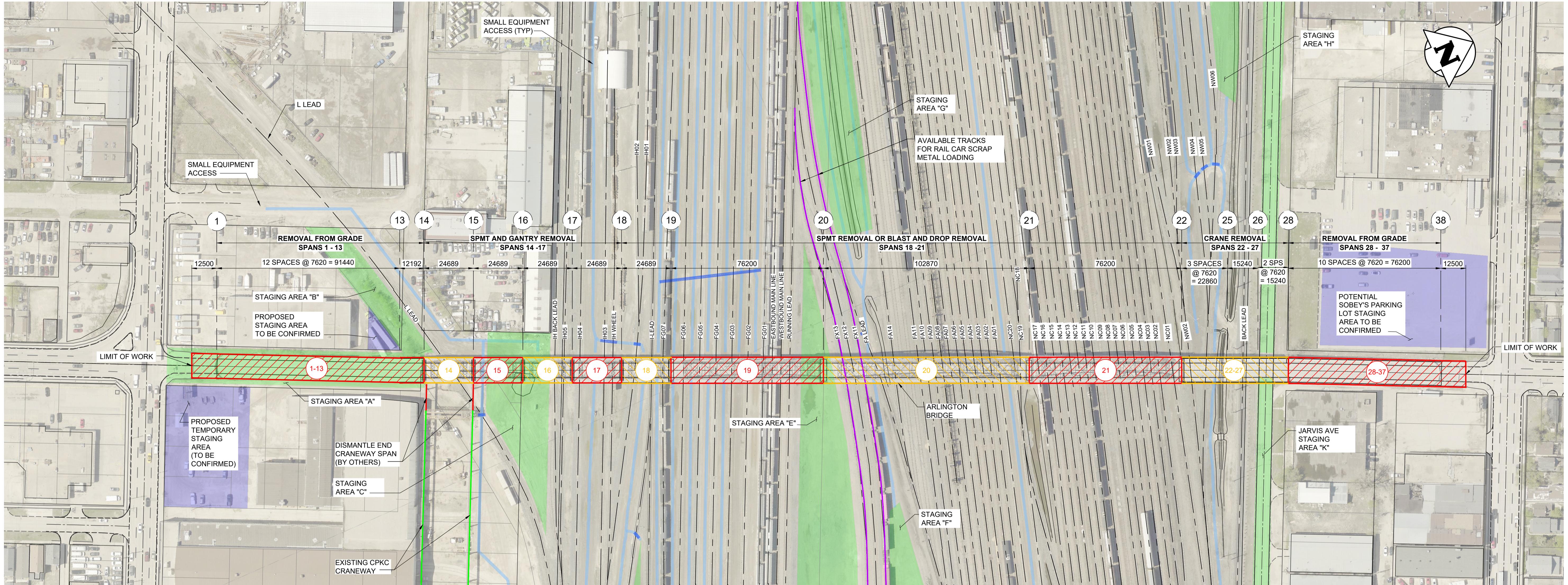
## PUBLIC WORKS DEPARTMENT

## ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING

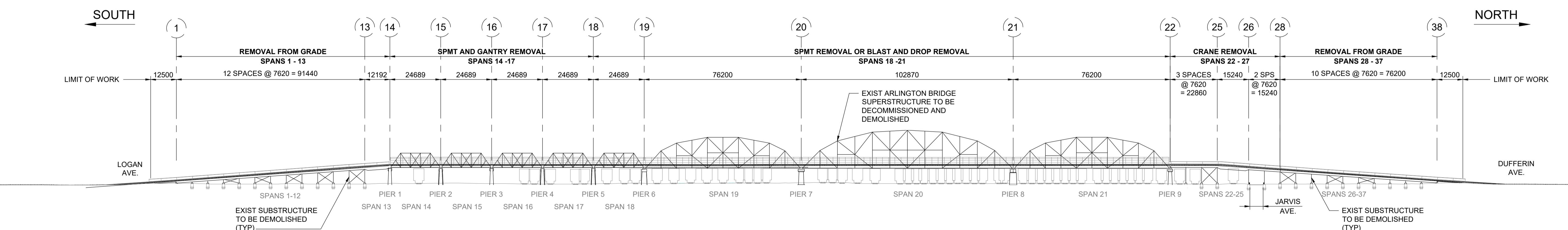
**DRAWING NUMBER**

## PARTIAL SITE PLAN

1



1 EXISTING BRIDGE - PLAN  
1 : 1000



NOTES:

1. REFER TO INDIVIDUAL SPAN ACCESS PLAN DRAWINGS FOR ANY TEMPORARY CROSSINGS REQUIRED PRIOR TO SPAN REMOVALS.
2. PROPOSED TRACK PROTECTION DETAIL, SHOWN ON DRAWING 42.
3. CONTRACTOR TO PREPARE AND SUBMIT DETAILED DECOMMISSIONING PLANS FOR EACH TRUSS, INCLUDING DETAILED SCHEDULE AND REQUIRED PREPARATION WORKS.
4. ALL SPAN REMOVALS AND ACTIVITIES REQUIRED WITHIN THE CPKC WINNIPEG YARD IS SUBJECT TO CPKC RAILWAY REVIEW AND ACCEPTANCE PRIOR TO WORKS COMPLETED.
5. CPKC CRANEWAY TO BE DISMANTLED (BY OTHERS).
6. CONTRACTOR REMOVAL PLANS TO INCLUDE: EQUIPMENT, ACCESS, NUMBER OF WORKERS, PREPARATION ACTIVITIES, LAYDOWN AREAS, AND ALL ACTIVITIES NECESSARY TO COMPLETE THE SPAN REMOVAL.
7. EXISTING BRIDGE TRAFFIC RAILING SYSTEM TO BE REMOVED BY OTHERS.

LEGEND:

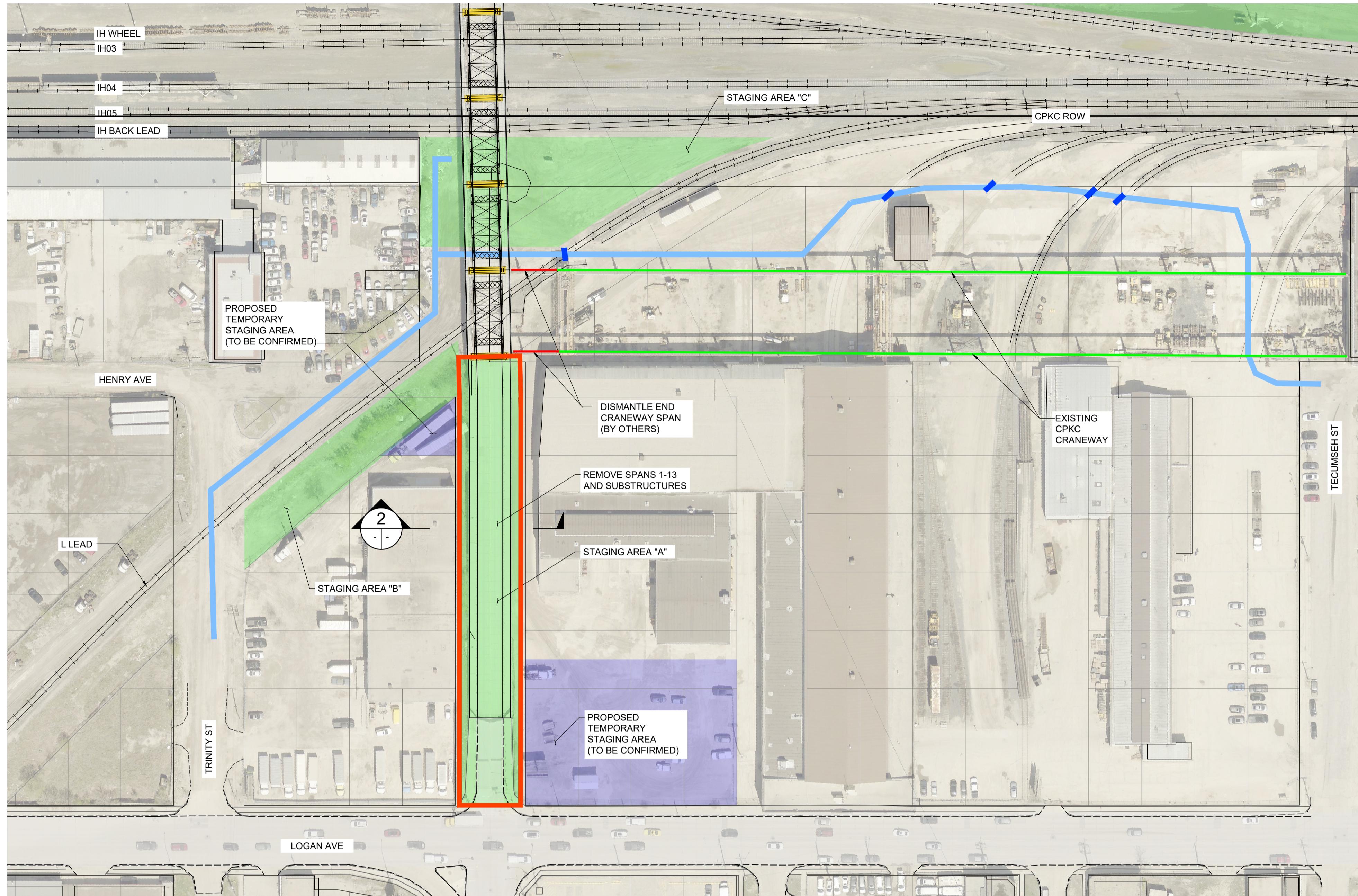
- EXISTING YARD ACCESS ROADS
- EXISTING AT GRADE CROSSING
- POTENTIAL CONSTRUCTION STAGING AREA
- POTENTIAL STAGING AREA TO BE CONFIRMED WITH PROPERTY OWNER

ISSUED FOR  
INFORMATION  
ONLY  
NOT TO BE  
USED FOR  
CONSTRUCTION

**ENGINEERS  
GEOSCIENTISTS  
MANITOBA**  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

2 EXISTING BRIDGE - EAST ELEVATION  
1 : 1000

B.M. ELEV.		TETRA TECH		PRELIMINARY DRAWING		THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT	
DESIGNED BY	ML	REVIEWED BY	MJB				
DRAWN BY	EV	APPROVED BY	MJB				
SCALE:	AS NOTED	ACCEPTED BY	DATE				
0 ISSUED FOR TENDER	25.12.01	MJB		NOT TO BE USED FOR CONSTRUCTION		ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	CITY DRAWING NUMBER
NO. REVISIONS	DATE	BY	DATE	2025.12.01			
				CONSULTANT DRAWING NO.	704-INF.MB0304301-S0007	GENERAL ARRANGEMENT EXISTING	SHEET 7 OF 42
							7



#### SEQUENCE OF WORK

##### PRE-WORKBLOCK DEMOLITION WORKS:

- L LEAD REMAINS IN SERVICE, PROTECT THE SOUTH SIDE OF THE TRACK WITH DELINEATORS
- END SPAN OF CPKC CRANEWAY DISMANTLED BY OTHERS
- OBTAIN APPROVAL FOR TEMPORARY STAGING AREA ON CPKC PROPERTY
- INSTALL UTILITY PROTECTION AS REQUIRED

##### DURING WORKBLOCK DEMOLITION WORKS:

- SOUTH APPROACH - SPANS 1-13

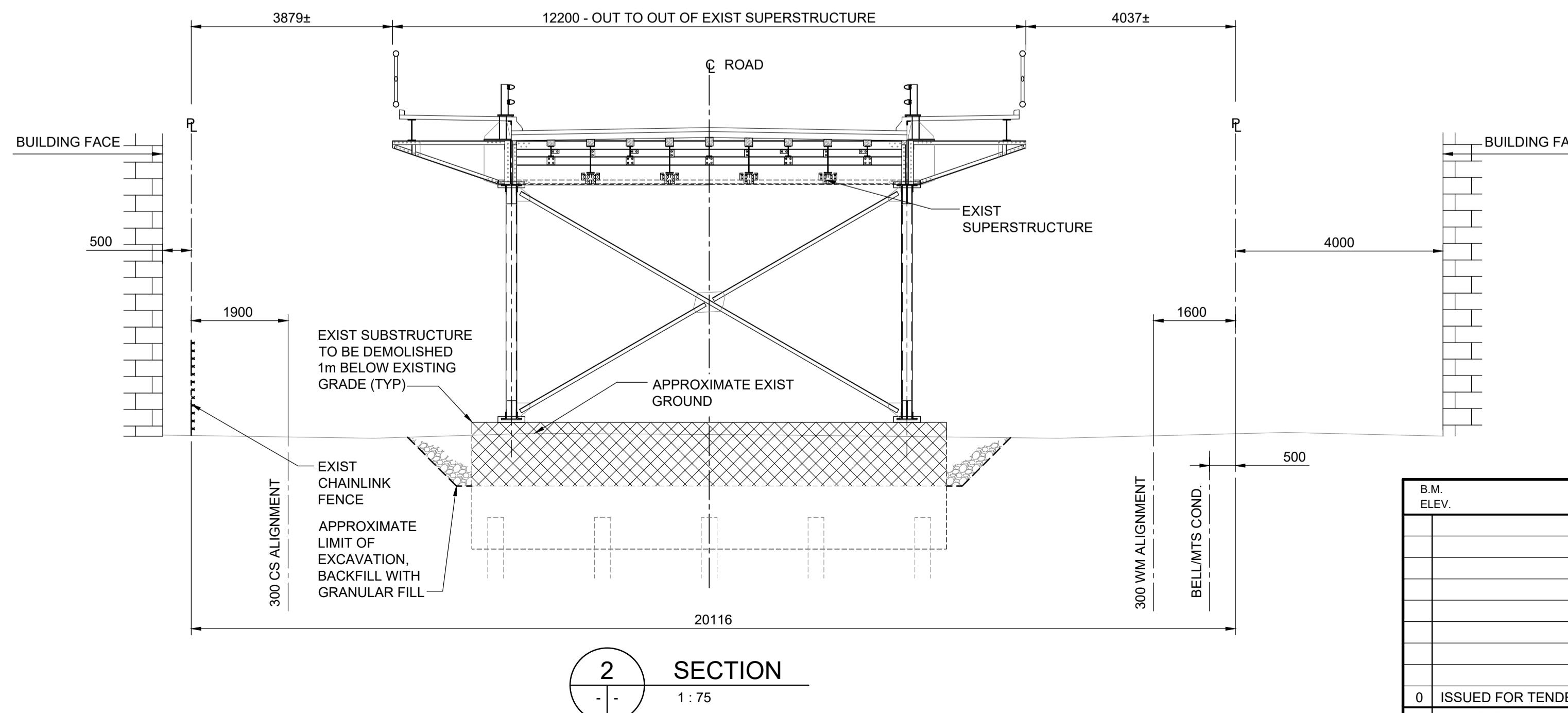
##### POST-WORKBLOCK DEMOLITION WORKS:

- CLEAN AND RESTORE GROUND SURFACE TO PRE-DEMOLITION STATE AS REQUIRED

#### LEGEND:

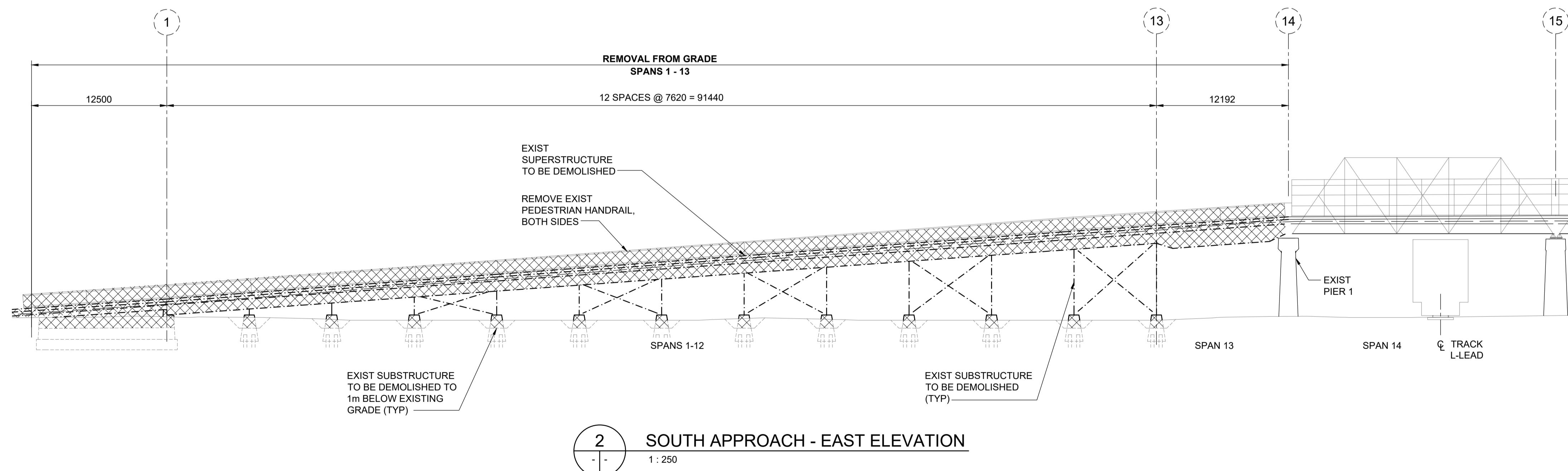
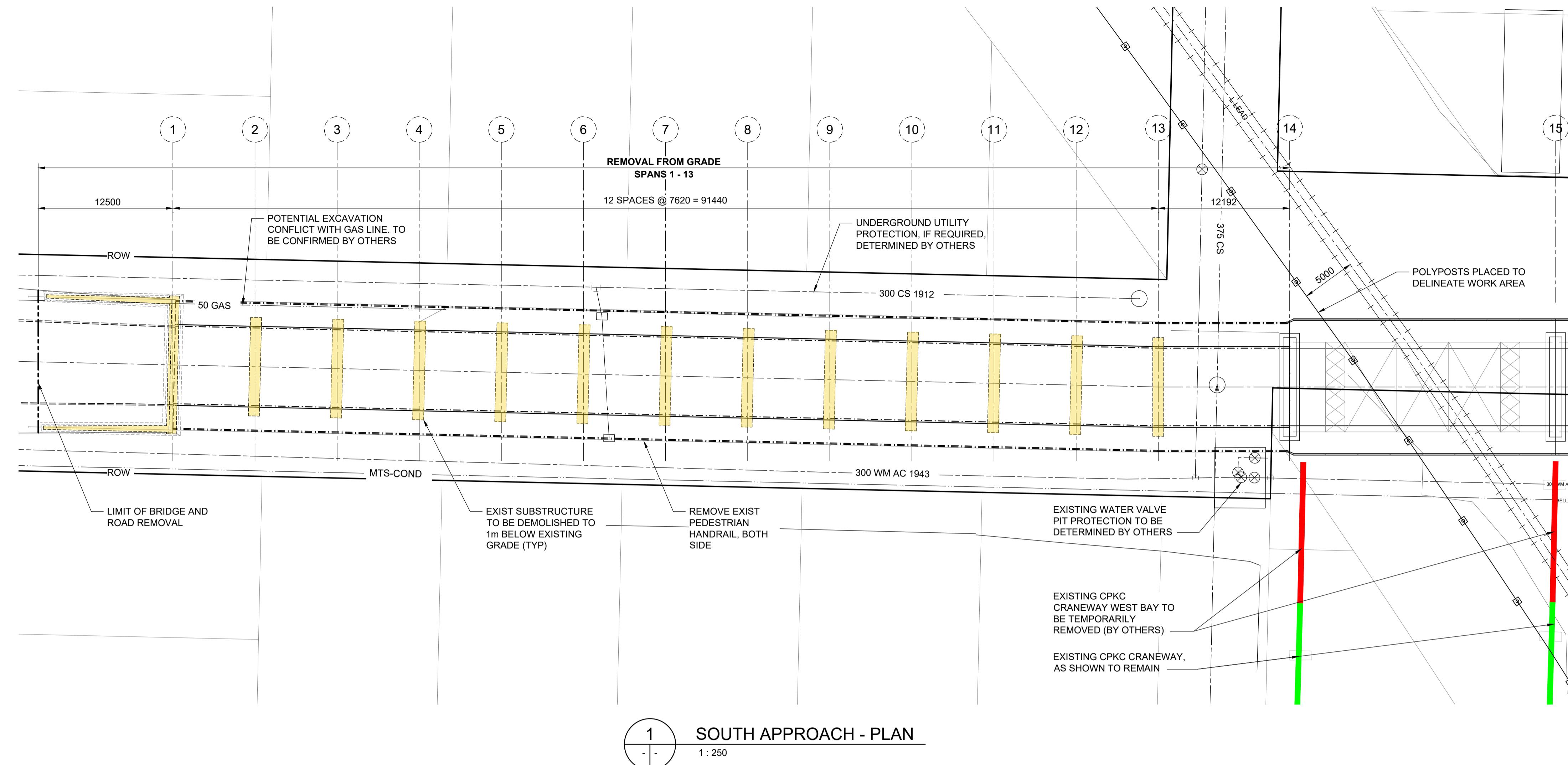
- EXISTING PIER LOCATIONS
- EXISTING YARD ACCESS ROADS
- EXISTING AT GRADE CROSSING
- CONSTRUCTION STAGING AREA
- TEMPORARY STAGING AREA
- OVERALL WORK AREA

1 SOUTH APPROACH - ACCESS PLAN  
1:750



2 SECTION  
1:75

B.M. ELEV.		TETRA TECH		PRELIMINARY DRAWING NOT TO BE USED FOR CONSTRUCTION		THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT	
DESIGNED BY	ML	REVIEWED BY	MJB	DRAWN BY	EV	APPROVED BY	MJB
SCALE:	AS NOTED	ACCEPTED BY	DATE	0 ISSUED FOR TENDER	25.12.01	MJB	CONSULTANT DRAWING NO. 704-INF.MB10304301-S0008
NO. REVISIONS	DATE	BY	DATE	2025.12.01			
ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING				CITY DRAWING NUMBER SHEET 8 OF 42			
SOUTH APPROACH ACCESS PLAN REMOVAL FROM GRADE OPTION				8			



ISSUED FOR INFORMATION ONLY  
NOT TO BE USED FOR CONSTRUCTION

**ENGINEERS GEOSCIENTISTS MANITOBA**  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

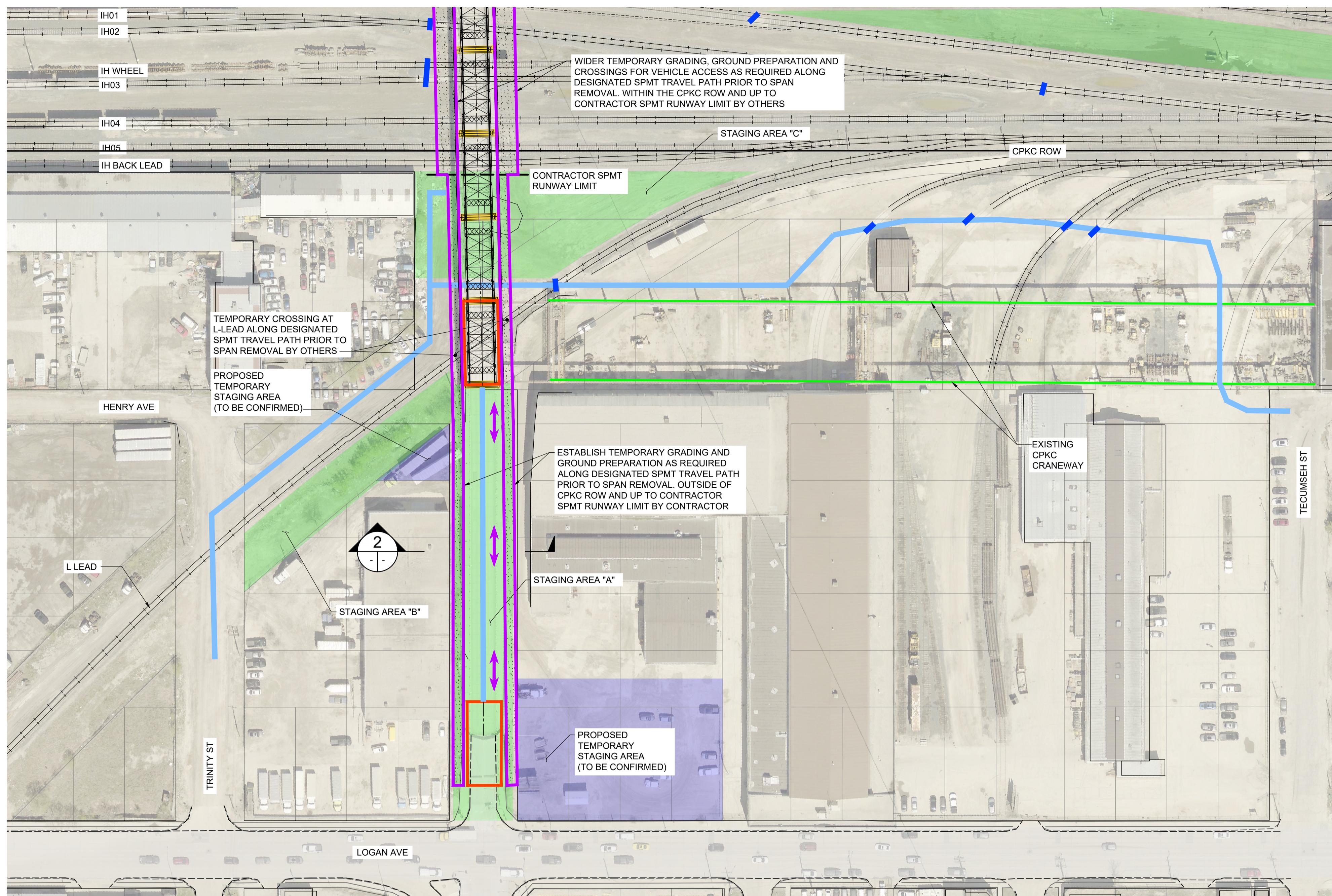
B.M. ELEV.		TETRA TECH	
DESIGNED BY	ML	REVIEWED BY	MJB
DRAWN BY	EV	APPROVED BY	MJB
SCALE:	AS NOTED	ACCEPTED BY	DATE
0 ISSUED FOR TENDER	25.12.01	MJB	
NO. REVISIONS	DATE	BY	DATE 2025.12.01

PRELIMINARY DRAWING  
NOT TO BE USED FOR CONSTRUCTION

CONSULTANT DRAWING NO.  
704-INF.MBI0304301-S0009

**THE CITY OF WINNIPEG**  
PUBLIC WORKS DEPARTMENT

ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	CITY DRAWING NUMBER
SHEET 9 OF 42	
SOUTH APPROACH SPANS 1-12 & 13 REMOVAL FROM GRADE OPTION	
9	



#### SEQUENCE OF WORK

##### PRE-DEMOLITION WORKS:

- INSTALL TEMPORARY GRADING AND CROSSINGS FROM LOGAN AVENUE TO PIER 6 ALONG DESIGNATED SPMT TRAVEL
- PERFORM LOCALIZED TRUSS MODIFICATIONS/REMOVALS TO FACILITATE TRUSS REMOVAL
- ERECT AND SETUP SELF-PROPELLED MODULAR TRANSPORTS (SPMT) AND GANTRY CRANES
- PRIOR TO SPAN REMOVAL WORKBLOCK, ALL TRAINS SHALL BE REMOVED WITHIN SPMT TRAVEL PATH

##### TRUSS REMOVAL WORKS:

- ENSURE L-LEAD IS REMOVED FROM SERVICE
- POSITION SPMT TO SPAN 14
- LIFT AND MOVE SPAN FROM BRIDGE LOCATION TO STAGING AREA "A"
- LOWER SPAN ONTO GROUND SURFACE
- RETURN SERVICE TO THE L-LEAD TRACK ONCE CLEAR OF RIGHT OF WAY

##### DEMOLITION WORKS:

- DISMANTLE/DEMOLISH TRUSS COMPONENTS ON GROUND, WHILE SIMULTANEOUSLY HAULING THEM OFF SITE
- HAULING EQUIPMENT TO REMOVE TRUSS COMPONENTS FROM STAGING AREAS "A" VIA LOGAN AVE

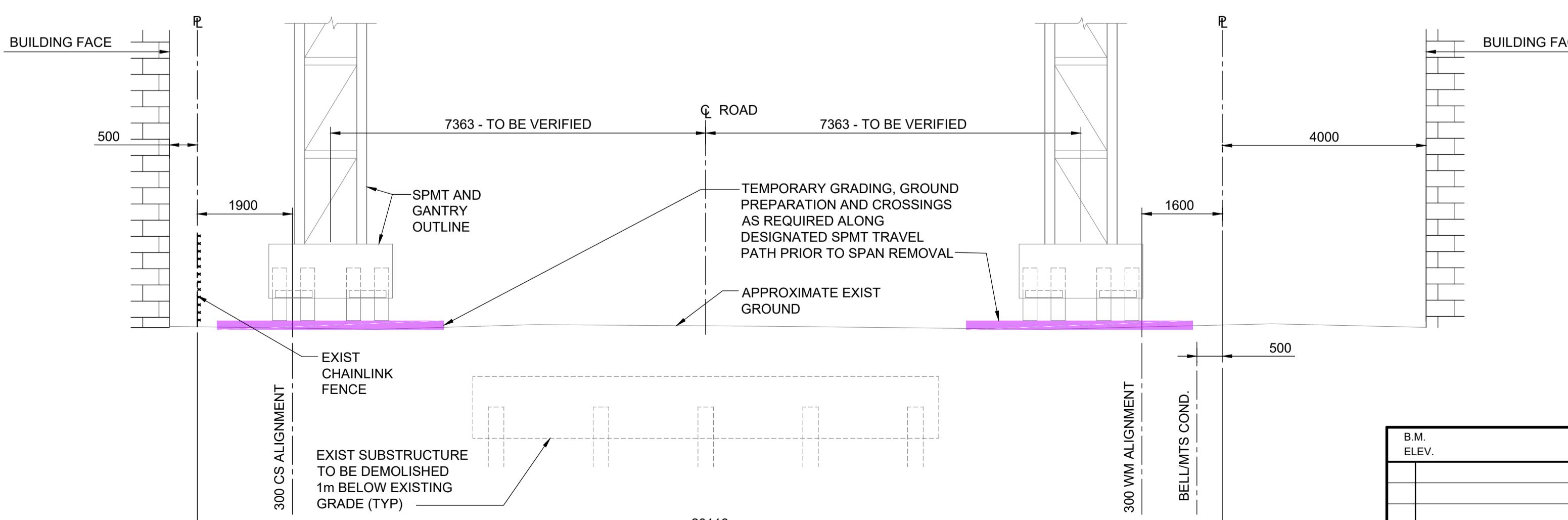
##### POST-DEMOLITION WORKS:

- CLEAN AND RESTORE GROUND SURFACE TO PRE-DEMOLITION STATE IF/AS REQUIRED

#### LEGEND:

- EXISTING PIER LOCATIONS
- ACCESS ROUTE FOR SPAN 14
- EXISTING AT GRADE CROSSING
- CONSTRUCTION STAGING AREA
- OVERALL WORK AREA
- SPMT TRAVEL PATH

1 SPAN 14 ACCESS PLAN  
1 : 750



2 SECTION  
1 : 75

B.M. ELEV.		TETRA TECH		PRELIMINARY DRAWING		THE CITY OF WINNIPEG	
DESIGNED BY	ML	REVIEWED BY	MJB	NOT TO BE USED FOR CONSTRUCTION	PUBLIC WORKS DEPARTMENT		
DRAWN BY	EV	APPROVED BY	MJB				ARLINGTON BRIDGE
SCALE:	AS NOTED	ACCEPTED BY	DATE				PHASE 4 - DECOMMISSIONING
0 ISSUED FOR TENDER	25.12.01	MJB					CITY DRAWING NUMBER
NO. REVISIONS	DATE	BY	DATE	2025.12.01			SHEET 10 OF 42
SPAN 14 ACCESS PLAN SPMT AND GANTRY REMOVAL OPTION							

ISSUED FOR  
INFORMATION  
ONLY  
NOT TO BE  
USED FOR  
CONSTRUCTION

ENGINEERS  
GEOSCIENTISTS  
MANITOBA  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499



TETRA TECH



Winnipeg

THE CITY OF WINNIPEG

PUBLIC WORKS DEPARTMENT

ARLINGTON BRIDGE

PHASE 4 - DECOMMISSIONING

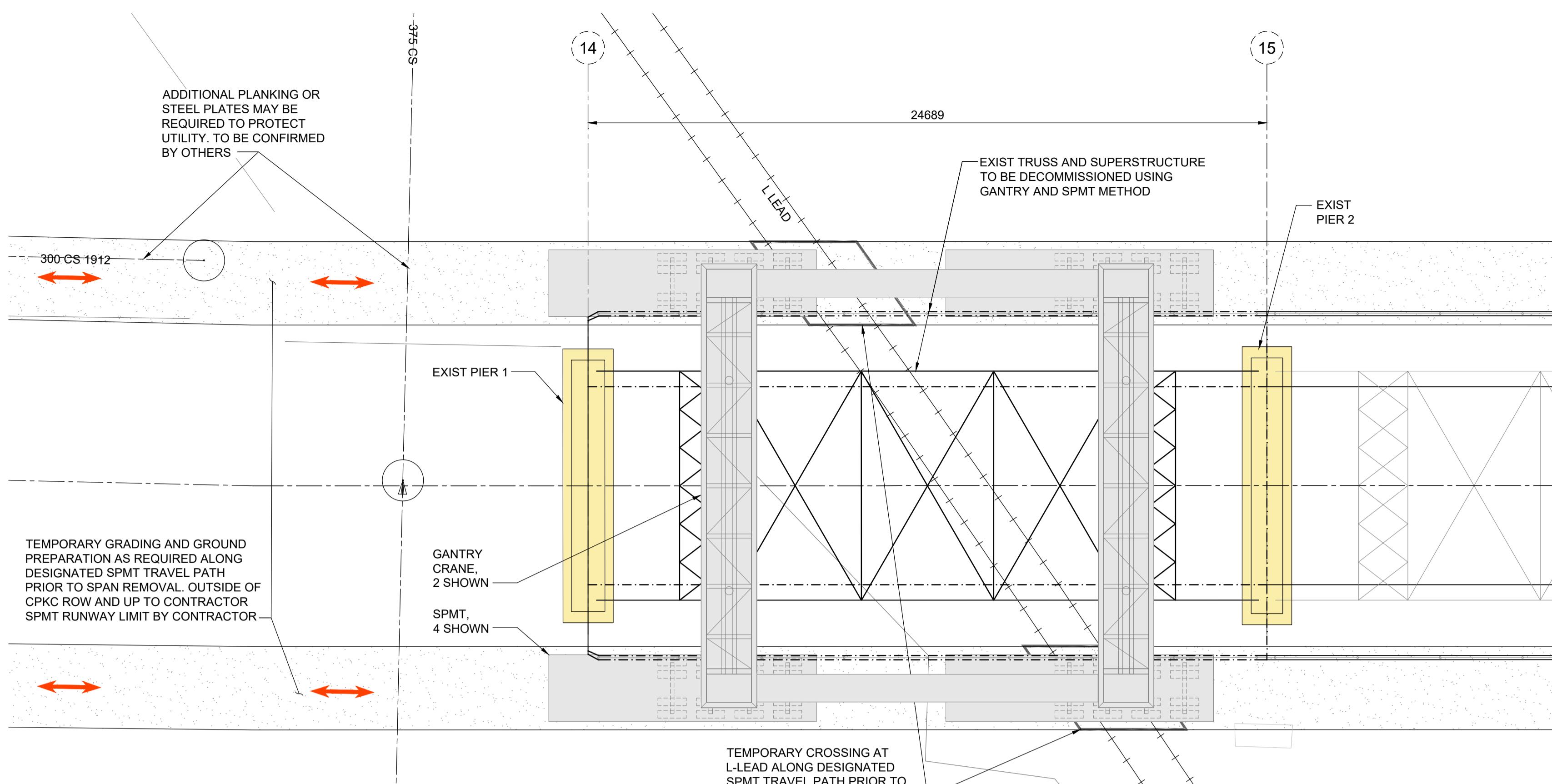
CITY DRAWING NUMBER

SHEET 10 OF 42

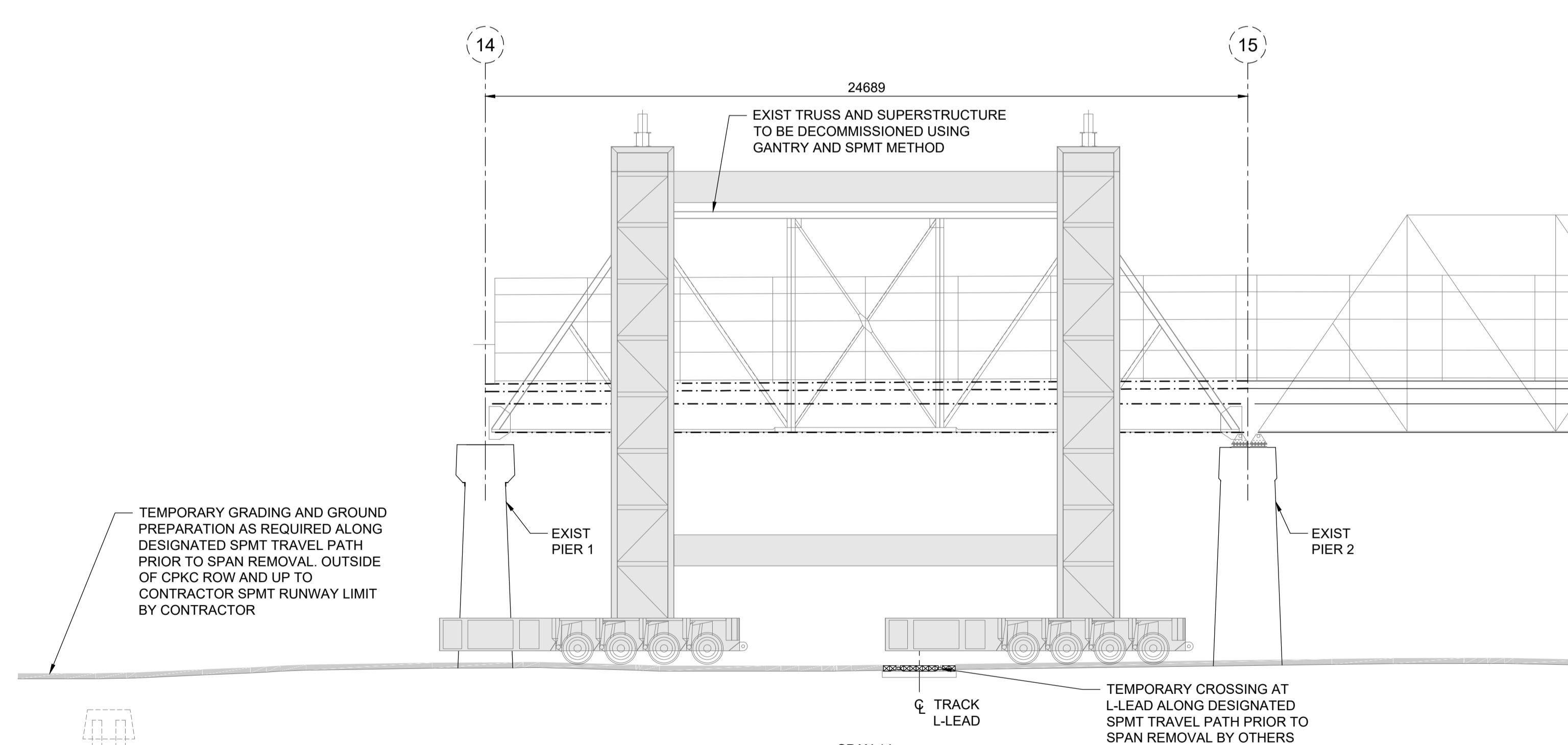
SPAN 14 ACCESS PLAN

SPMT AND GANTRY REMOVAL OPTION

10



1 SPAN 14 - TRUSS PLAN  
1:125



2 SPAN 14 - EAST ELEVATION  
1:125

PRATT TRUSS ESTIMATED MASS: 200,000 kg

ISSUED FOR INFORMATION ONLY  
NOT TO BE USED FOR CONSTRUCTION

**ENGINEERS  
GEOSCIENTISTS**  
MANITOBA  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

B.M. ELEV.		TETRA TECH	
DESIGNED BY	ML	REVIEWED BY	MJB
DRAWN BY	EV	APPROVED BY	MJB
SCALE:	AS NOTED	ACCEPTED BY	DATE
0 ISSUED FOR TENDER	25.12.01	MJB	
NO. REVISIONS	DATE	BY	DATE 2025.12.01

PRELIMINARY DRAWING  
NOT TO BE USED FOR CONSTRUCTION  
CONSULTANT DRAWING NO.  
704-INF.MB0304301-S0011



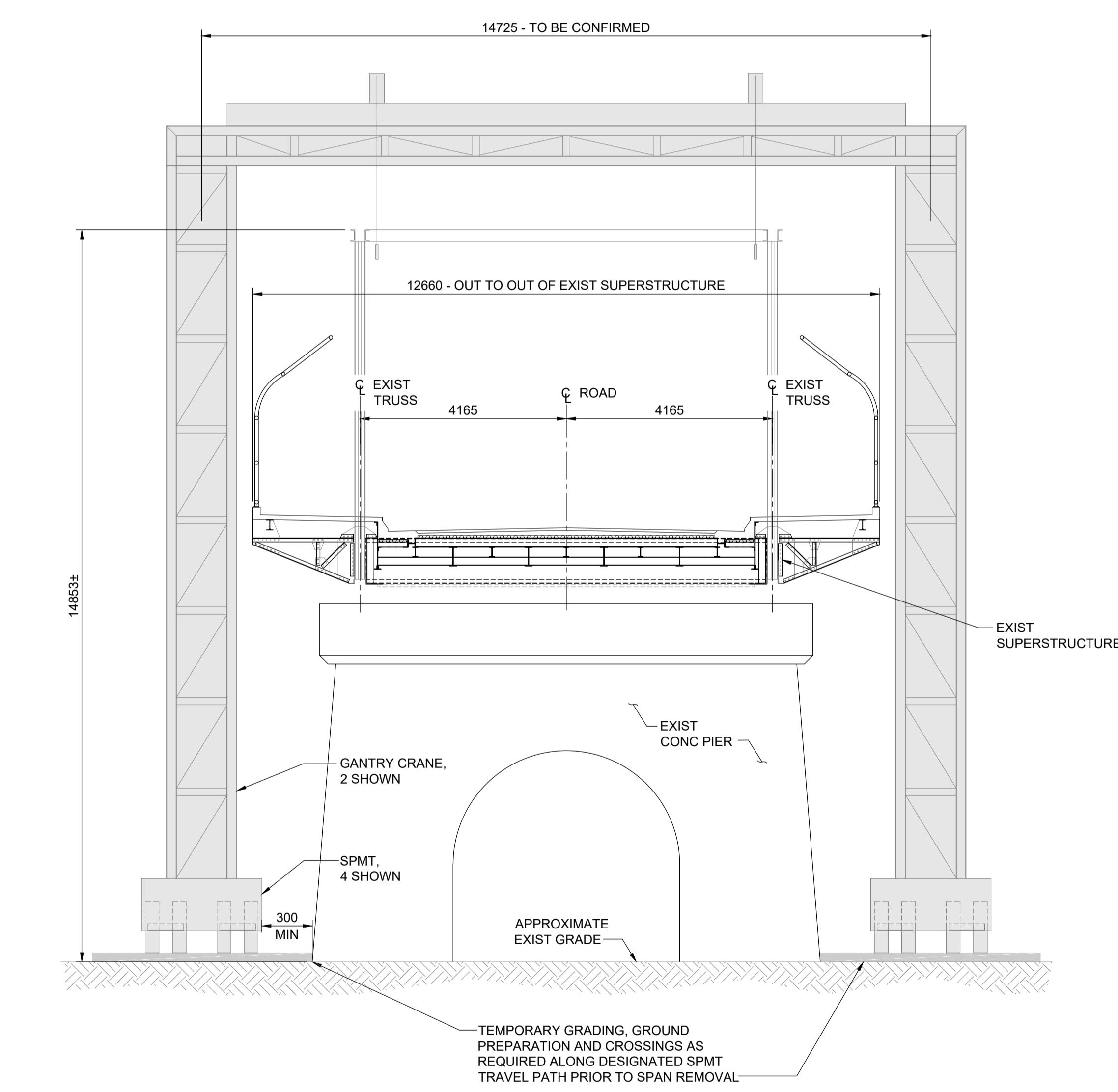
THE CITY OF WINNIPEG  
PUBLIC WORKS DEPARTMENT

ARLINGTON BRIDGE  
PHASE 4 - DECOMMISSIONING

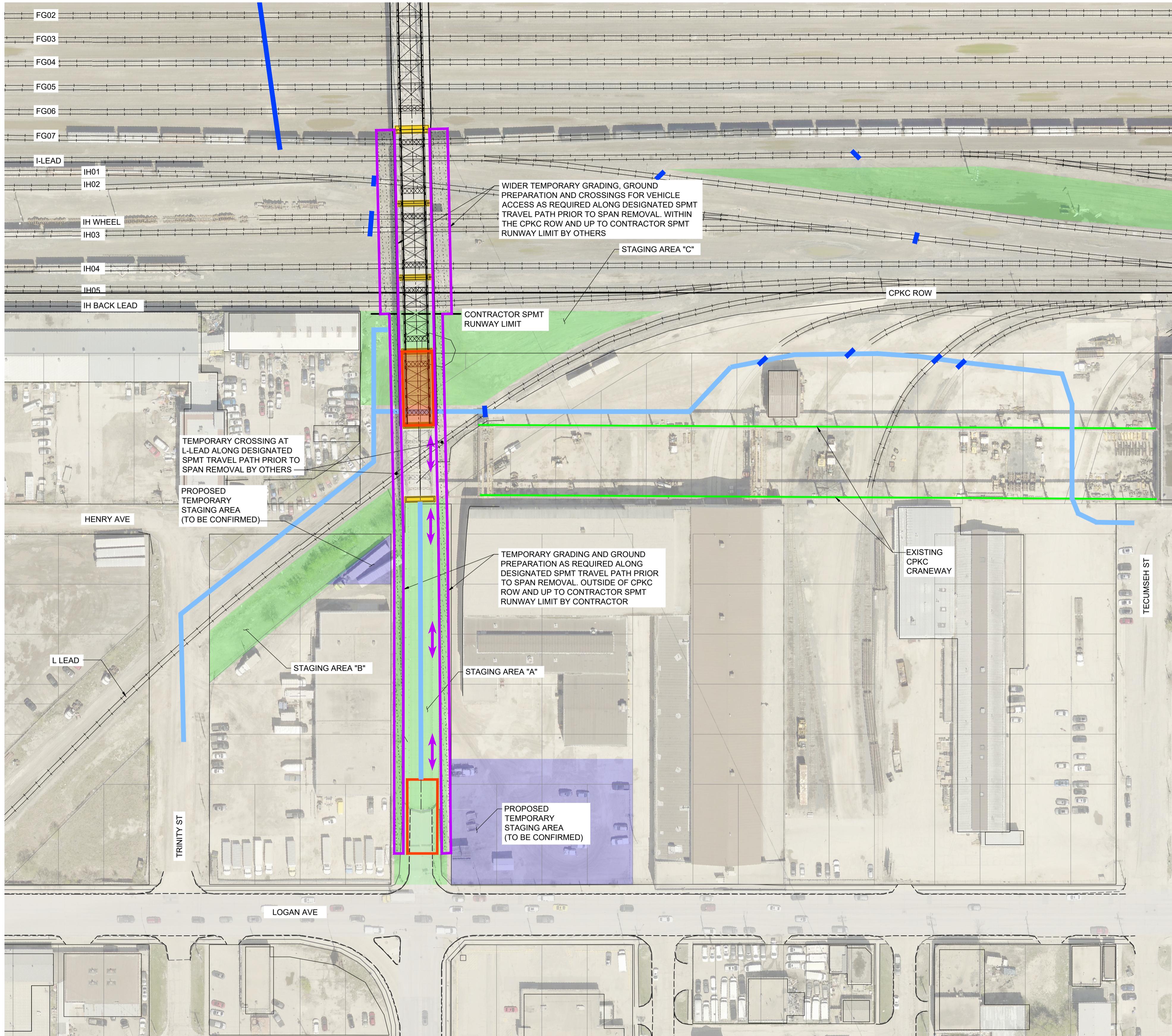
CITY DRAWING NUMBER  
SHEET 11 OF 42

SPAN 14  
SPMT AND GANTRY REMOVAL OPTION

11



3 SPAN 14 - CROSS SECTION  
1:75



1 SPAN 15 ACCESS PLAN  
1:750

ISSUED FOR  
INFORMATION  
ONLY  
NOT TO BE  
USED FOR  
CONSTRUCTION

ENGINERS  
GEOSCIENTISTS  
MANITOBA  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

#### SEQUENCE OF WORK

##### PRE-WORKBLOCK DEMOLITION WORKS:

- PERFORM LOCALIZED TRUSS MODIFICATIONS/REMOVALS TO FACILITATE TRUSS REMOVAL
- ERECT AND SETUP SELF-PROPELLED MODULAR TRANSPORTS (SPMT) AND GANTRY CRANES
- PRIOR TO SPAN REMOVAL WORKBLOCK, ALL TRAINS SHALL BE REMOVED WITHIN SPMT TRAVEL PATH

##### TRUSS REMOVAL WORKS:

- ENSURE L-LEAD IS REMOVED FROM SERVICE
- POSITION SPMT TO SPAN 15
- LIFT AND MOVE SPAN FROM BRIDGE LOCATION TO STAGING AREA "A"
- LOWER SPAN ONTO GROUND SURFACE
- RETURN SERVICE TO THE L-LEAD TRACK ONCE CLEAR OF RIGHT OF WAY

##### DURING WORKBLOCK DEMOLITION WORKS:

- DISMANTLE/DEMOLISH TRUSS COMPONENTS ON GROUND, WHILE SIMULTANEOUSLY HAULING THEM OFF SITE
- HAULING EQUIPMENT TO REMOVE TRUSS COMPONENTS FROM STAGING AREAS "A" REMOVE OFFSITE VIA LOGAN AVE

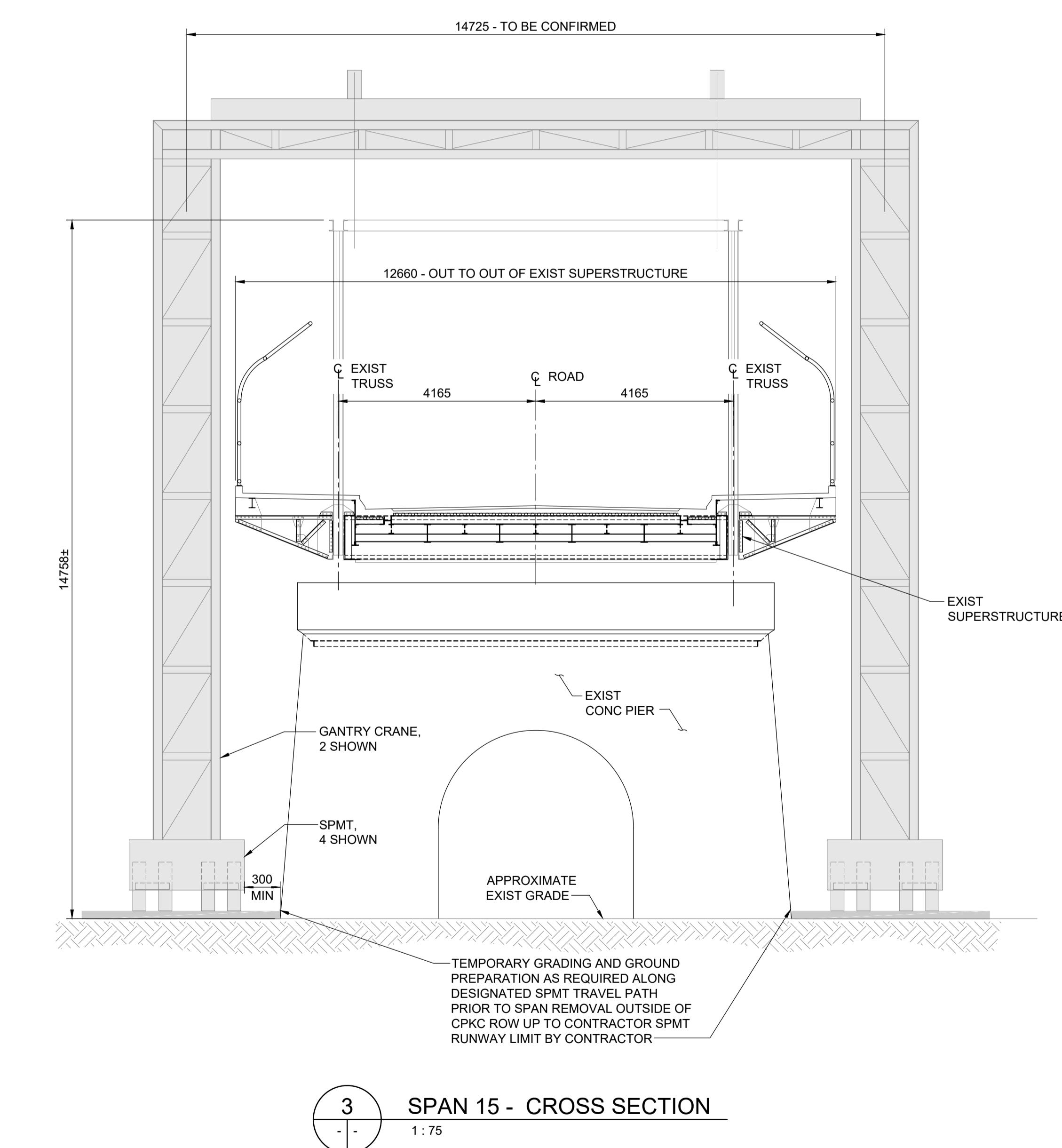
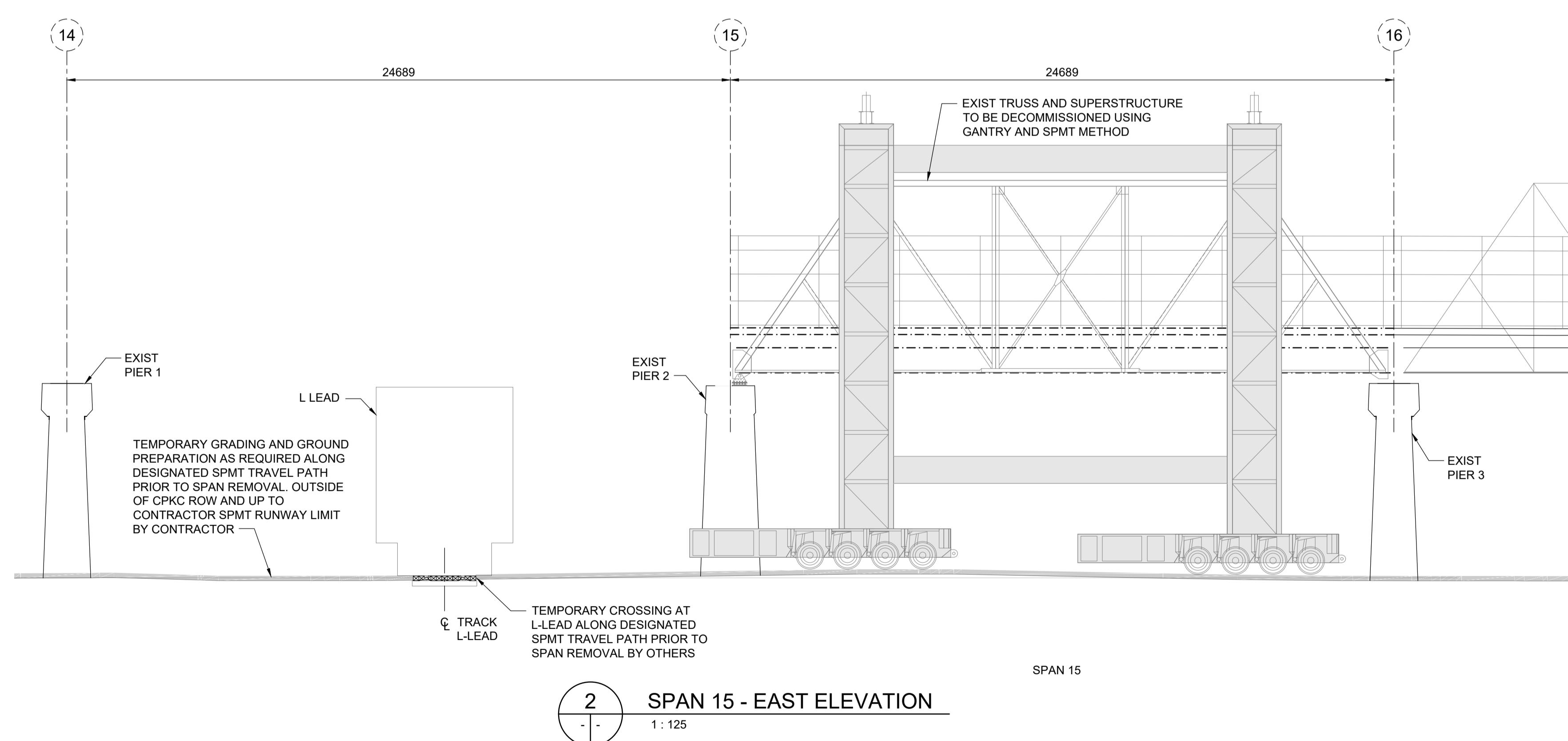
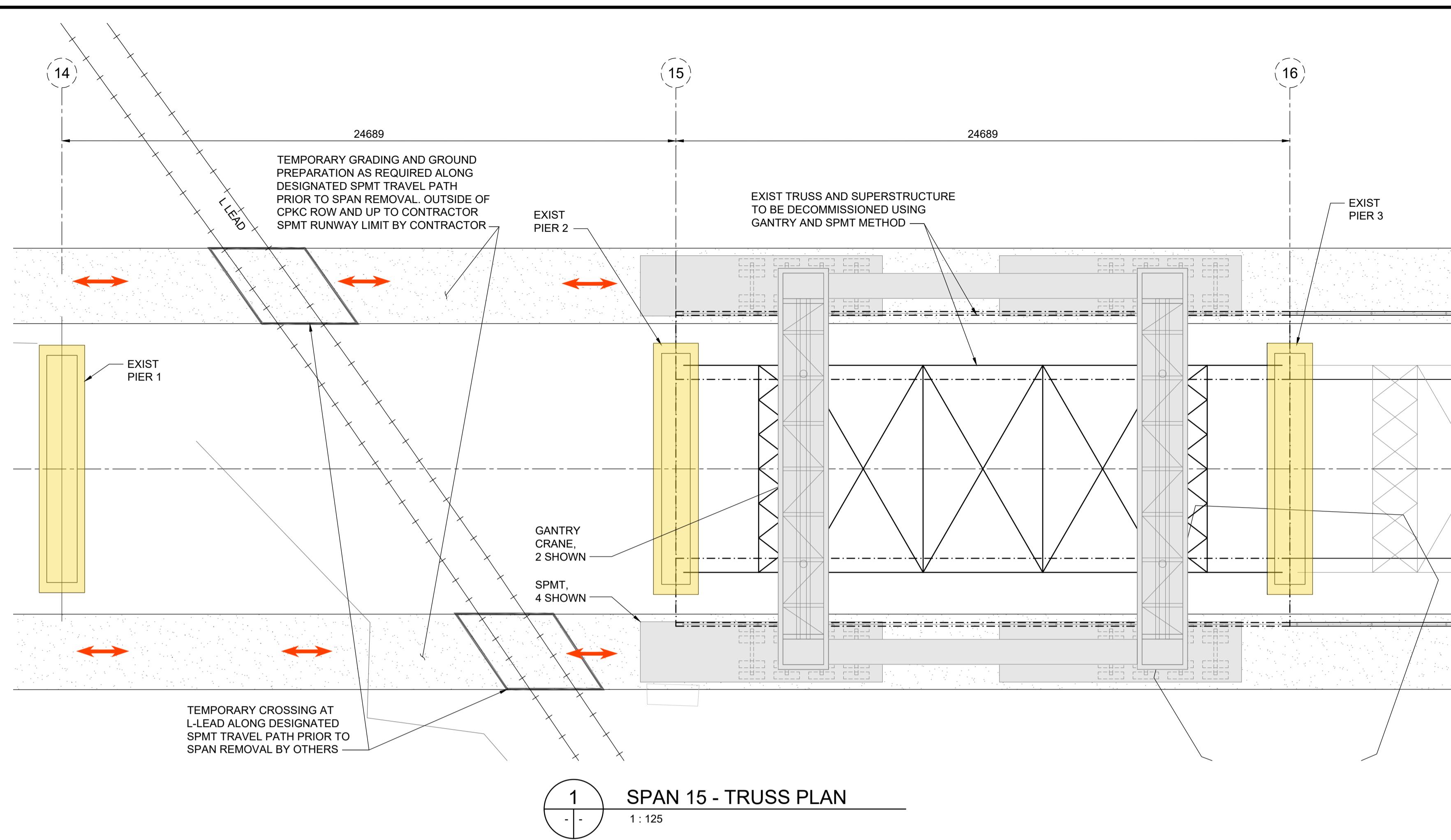
##### POST-WORKBLOCK DEMOLITION WORKS:

- CLEAN AND RESTORE GROUND SURFACE TO PRE-DEMOLITION STATE AS REQUIRED

#### LEGEND:

- EXISTING PIER LOCATIONS
- ACCESS ROUTE FOR SPAN 15
- EXISTING AT GRADE CROSSING
- CONSTRUCTION STAGING AREA
- OVERALL WORK AREA
- SPMT TRAVEL PATH

B.M. ELEV.	<b>TETRA TECH</b>		PRELIMINARY DRAWING NOT TO BE USED FOR CONSTRUCTION	THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT
DESIGNED BY	ML	REVIEWED BY	MJB	
DRAWN BY	EV	APPROVED BY	MJB	
SCALE:	AS NOTED	ACCEPTED BY	DATE	
0 ISSUED FOR TENDER	25.12.01	MJB		
NO. REVISIONS	DATE	BY	DATE	2025.12.01
CONSULTANT DRAWING NO. 704-INF.MB10304301-S0012				CITY DRAWING NUMBER SHEET 12 OF 42
SPAN 15 ACCESS PLAN SPMT AND GANTRY REMOVAL OPTION				12



PRATT TRUSS ESTIMATED  
MASS: 200,000 kg

ISSUED FOR  
INFORMATION  
ONLY  
NOT TO BE  
USED FOR  
CONSTRUCTION

**ENGINEERS  
GEOSCIENTISTS  
MANITOBA**  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

B.M. ELEV.		TETRA TECH		PRELIMINARY DRAWING	
DESIGNED BY	ML	REVIEWED BY	MJB	NOT TO BE USED FOR CONSTRUCTION	
DRAWN BY	EV	APPROVED BY	MJB		
SCALE:	AS NOTED	ACCEPTED BY	DATE		
0 ISSUED FOR TENDER	25.12.01	MJB			
NO. REVISIONS	DATE	BY	DATE	2025.12.01	



PRELIMINARY  
DRAWING  
NOT TO BE  
USED FOR  
CONSTRUCTION  
CONSULTANT DRAWING NO.  
704-INF.MB10304301-S0013



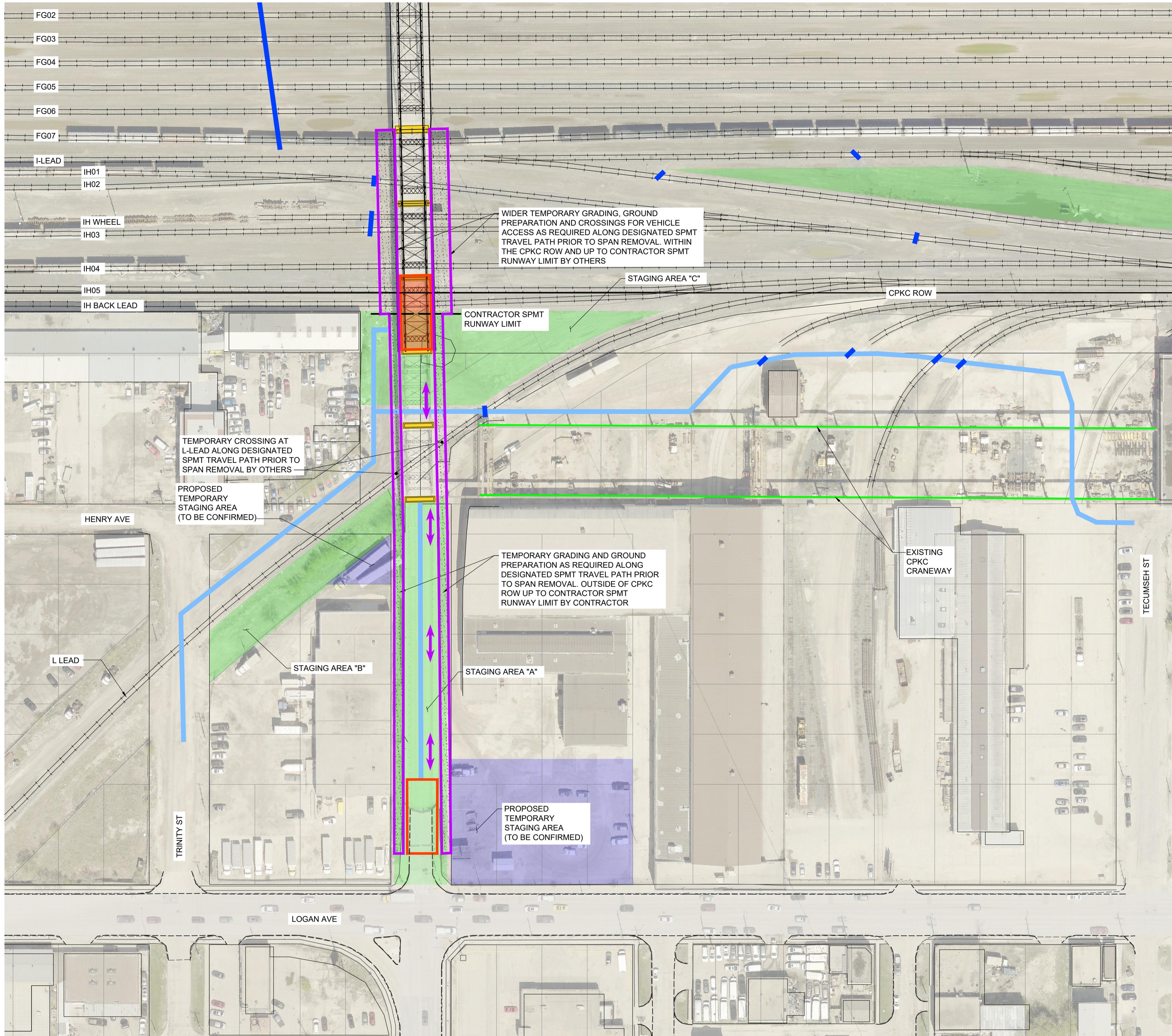
THE CITY OF WINNIPEG  
PUBLIC WORKS DEPARTMENT

ARLINGTON BRIDGE  
PHASE 4 - DECOMMISSIONING

CITY DRAWING NUMBER  
SHEET 13 OF 42

SPAN 15  
SPMT AND GANTRY REMOVAL OPTION

13



1 SPAN 16 ACCESS PLAN  
1:750

ISSUED FOR  
INFORMATION  
ONLY  
NOT TO BE  
USED FOR  
CONSTRUCTION

**ENGINEERS  
GEOSCIENTISTS  
MANITOBA**  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

#### SEQUENCE OF WORK

##### PRE-WORKBLOCK DEMOLITION WORKS:

- PERFORM LOCALIZED TRUSS MODIFICATIONS/REMOVALS TO FACILITATE TRUSS REMOVAL
- ERECT AND SETUP SELF-PROPELLED MODULAR TRANSPORTS (SPMT) AND GANTRY CRANES
- PRIOR TO SPAN REMOVAL WORKBLOCK, ALL TRAINS SHALL BE REMOVED WITHIN SPMT TRAVEL PATH

##### TRUSS REMOVAL WORKS:

- ENSURE L-LEAD, IH BACK LEAD, IH05, IH04 TRACKS ARE REMOVED FROM SERVICE
- POSITION SPMT TO SPAN 16
- LIFT AND MOVE SPAN FROM BRIDGE LOCATION TO STAGING AREA "A"
- LOWER SPAN ONTO GROUND SURFACE
- RETURN SERVICE TO THE IH BACK LEAD, IH05 AND IH04 TRACKS ONCE SPMT CLEARS IH BACK LEAD RIGHT OF WAY
- RETURN SERVICE TO THE L-LEAD TRACKS ONCE SPMT CLEARS L LEAD RIGHT OF WAY

##### DURING WORKBLOCK DEMOLITION WORKS:

- DISMANTLE/DEMOLISH TRUSS COMPONENTS ON GROUND, WHILE SIMULTANEOUSLY HAULING THEM OFF SITE
- HAULING EQUIPMENT TO REMOVE TRUSS COMPONENTS FROM STAGING AREAS "A" VIA LOGAN AVE

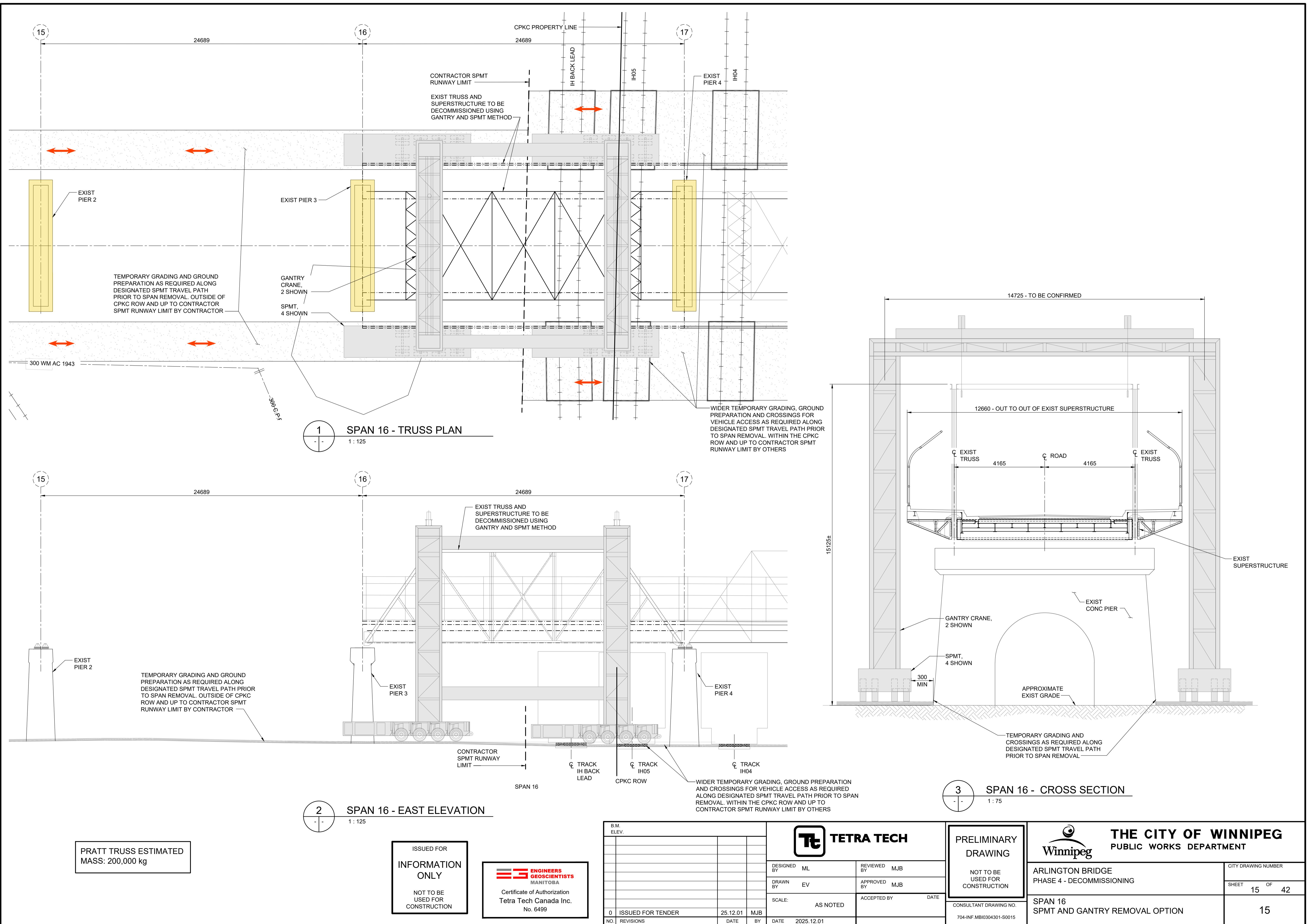
##### POST-WORKBLOCK DEMOLITION WORKS:

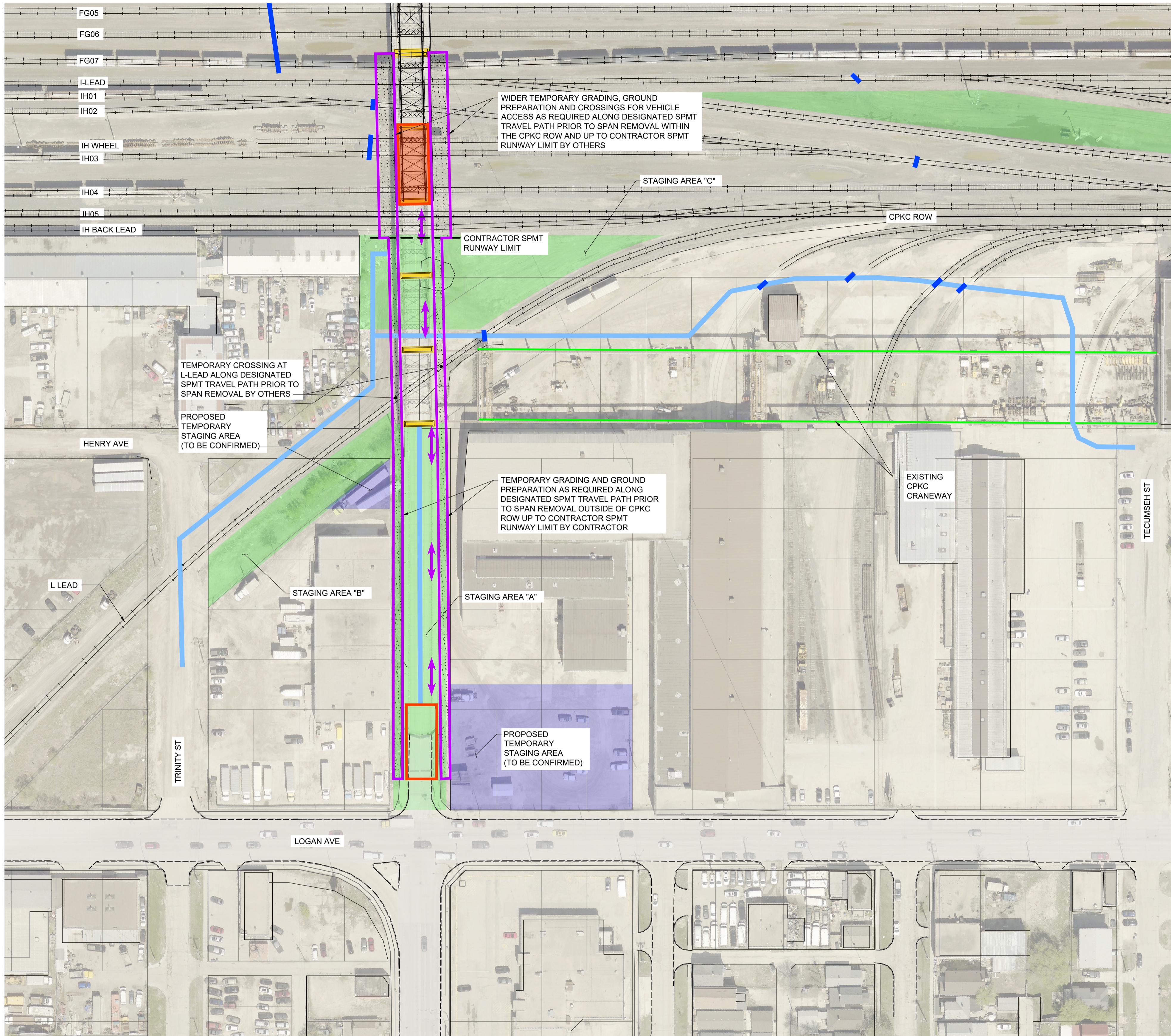
- CLEAN AND RESTORE GROUND SURFACE TO PRE-DEMOLITION STATE AS REQUIRED

#### LEGEND:

- EXISTING PIER LOCATIONS
- ACCESS ROUTE FOR SPAN 16
- EXISTING AT GRADE CROSSING
- CONSTRUCTION STAGING AREA
- OVERALL WORK AREA
- SPMT TRAVEL PATH

B.M. ELEV.		<b>TETRA TECH</b>		PRELIMINARY DRAWING NOT TO BE USED FOR CONSTRUCTION		THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT	
DESIGNED BY	ML	REVIEWED BY	MJB				
DRAWN BY	EV	APPROVED BY	MJB				
SCALE:	AS NOTED	ACCEPTED BY	DATE				
0 ISSUED FOR TENDER	25.12.01	MJB					
NO. REVISIONS	DATE	BY	DATE	2025.12.01			
CONSULTANT DRAWING NO. 704-INF.MB10304301-S0014				CITY DRAWING NUMBER SHEET 14 OF 42			
SPAN 16 ACCESS PLAN SPMT AND GANTRY REMOVAL OPTION				14			





1 SPAN 17 ACCESS PLAN  
1:750

ISSUED FOR  
INFORMATION  
ONLY  
NOT TO BE  
USED FOR  
CONSTRUCTION

ENGINEERS  
GEOSCIENTISTS  
MANITOBA  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

#### SEQUENCE OF WORK

##### PRE-WORKBLOCK DEMOLITION WORKS:

- PERFORM LOCALIZED TRUSS MODIFICATIONS/REMOVALS TO FACILITATE TRUSS REMOVAL
- ERECT AND SETUP SELF-PROPELLED MODULAR TRANSPORTS (SPMT) AND GANTRY CRANES
- PRIOR TO SPAN REMOVAL WORKBLOCK, ALL TRAINS SHALL BE REMOVED WITHIN SPMT TRAVEL PATH

##### TRUSS REMOVAL WORKS:

- ENSURE L-LEAD, IH BACK LEAD, IH04, IH03 AND IH WHEEL TRACKS ARE REMOVED FROM SERVICE
- POSITION SPMT TO SPAN 17
- LIFT AND MOVE SPAN FROM BRIDGE LOCATION TO STAGING AREA "A"
- LOWER SPAN ONTO GROUND SURFACE
- RETURN SERVICE TO THE IH BACK LEAD, IH05, IH04, IH03 AND IH WHEEL TRACK ONCE SPMT CLEARS IH BACK LEAD RIGHT OF WAY
- RETURN SERVICE TO THE L-LEAD TRACK ONCE SPMT CLEARS L LEAD RIGHT OF WAY

##### DURING WORKBLOCK DEMOLITION WORKS:

- DISMANTLE/DEMOLISH TRUSS COMPONENTS ON GROUND, WHILE SIMULTANEOUSLY HAULING THEM OFF SITE
- HAULING EQUIPMENT TO REMOVE TRUSS COMPONENTS FROM STAGING AREAS "A" VIA LOGAN AVE

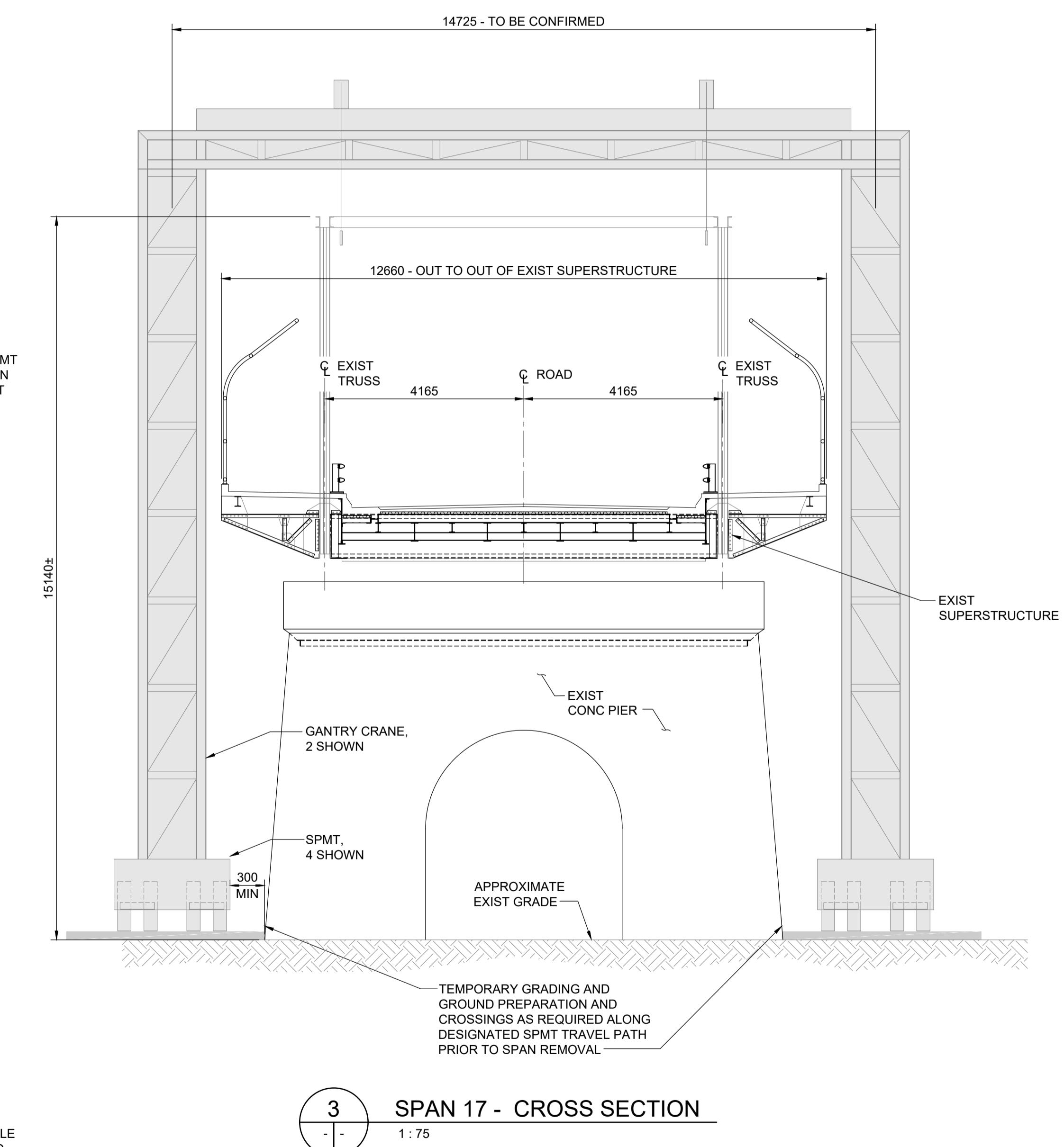
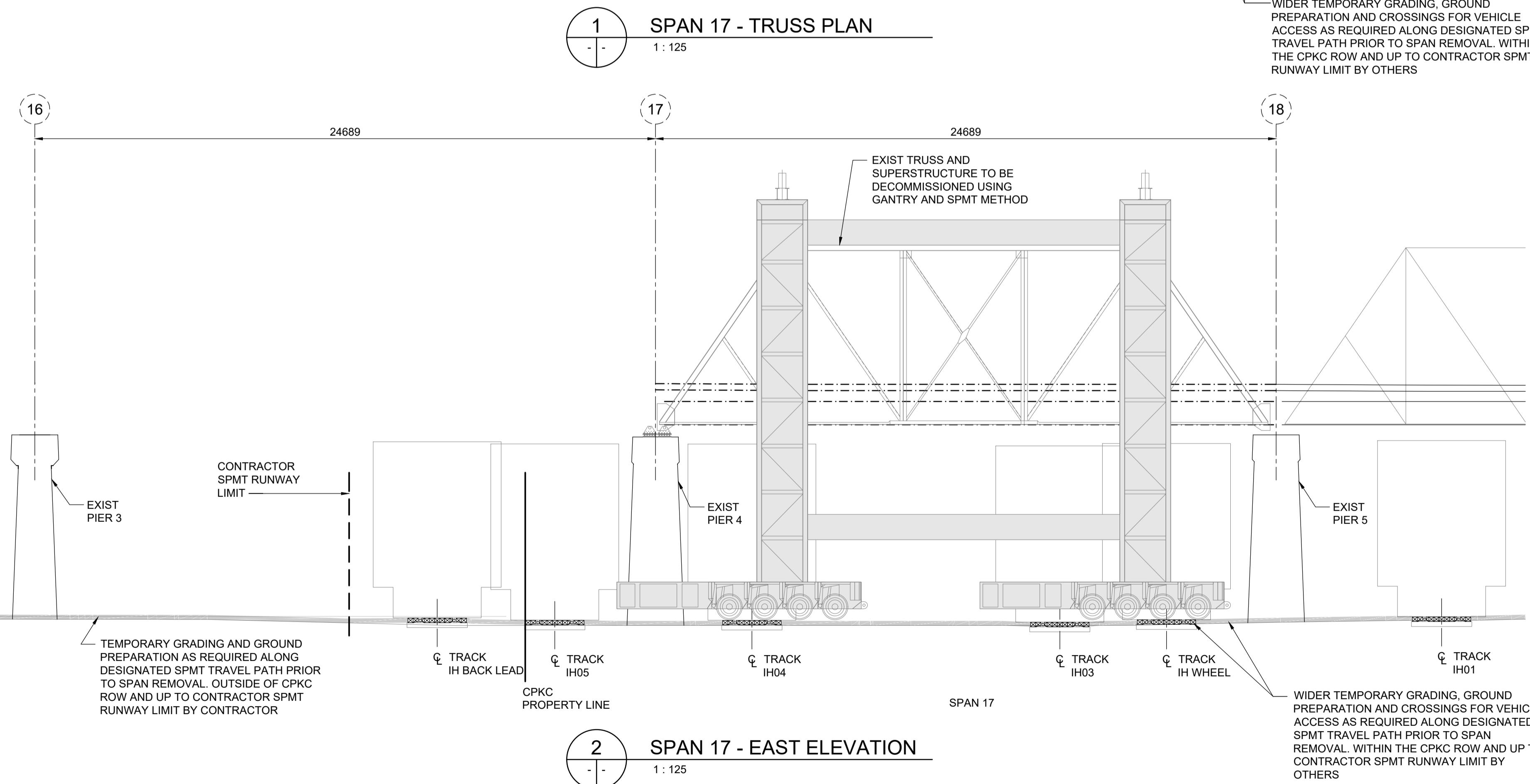
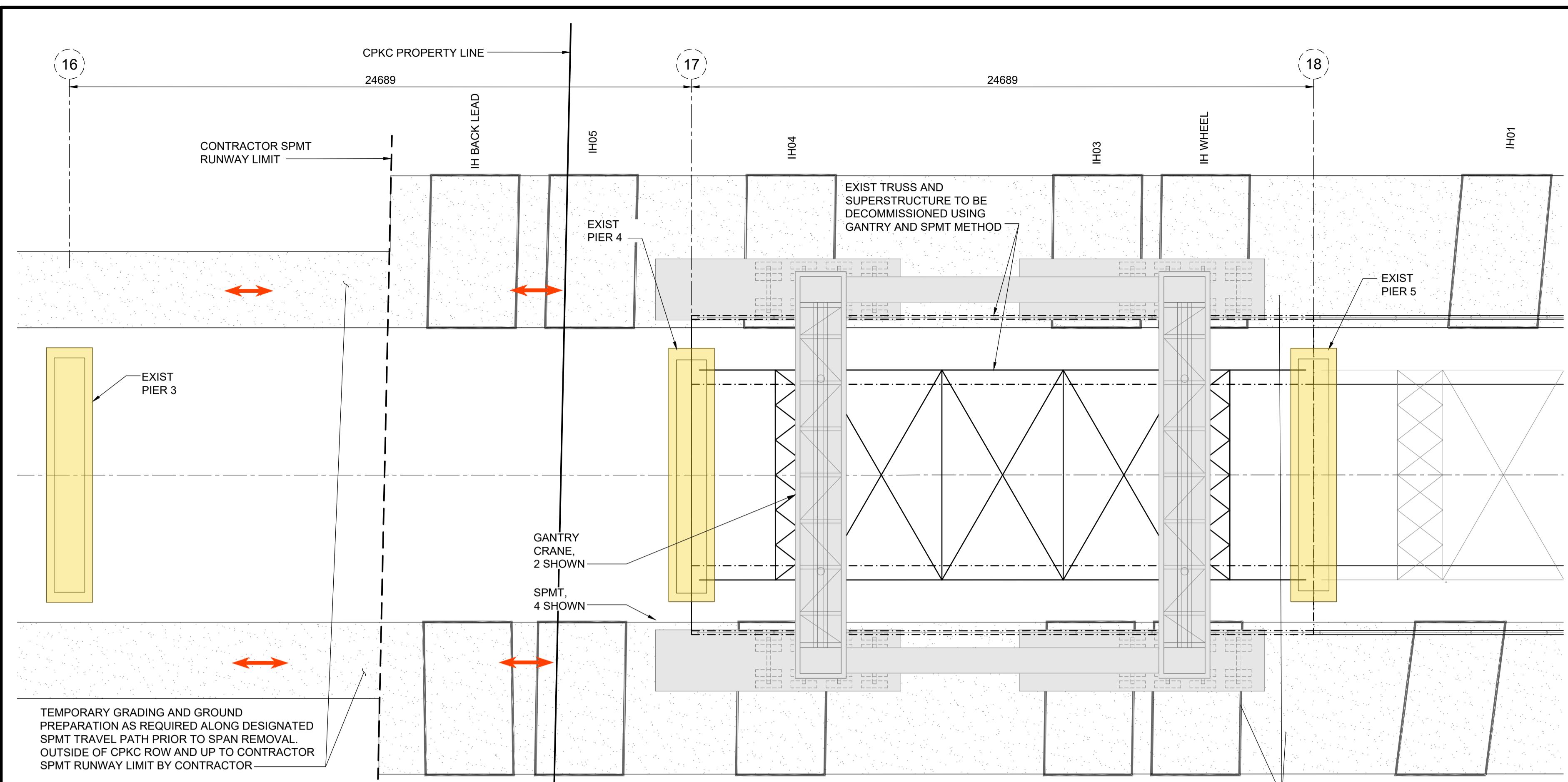
##### POST-WORKBLOCK DEMOLITION WORKS:

- REMOVE TEMPORARY GRADING AND CROSSINGS FOR SPMT RUNWAY. CLEAN AND RESTORE GROUND SURFACE TO PRE-DEMOLITION STATE AS REQUIRED

#### LEGEND:

- EXISTING PIER LOCATIONS
- ACCESS ROUTE FOR SPMT 17
- EXISTING AT GRADE CROSSING
- CONSTRUCTION STAGING AREA
- OVERALL WORK AREA
- SPMT TRAVEL PATH

B.M. ELEV.		TETRA TECH		PRELIMINARY DRAWING NOT TO BE USED FOR CONSTRUCTION		THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT	
DESIGNED BY	ML	REVIEWED BY	MJB				
DRAWN BY	EV	APPROVED BY	MJB				
SCALE:	AS NOTED	ACCEPTED BY	DATE				
0 ISSUED FOR TENDER	25.12.01	MJB					
NO. REVISIONS	DATE	BY	DATE	2025.12.01			
CONSULTANT DRAWING NO. 704-INF.MB10304301-S0016				CITY DRAWING NUMBER SHEET 16 OF 42			
SPAN 17 ACCESS PLAN SPMT AND GANTRY REMOVAL OPTION				16			



PRATT TRUSS ESTIMATED  
MASS: 200,000 kg

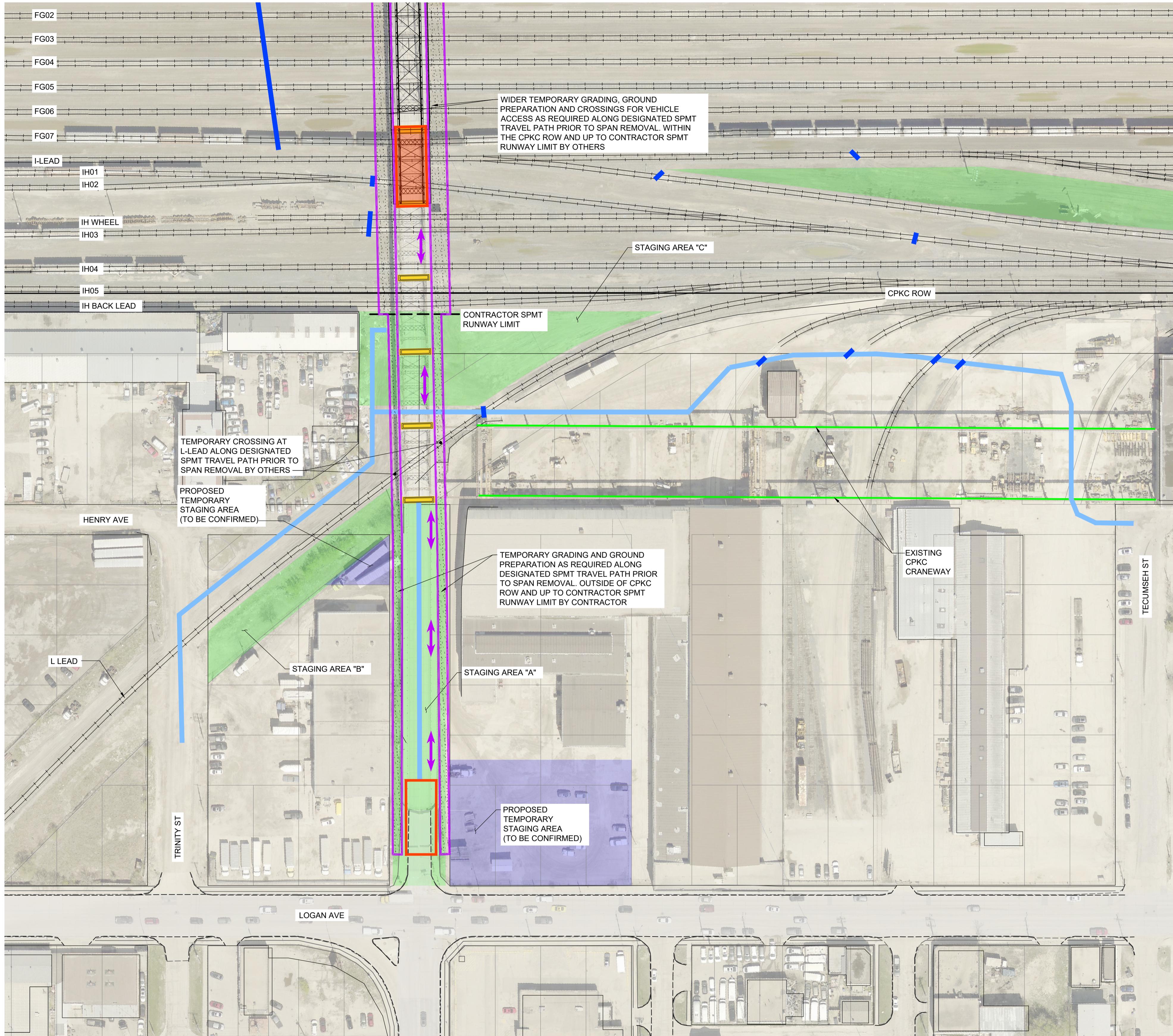
ISSUED FOR  
INFORMATION  
ONLY  
NOT TO BE  
USED FOR  
CONSTRUCTION

ENGINERS  
GEOSCIENTISTS  
MANITOBA  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

B.M. ELEV.		TETRA TECH	
DESIGNED BY	ML	REVIEWED BY	MJB
DRAWN BY	EV	APPROVED BY	MJB
SCALE:	AS NOTED	ACCEPTED BY	DATE
0 ISSUED FOR TENDER	25.12.01	MJB	
NO. REVISIONS	DATE	BY	DATE 2025.12.01

PRELIMINARY DRAWING	
NOT TO BE USED FOR CONSTRUCTION	
CONSULTANT DRAWING NO.	704-INF.MB0304301-S0017
ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	CITY DRAWING NUMBER 17 OF 42

THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT	
ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	SPAN 17 SPMT AND GANTRY REMOVAL OPTION
	17



1 SPAN 18 ACCESS PLAN  
1:750

ISSUED FOR  
INFORMATION  
ONLY  
NOT TO BE  
USED FOR  
CONSTRUCTION

ENGINEERS  
GEOSCIENTISTS  
MANITOBA  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

#### SEQUENCE OF WORK

##### PRE-WORKBLOCK DEMOLITION WORKS:

- PERFORM LOCALIZED TRUSS MODIFICATIONS/REMOVALS TO FACILITATE TRUSS REMOVAL
- ERECT AND SETUP SELF-PROPELLED MODULAR TRANSPORTS (SPMT) AND GANTRY CRANES
- PRIOR TO SPAN REMOVAL WORKBLOCK, ALL TRAINS SHALL BE REMOVED WITHIN SPMT TRAVEL PATH
- EXTEND TEMPORARY GRADING AND CROSSINGS FROM IH WHEEL TO FG06 TRACK ALONG DESIGNATED SPMT TRAVEL PATH (BY OTHERS)

##### TRUSS REMOVAL WORKS:

- ENSURE I-LEAD, IH BACK LEAD, IH05, IH04, IH03 IH WHEEL, IH01, I-LEAD AND FG07 TRACKS ARE REMOVED FROM SERVICE
- POSITION SPMT TO SPAN 18
- LIFT AND MOVE SPAN FROM BRIDGE LOCATION TO STAGING AREA "A"
- LOWER SPAN ONTO GROUND SURFACE
- RETURN SERVICE TO THE IH BACK LEAD, IH05, IH04, IH03, IH WHEEL, IH01, I-LEAD AND FG07 TRACK ONCE SPMT CLEARS IH BACK LEAD RIGHT OF WAY
- RETURN SERVICE TO THE L-LEAD TRACK ONCE SPMT CLEARS L LEAD RIGHT OF WAY

##### DURING WORKBLOCK DEMOLITION WORKS:

- DISMANTLE/DEMOLISH TRUSS COMPONENTS ON GROUND, WHILE SIMULTANEOUSLY HAULING THEM OFF SITE
- HAULING EQUIPMENT TO REMOVE TRUSS COMPONENTS FROM STAGING AREAS "A" VIA LOGAN AVE
- TRACK BLOCKS AND SERVICE REINSTATEMENTS CAN BE STAGED TO REDUCE OVERALL OUT OF SERVICE IMPACTS
- ONCE SPMT GETS TO SPAN 18, IH BACK LEAD, IH04 AND IH05 CAN RETURN TO SERVICE (UPON CPKC DIRECTION) UNTIL SPMT IS READY TO MOVE
- ONCE SPMT STARTS MOVING TRACKS THAT HAVE BEEN CLEARED CAN BE PUT BACK IN SERVICE IN A PHASED APPROACH

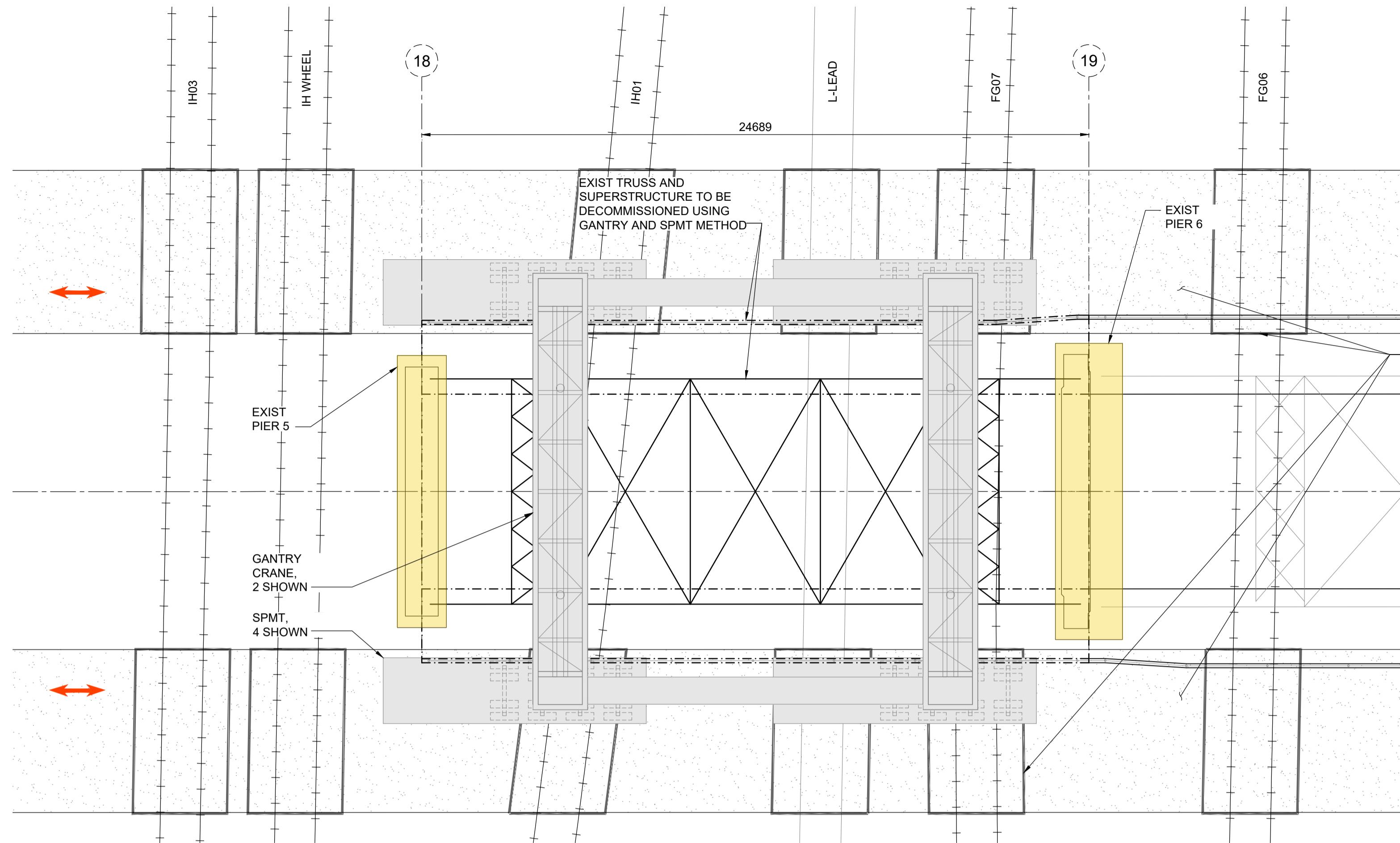
##### POST-WORKBLOCK DEMOLITION WORKS:

- REMOVE TEMPORARY GRADING AND CROSSINGS FOR SPMT RUNWAY. CLEAN AND RESTORE GROUND SURFACE TO PRE-DEMOLITION STATE AS REQUIRED

#### LEGEND:

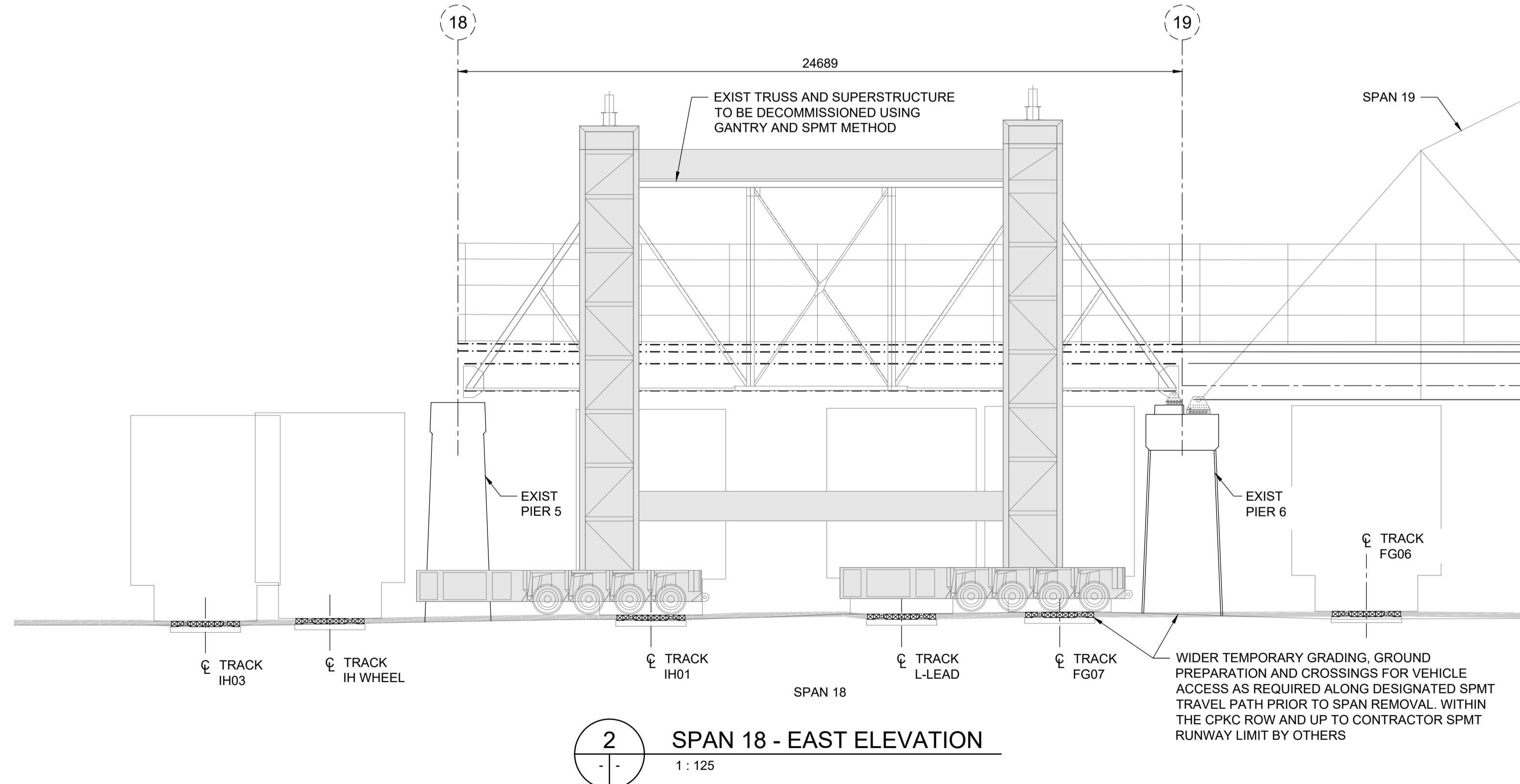
- EXISTING PIER LOCATIONS
- ACCESS ROUTE FOR SPAN 18
- EXISTING AT GRADE CROSSING
- CONSTRUCTION STAGING AREA
- OVERALL WORK AREA
- SPMT TRAVEL PATH

B.M. ELEV.		TETRA TECH		PRELIMINARY DRAWING NOT TO BE USED FOR CONSTRUCTION		THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT	
DESIGNED BY	ML	REVIEWED BY	MJB				
DRAWN BY	EV	APPROVED BY	MJB				
SCALE:	AS NOTED	ACCEPTED BY	DATE				
0 ISSUED FOR TENDER	25.12.01	MJB					
NO. REVISIONS	DATE	BY	DATE	2025.12.01			
CONSULTANT DRAWING NO. 704-INF.MB10304301-S0018				CITY DRAWING NUMBER 18 OF 42			
SPAN 18 ACCESS PLAN SPMT AND GANTRY REMOVAL OPTION				18			



WIDER TEMPORARY GRADING, GROUND PREPARATION AND CROSSINGS FOR VEHICLE ACCESS AS REQUIRED ALONG DESIGNATED SPMT TRAVEL PATH PRIOR TO SPAN REMOVAL. WITHIN THE CPKC ROW AND UP TO CONTRACTOR SPMT RUNWAY LIMIT BY OTHERS

1 SPAN 18 - TRUSS PLAN  
1:125



WIDER TEMPORARY GRADING, GROUND PREPARATION AND CROSSINGS FOR VEHICLE ACCESS AS REQUIRED ALONG DESIGNATED SPMT TRAVEL PATH PRIOR TO SPAN REMOVAL. WITHIN THE CPKC ROW AND UP TO CONTRACTOR SPMT RUNWAY LIMIT BY OTHERS

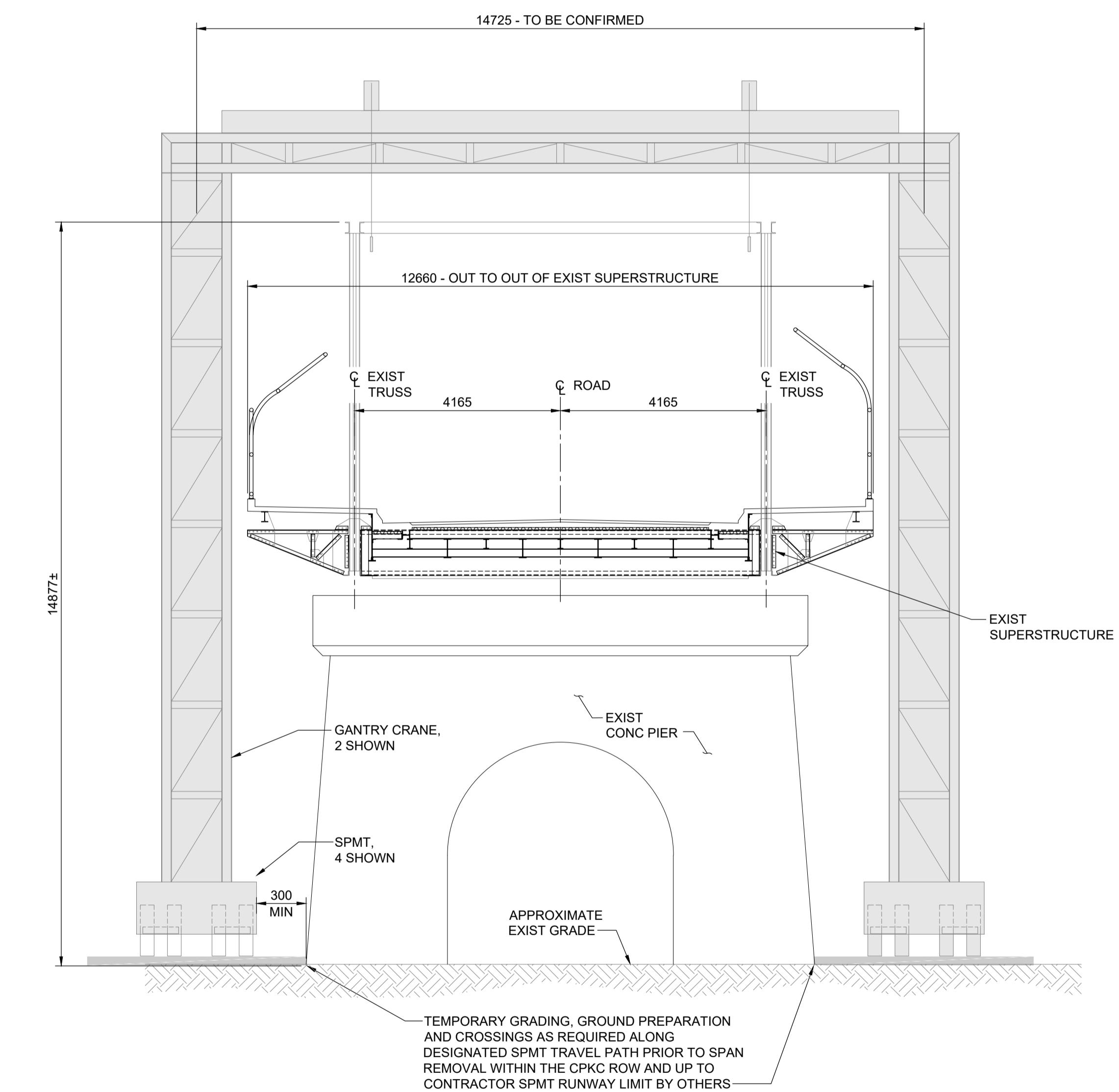
2 SPAN 18 - EAST ELEVATION  
1:125

PRATT TRUSS ESTIMATED MASS: 200,000 kg

ISSUED FOR INFORMATION ONLY  
NOT TO BE USED FOR CONSTRUCTION

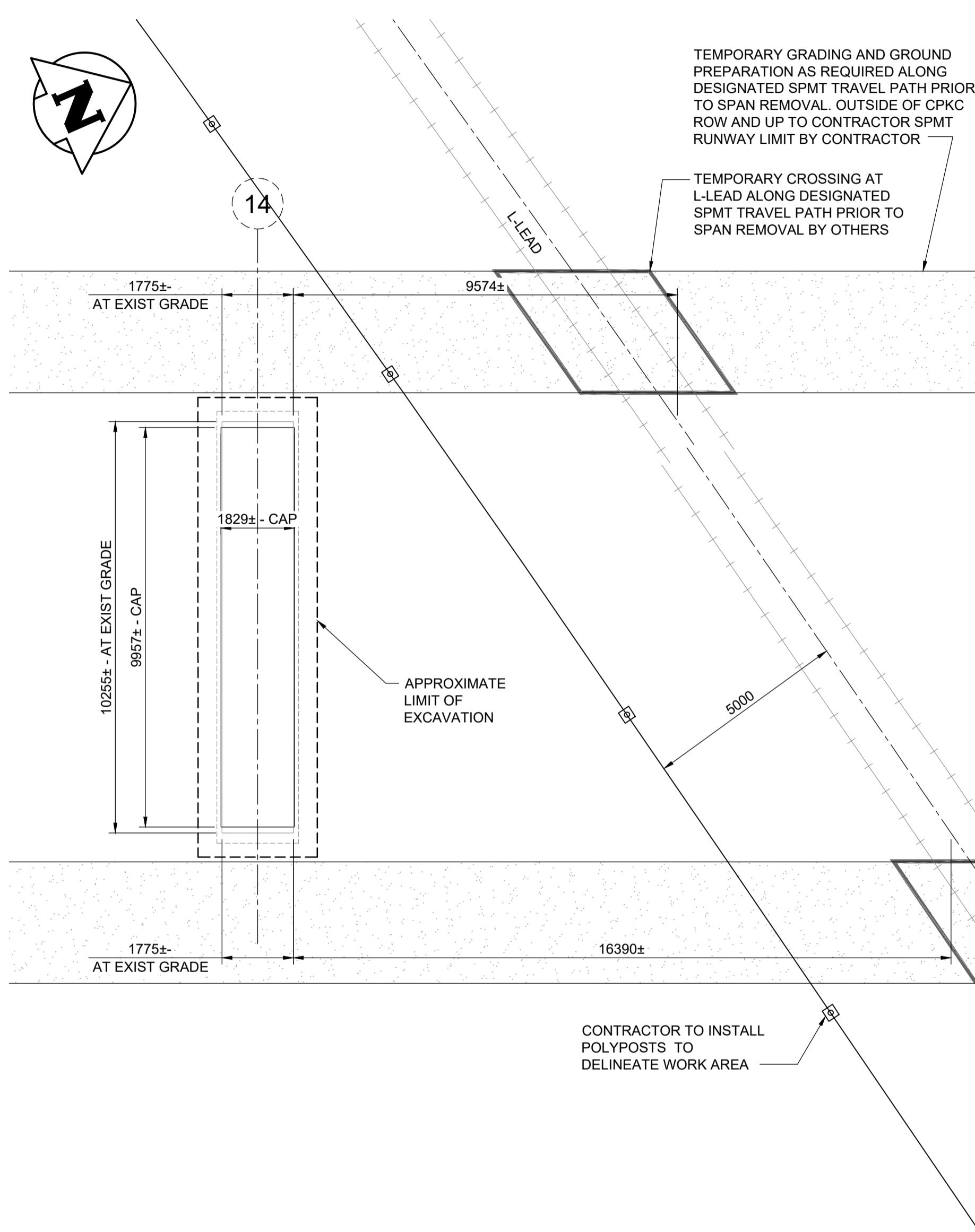
ENRGENS  
GEOLOGISTS  
MANITOBA  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

B.M. ELEV.		TETRA TECH		PRELIMINARY DRAWING	
DESIGNED BY	ML	REVIEWED BY	MJB	NOT TO BE USED FOR CONSTRUCTION	
DRAWN BY	EV	APPROVED BY	MJB		
SCALE:	AS NOTED	ACCEPTED BY	DATE		
0 ISSUED FOR TENDER	25.12.01	MJB			
NO. REVISIONS	DATE	BY	DATE	2025.12.01	

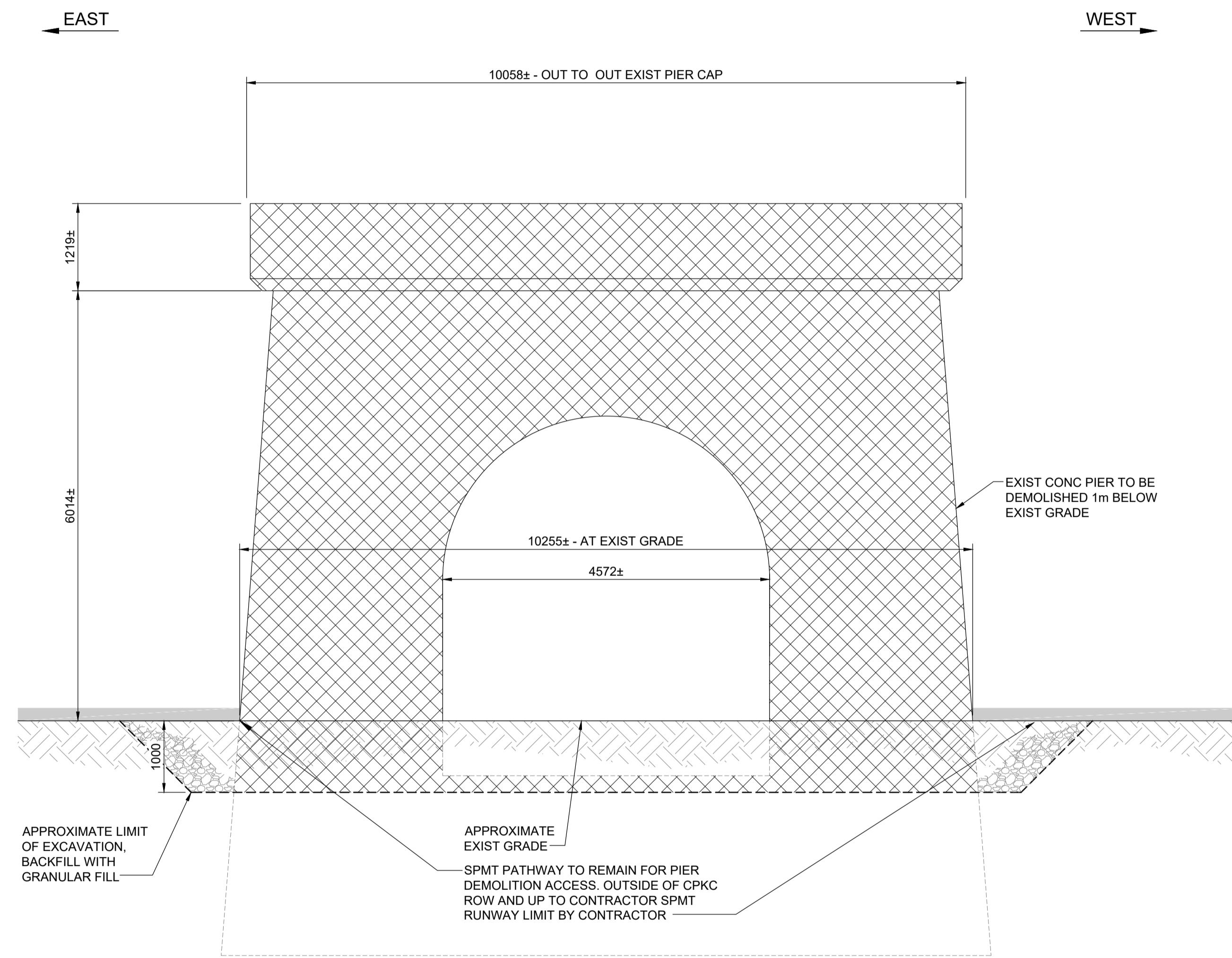


3 SPAN 18 - CROSS SECTION  
1:75

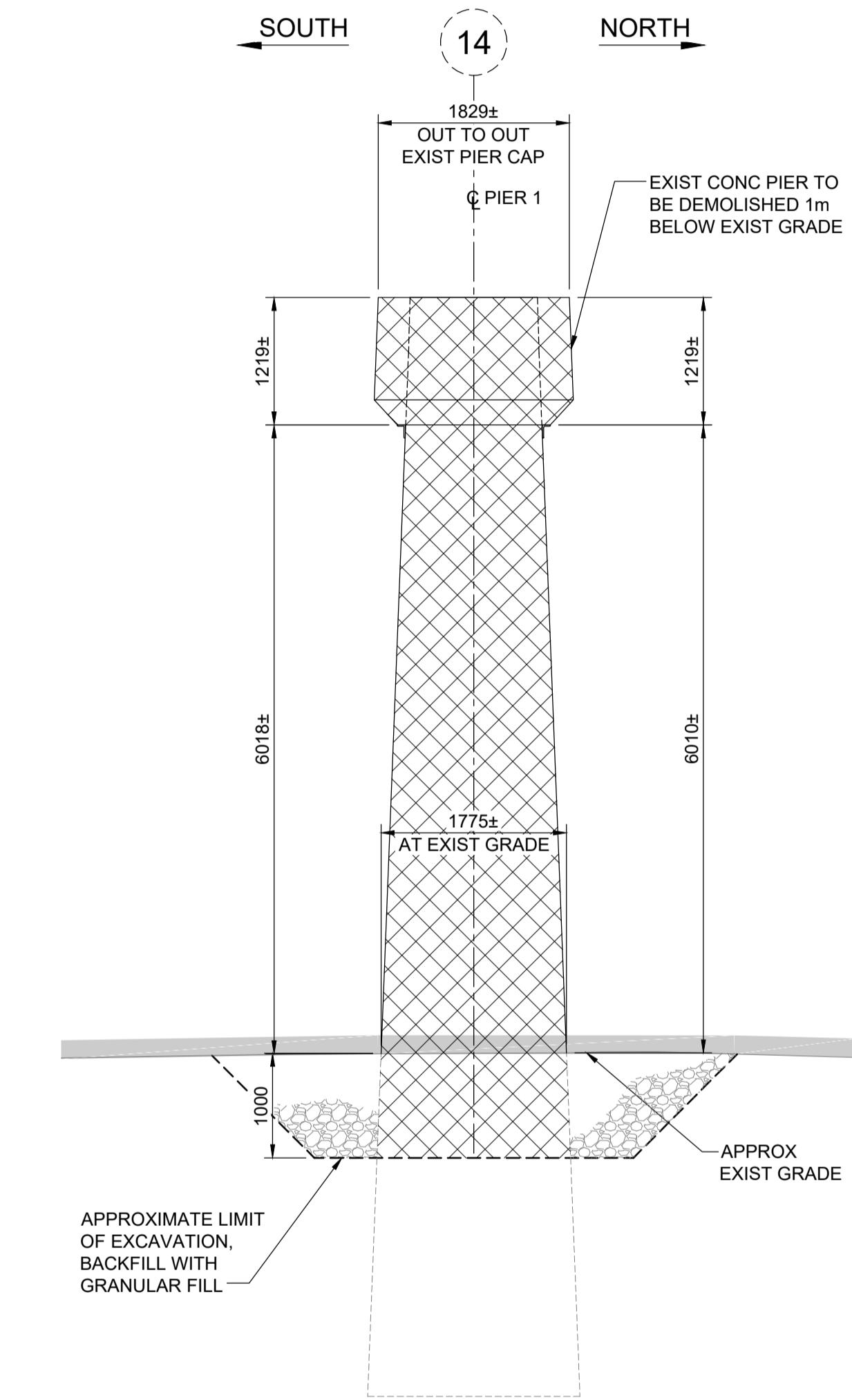
THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT		CITY DRAWING NUMBER
ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING		SHEET 19 OF 42
SPAN 18 SPMT AND GANTRY REMOVAL OPTION		19



1 PIER 1 - PLAN  
1 : 100



2 PIER 1 - NORTH ELEVATION  
1 : 50

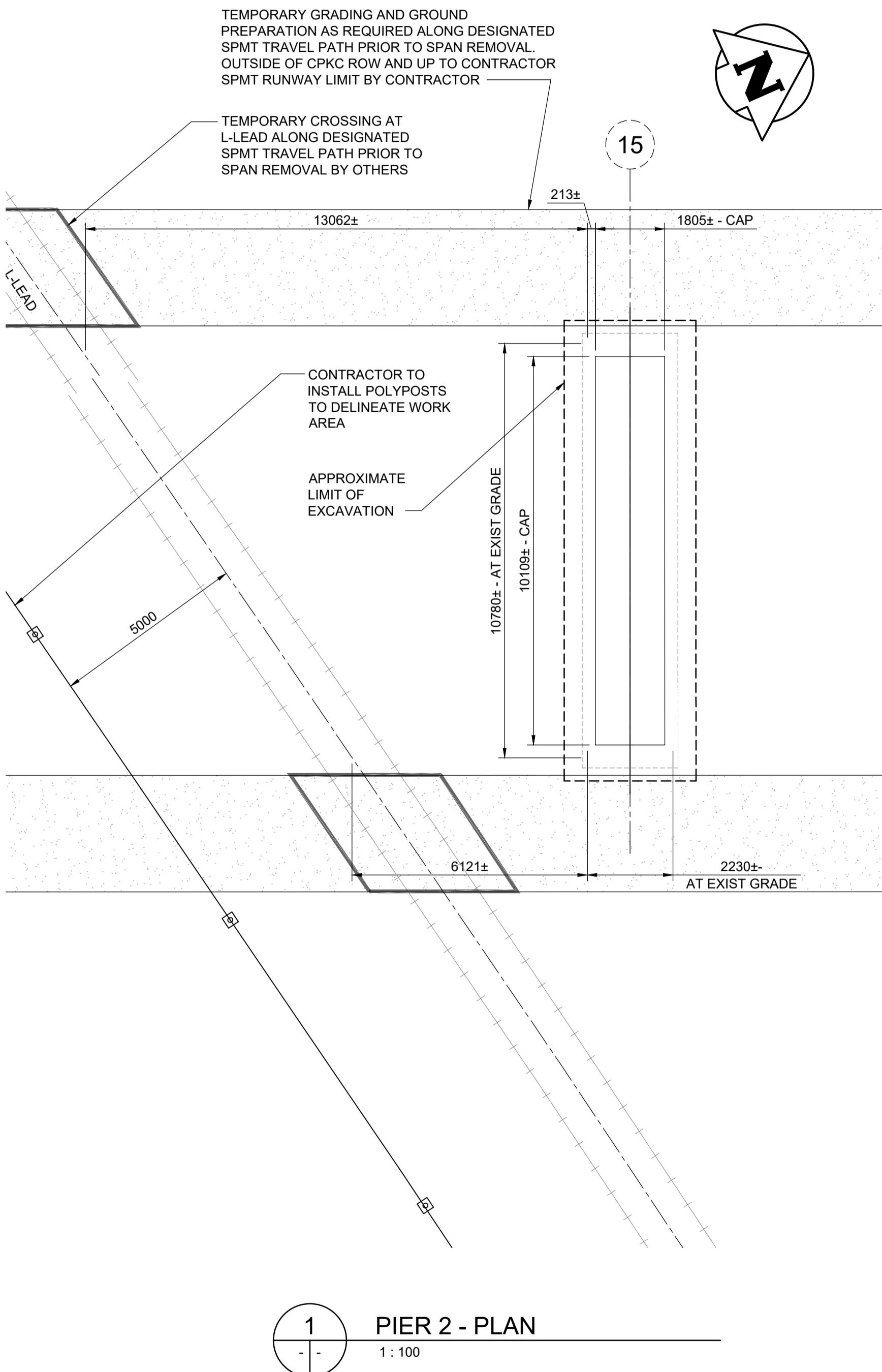


3 PIER 1 - END EAST ELEVATION  
1 : 50

ISSUED FOR  
INFORMATION  
ONLY  
NOT TO BE  
USED FOR  
CONSTRUCTION

**ENGINEERS  
GEOSCIENTISTS  
MANITOBA**  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

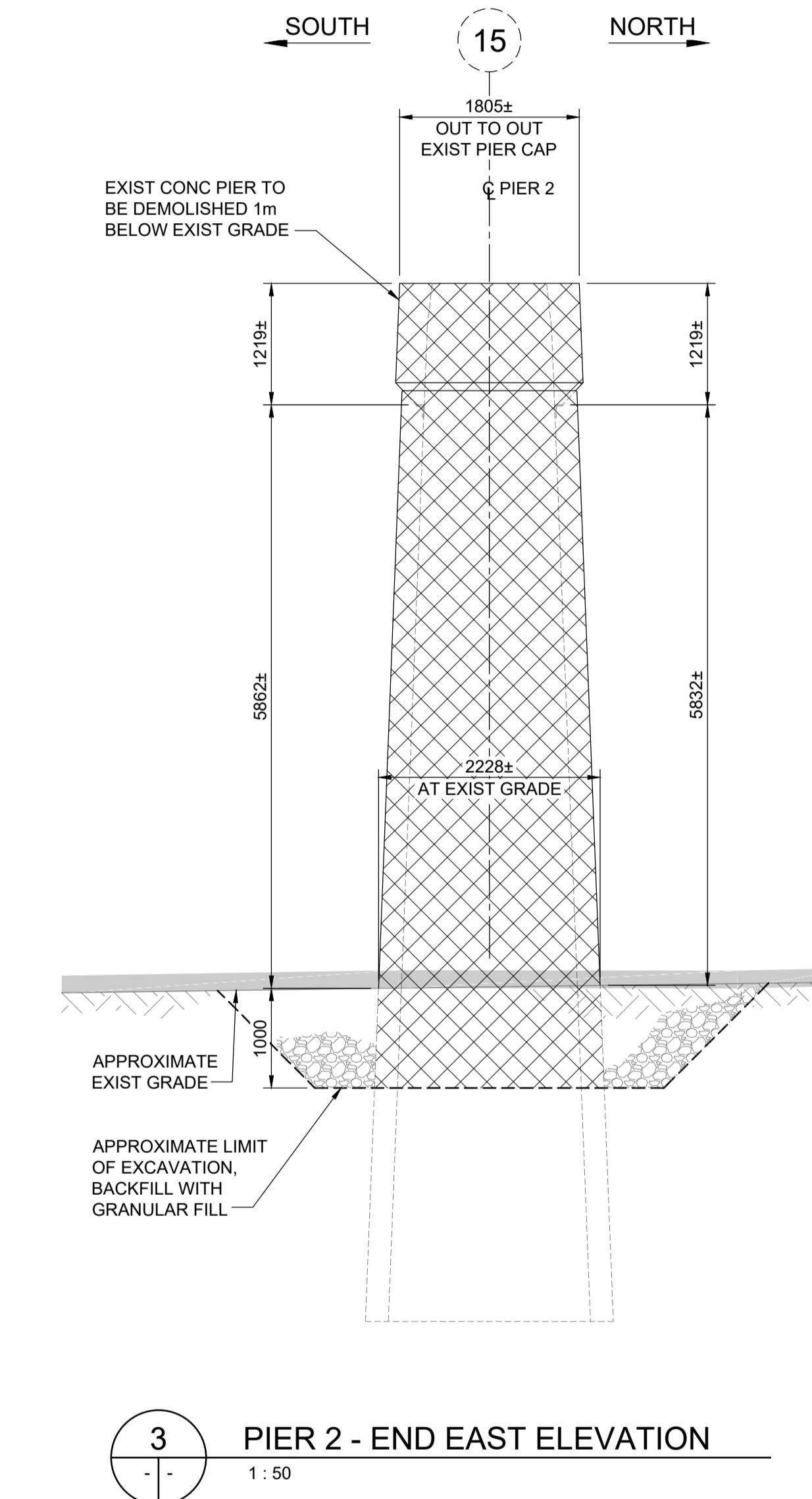
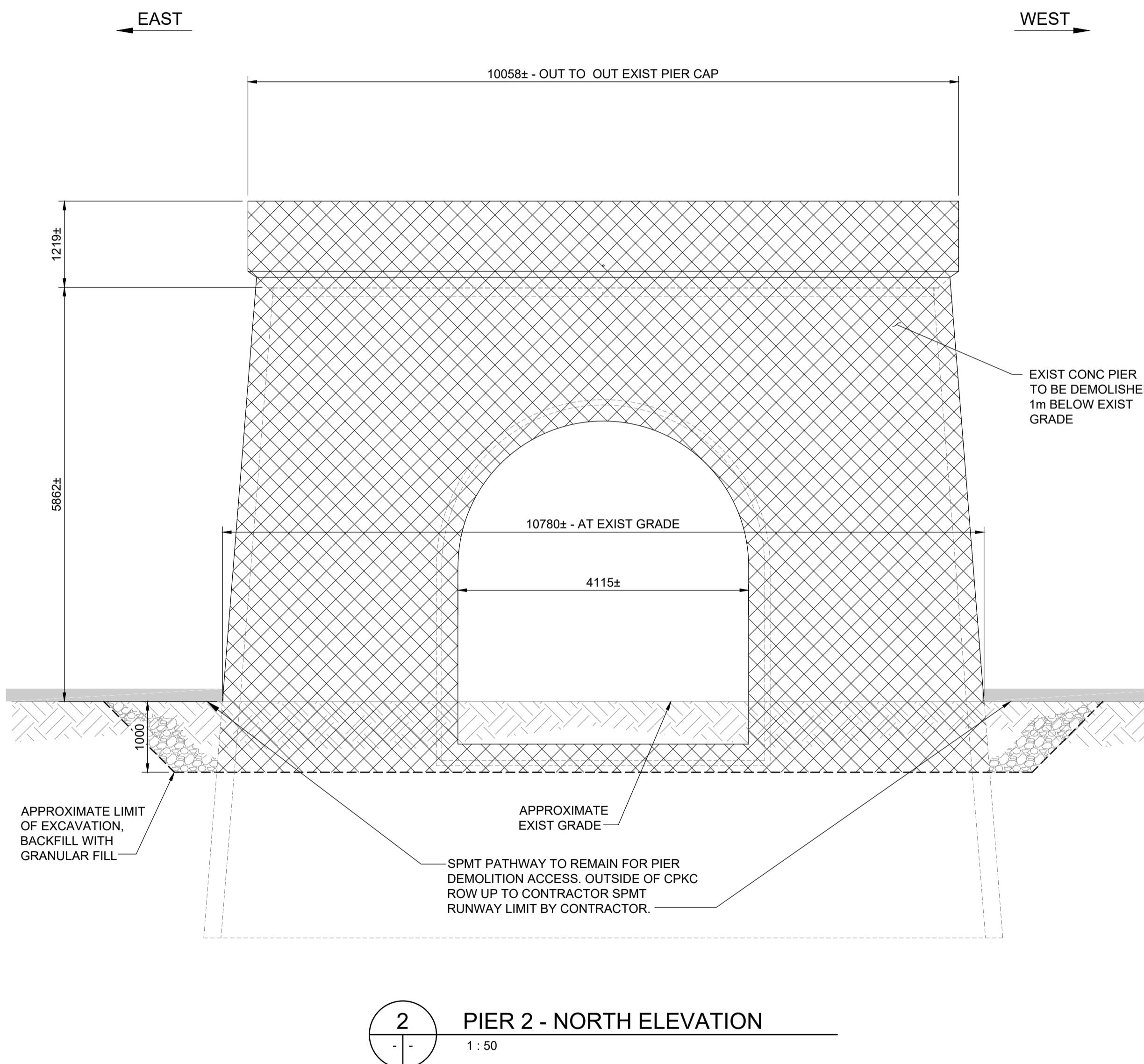
B.M. ELEV.		TETRA TECH		PRELIMINARY DRAWING NOT TO BE USED FOR CONSTRUCTION		THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT	
DESIGNED BY	ML	REVIEWED BY	MJB				
DRAWN BY	EV	APPROVED BY	MJB				
SCALE:	AS NOTED	ACCEPTED BY	DATE				
0 ISSUED FOR TENDER	25.12.01	MJB					
NO. REVISIONS	DATE	BY	DATE	2025.12.01			
CONSULTANT DRAWING NO. 704-INF.MB10304301-S0020							
ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING						CITY DRAWING NUMBER SHEET 20 OF 42	
PIER 1 DEMOLITION						20	



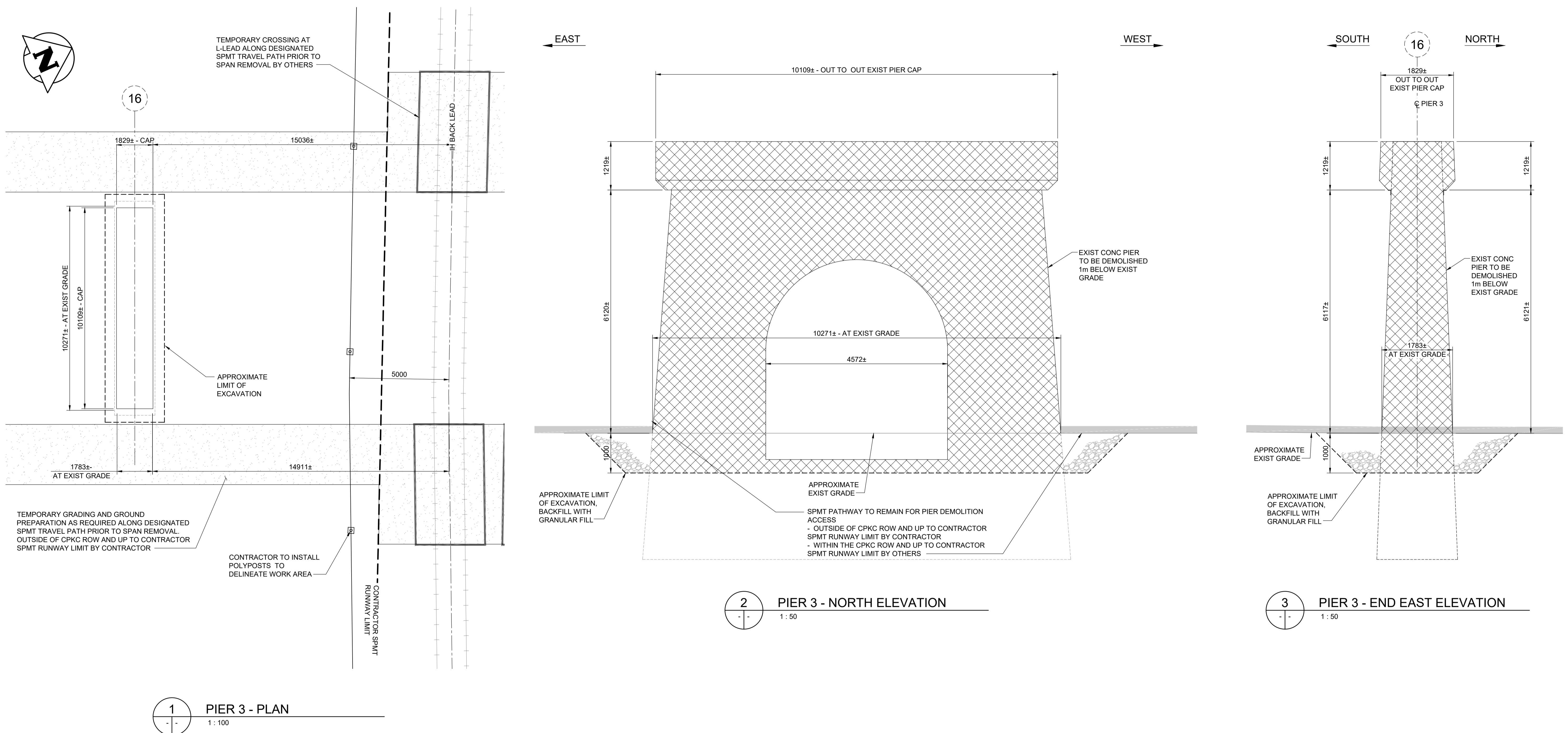
NOTES:

1. ENSURE L-LEAD TRACK IS REMOVED FROM SERVICE PRIOR TO DEMOLITION.
2. ACCESS TO PIER 2 FROM TRINITY STREET.
3. CONCRETE RUBBLE FROM PIER DEMOLITION TO BE REMOVED FROM SITE VIA SPMT PATHWAY AND LOGA AVE.
4. RETURN TO SERVICE L-LEAD TRACK ONCE SITE HAS BE CLEARED OF DEBRIS.

ISSUED FOR  
**INFORMATION  
ONLY**  
NOT TO BE  
USED FOR  
CONSTRUCTION



B.M. ELEV.				 <b>TETRA TECH</b>	PRELIMINARY DRAWING		 <b>THE CITY OF WINNIPEG</b> PUBLIC WORKS DEPARTMENT
DESIGNED BY		ML	REVIEWED BY		MJB		
DRAWN BY		EV	APPROVED BY		MJB		
SCALE:		AS NOTED	ACCEPTED BY		DATE		
0	ISSUED FOR TENDER		25.12.01	MJB	NOT TO BE USED FOR CONSTRUCTION		
NO.	REVISIONS	DATE	BY	DATE	2025.12.01	CONSULTANT DRAWING NO.	704-INF.MBI0304301-S0021
CITY DRAWING NUMBER							
SHEET 21 OF 42							
ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING							
PIER 2 DEMOLITION							
							21



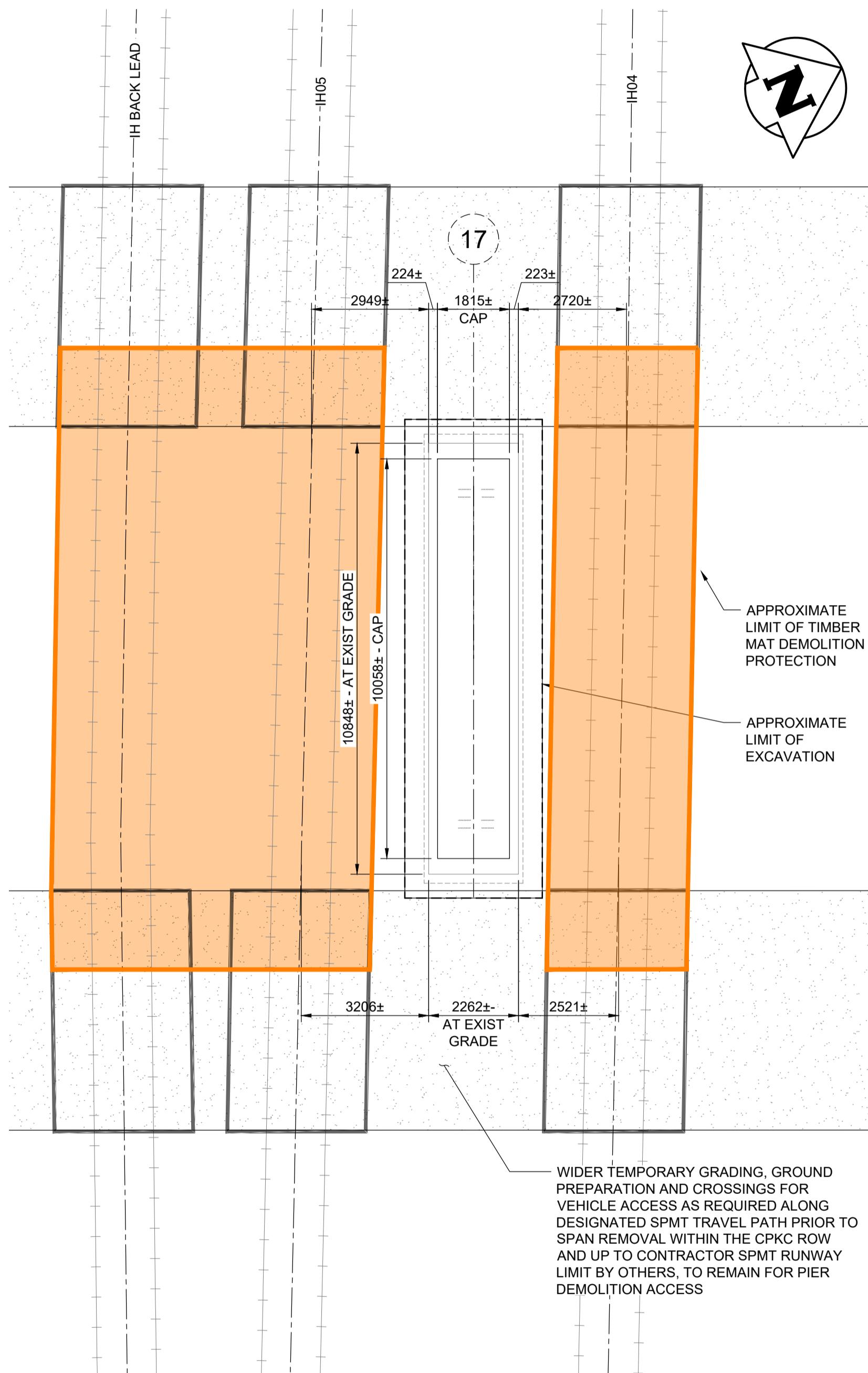
NOTES:

1. ENSURE L-LEAD TRACK IS REMOVED FROM SERVICE PRIOR TO DEMOLITION.
2. ACCESS TO PIER 3 FROM TRINITY STREET.
3. CONCRETE RUBBLE FROM PIER DEMOLITION TO BE REMOVED FROM SITE SPMT PATHWAY AND VIA LOGAN AVE.
4. RETURN TO SERVICE L-LEAD TRACK ONCE SITE HAS BE CLEARED OF DEBRIS.

ISSUED FOR INFORMATION ONLY  
NOT TO BE USED FOR CONSTRUCTION

ENGINEERS GEOSCIENTISTS MANITOBA  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

B.M. ELEV.		TETRA TECH		PRELIMINARY DRAWING NOT TO BE USED FOR CONSTRUCTION		THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT	
DESIGNED BY	ML	REVIEWED BY	MJB	ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING		CITY DRAWING NUMBER	
DRAWN BY	EV	APPROVED BY	MJB	SHEET 22 OF 42			
SCALE:	AS NOTED	ACCEPTED BY	DATE	PIER 3 DEMOLITION			
0 ISSUED FOR TENDER	25.12.01	MJB		704-INF.MB10304301-S0022		22	
NO. REVISIONS	DATE	BY	DATE	2025.12.01			



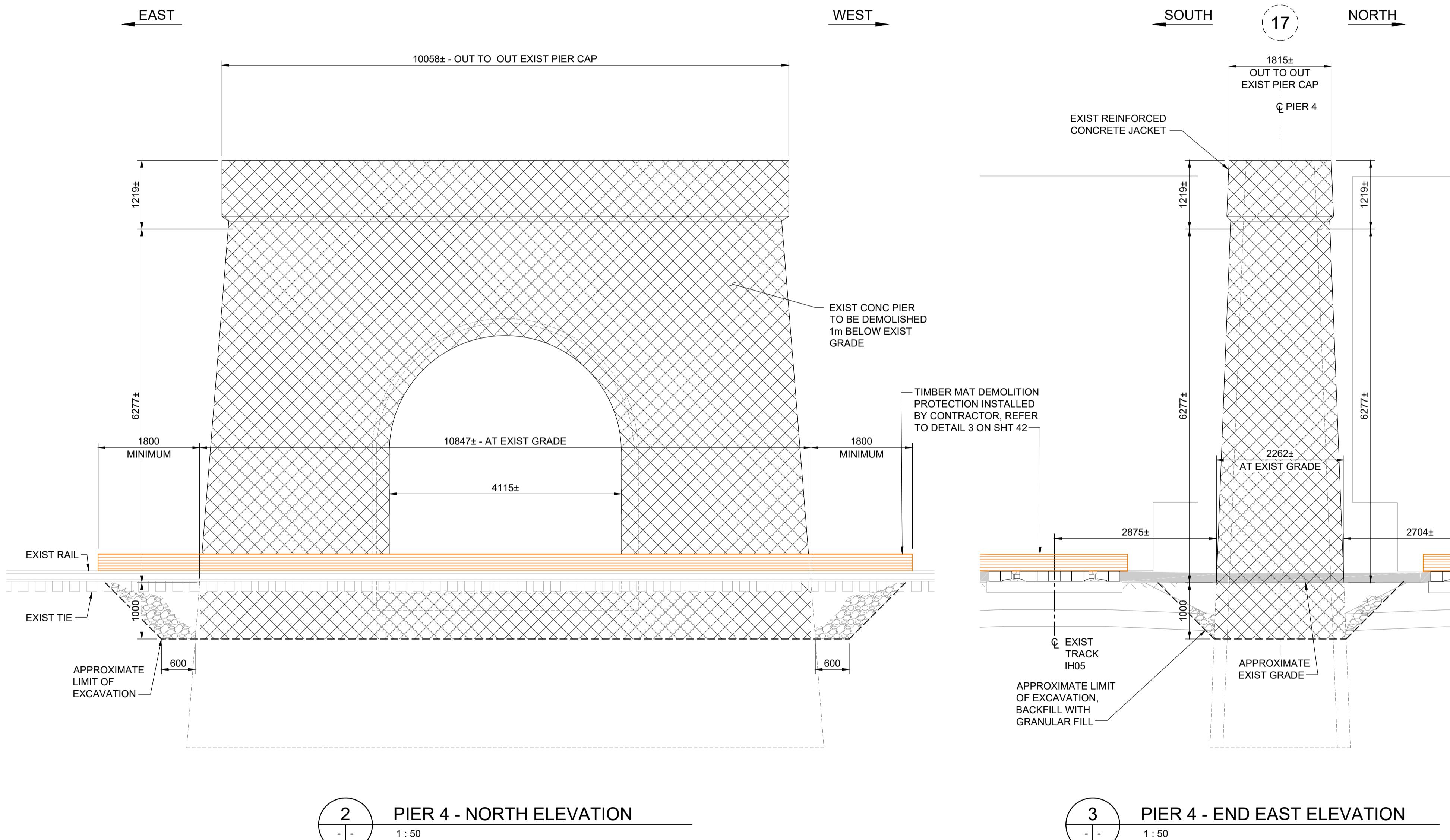
1 PIER 4 - PLAN  
1 : 100

NOTES:

1. ENSURE L-LEAD, IH BACK LEAD, IH05, IH04 TRACKS ARE REMOVED FROM SERVICE PRIOR TO DEMOLITION.
2. ACCESS TO PIER 4 FROM TRINITY STREET AND SPMT PATHWAY.
3. CONCRETE RUBBLE FROM PIER DEMOLITION TO BE REMOVED FROM SITE VIA SPMT PATHWAY AND LOGAN AVE.
4. RETURN TO SERVICE L-LEAD, IH BACK LEAD, IH05, IH04 TRACKS ONCE SITE HAS BE CLEARED OF DEBRIS.

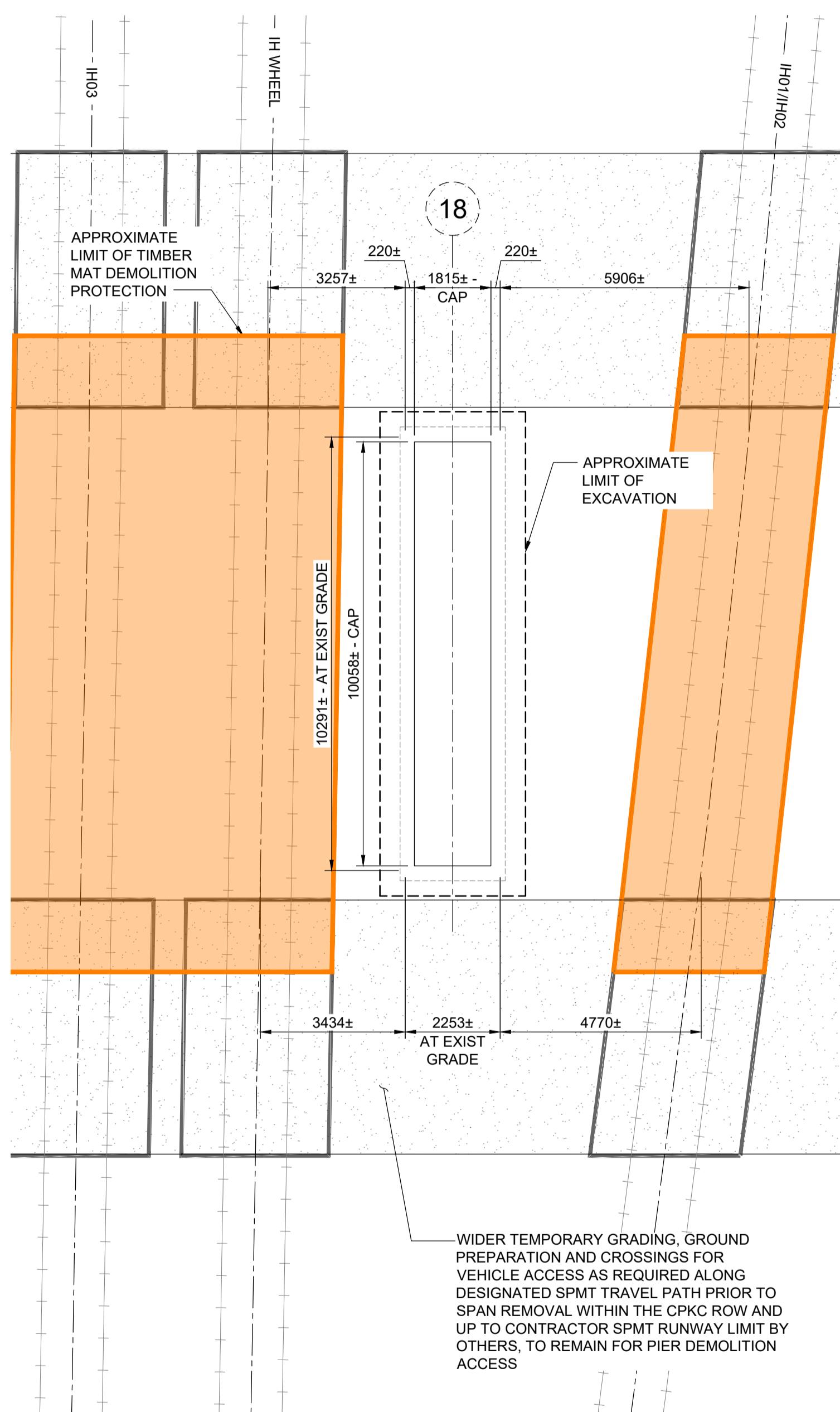
ISSUED FOR  
INFORMATION  
ONLY  
NOT TO BE  
USED FOR  
CONSTRUCTION

**ENGINEERS  
GEOSCIENTISTS  
MANITOBA**  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

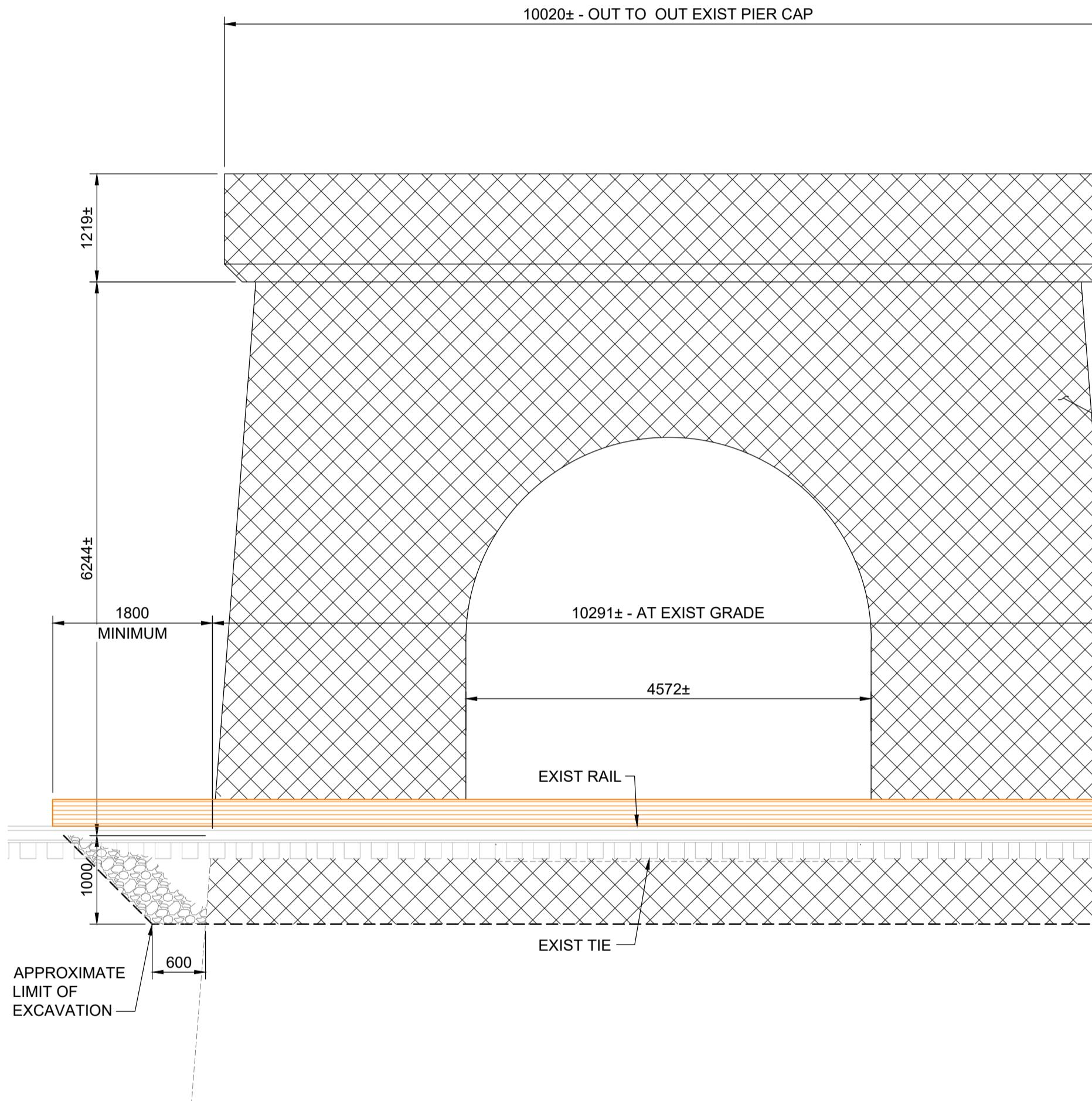


2 PIER 4 - NORTH ELEVATION  
1 : 50

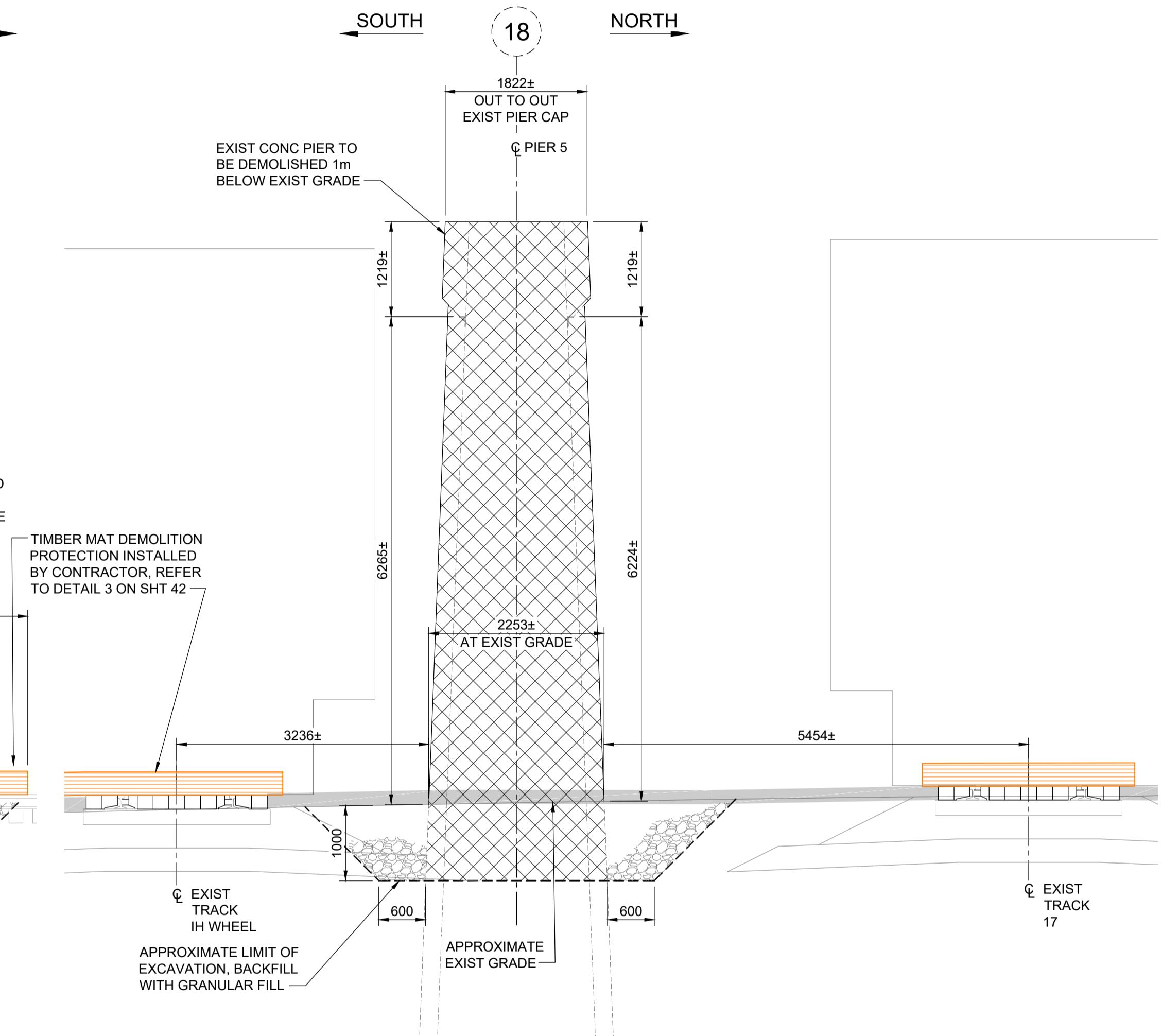
B.M. ELEV.		TETRA TECH		PRELIMINARY DRAWING NOT TO BE USED FOR CONSTRUCTION		THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT	
DESIGNED BY	ML	REVIEWED BY	MJB				
DRAWN BY	EV	APPROVED BY	MJB				
SCALE:	AS NOTED	ACCEPTED BY	DATE				
0 ISSUED FOR TENDER	25.12.01	MJB					
NO. REVISIONS	DATE	BY	DATE	2025.12.01			
CONSULTANT DRAWING NO.				704-INF.MB10304301-S0023			
ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING						CITY DRAWING NUMBER SHEET 23 OF 42	
PIER 4 DEMOLITION						23	



EAST



WEST

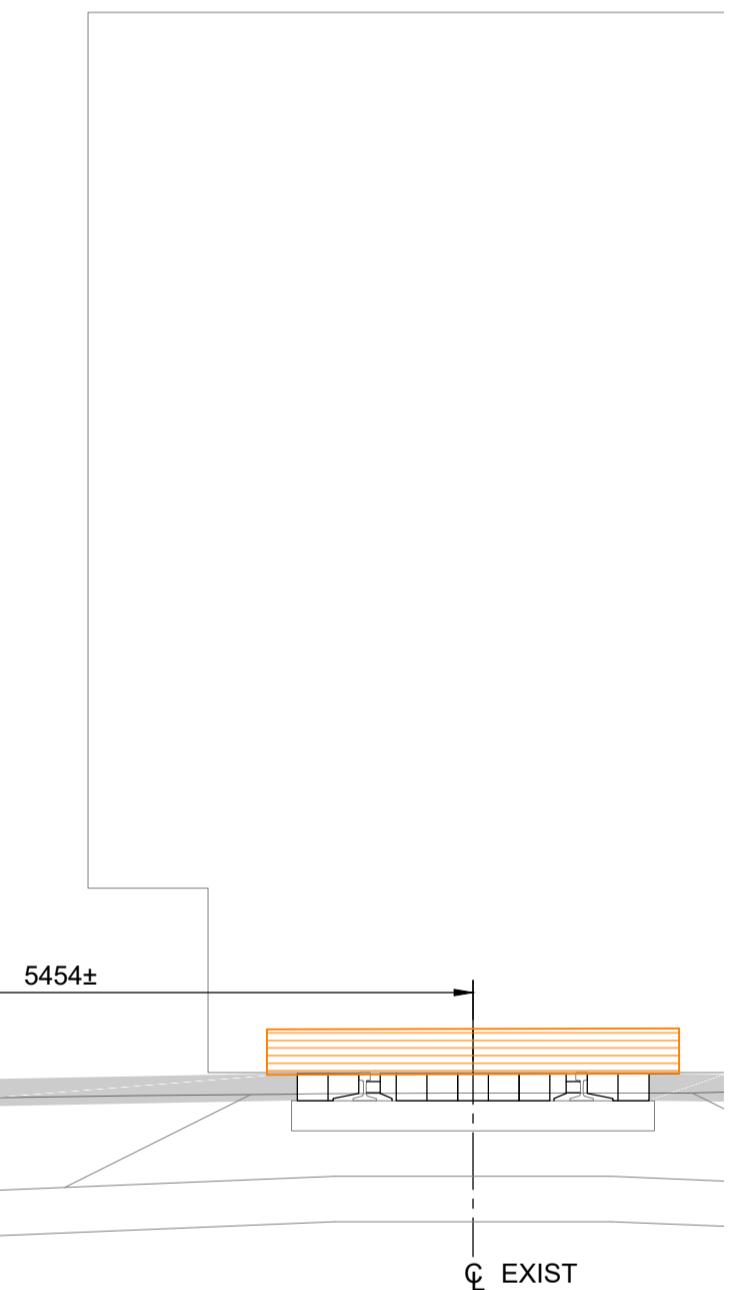


SOUTH

18

C PIER 5

NORTH



ISSUED FOR  
INFORMATION  
ONLY  
NOT TO BE  
USED FOR  
CONSTRUCTION

**ENGINEERS  
GEOSCIENTISTS**  
MANITOBA  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

B.M. ELEV.		TETRA TECH		PRELIMINARY DRAWING	
DESIGNED BY	ML	REVIEWED BY	MJB	NOT TO BE USED FOR CONSTRUCTION	
DRAWN BY	EV	APPROVED BY	MJB		
SCALE:	AS NOTED	ACCEPTED BY	DATE		
0 ISSUED FOR TENDER	25.12.01	MJB			
NO. REVISIONS	DATE	BY	DATE	2025.12.01	
				CONSULTANT DRAWING NO.	
				704-INF.MB10304301-S0024	

**THE CITY OF WINNIPEG**  
PUBLIC WORKS DEPARTMENT

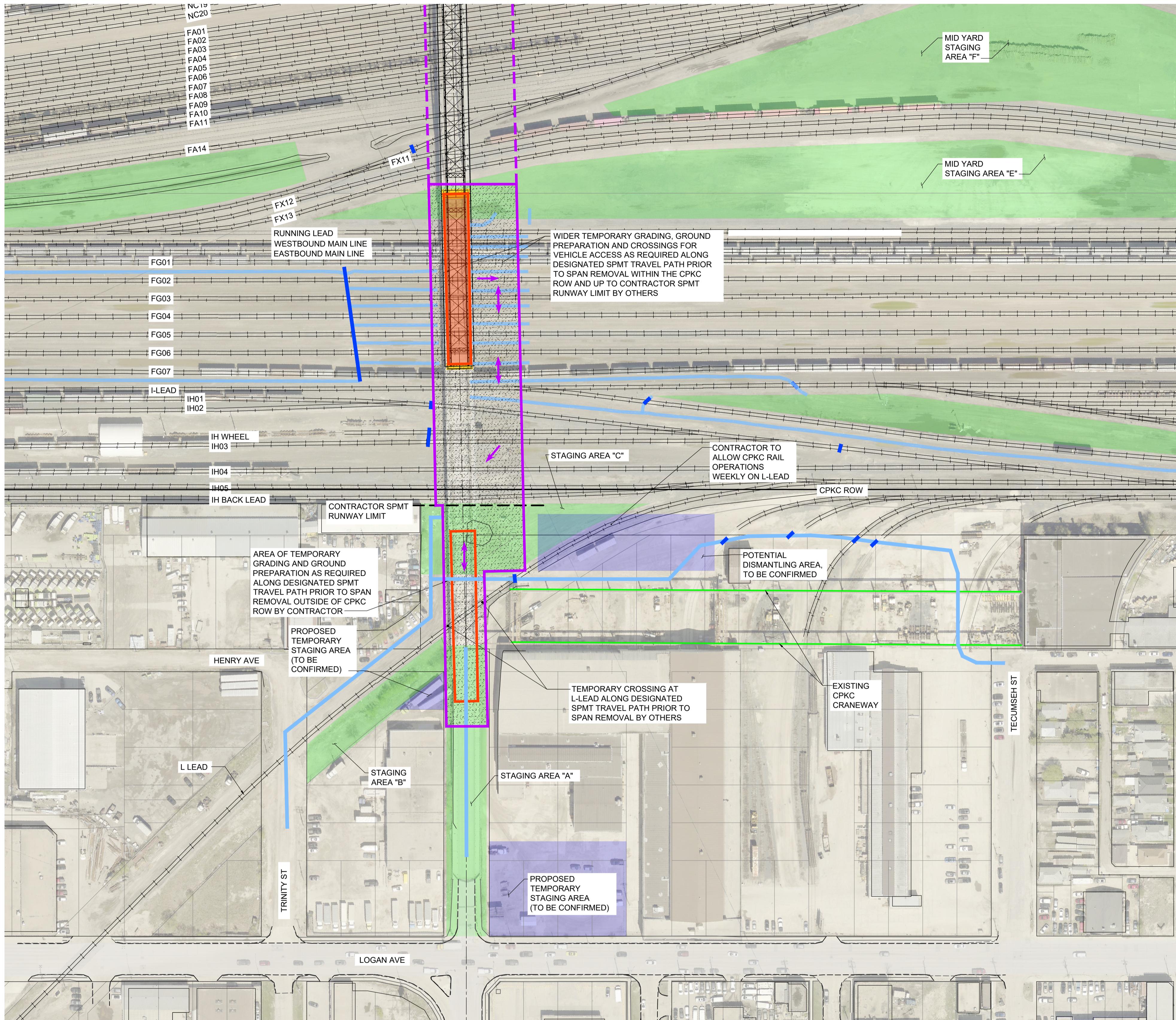
 Winnipeg

ARLINGTON BRIDGE  
PHASE 4 - DECOMMISSIONING

CITY DRAWING NUMBER  
SHEET 24 OF 42

PIER 5 DEMOLITION

24



1 SPAN 19 ACCESS PLAN  
1:1000

ISSUED FOR  
INFORMATION  
ONLY  
NOT TO BE  
USED FOR  
CONSTRUCTION

**ENGINEERS  
GEOSCIENTISTS**  
MANITOBA  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

#### SEQUENCE OF WORK

##### PRE-WORKBLOCK DEMOLITION WORKS:

- PERFORM LOCALIZED TRUSS MODIFICATIONS/REMOVALS TO FACILITATE TRUSS REMOVAL
- ERECT AND SETUP SELF-PROPELLED MODULAR TRANSPORTS (SPMT)
- PRIOR TO SPAN REMOVAL WORKBLOCK, ALL TRAINS SHALL BE REMOVED WITHIN SPMT TRAVEL PATH
- EXTEND TEMPORARY GRADING, GROUND PREPARATION AND CROSSINGS FROM ALONG DESIGNATED SPMT TRAVEL PATH

##### TRUSS REMOVAL WORKS:

- ENSURE RUNNING LEAD, WESTBOUND MAINLINE, EASTBOUND MAINLINE, FG01, FG02, FG03, FG04, FG05, FG06 AND FG07 TRACKS ARE REMOVED FROM SERVICE
- FG07, I-LEAD, IH01, IH WHEEL, IH03, IH04, IH05, IH BACKLEAD AND L-LEAD TRACKS TO BE UNDER WORKBLOCKS AS REQUIRED DURING SPMT MOVEMENT TO FINAL SPOT IN FRONT STAGING AREA "C"
- EXISTING PIER 1, 2, 3, 4 AND 5 TO BE DEMOLISHED PRIOR TO SPAN 19 REMOVAL
- POSITION SPMT TO SPAN 19
- LIFT AND MOVE SPAN FROM BRIDGE LOCATION TO IN FRONT OF STAGING AREA "C"
- LOWER SPAN ONTO GROUND SURFACE
- RETURN SERVICE TO THE RUNNING LEAD, WESTBOUND MAINLINE, EASTBOUND MAINLINE, FG01, FG02, FG03, FG04, FG05, FG06, FG07 AND I-LEAD TRACKS ONCE SPMT CLEARS I-LEAD LEAD RIGHT OF WAY
- RETURN SERVICE TO THE IH01, IH03, IH WHEEL, IH04, IH05, IH BACKLEAD AND L-LEAD TRACKS ONCE SPAN 19 DEMOLITION HAS CLEARED THAT TRACKS RIGHT OF WAYS

##### DURING WORKBLOCK DEMOLITION WORKS:

- DISMANTLE/DEMOLISH TRUSS COMPONENTS ON GROUND, WHILE SIMULTANEOUSLY HAULING THEM OFF SITE
- HAULING EQUIPMENT TO REMOVE TRUSS COMPONENTS FROM STAGING AREAS "C" VIA LOGAN AVE
- TRACK BLOCKS AND SERVICE REINSTATEMENTS CAN BE STAGED TO REDUCE OVERALL OUT OF SERVICE IMPACTS
- ONCE SPMT GETS TO SPAN 19, IH BACK LEAD TO IH01 CAN BE RETURNED TO SERVICE (UPON CPKC DIRECTION) UNTIL SPMT IS READY TO MOVE
- ONCE SPMT STARTS MOVING, TRACKS THAT HAVE BEEN CLEARED CAN BE PUT BACK IN SERVICE IN A PHASED APPROACH

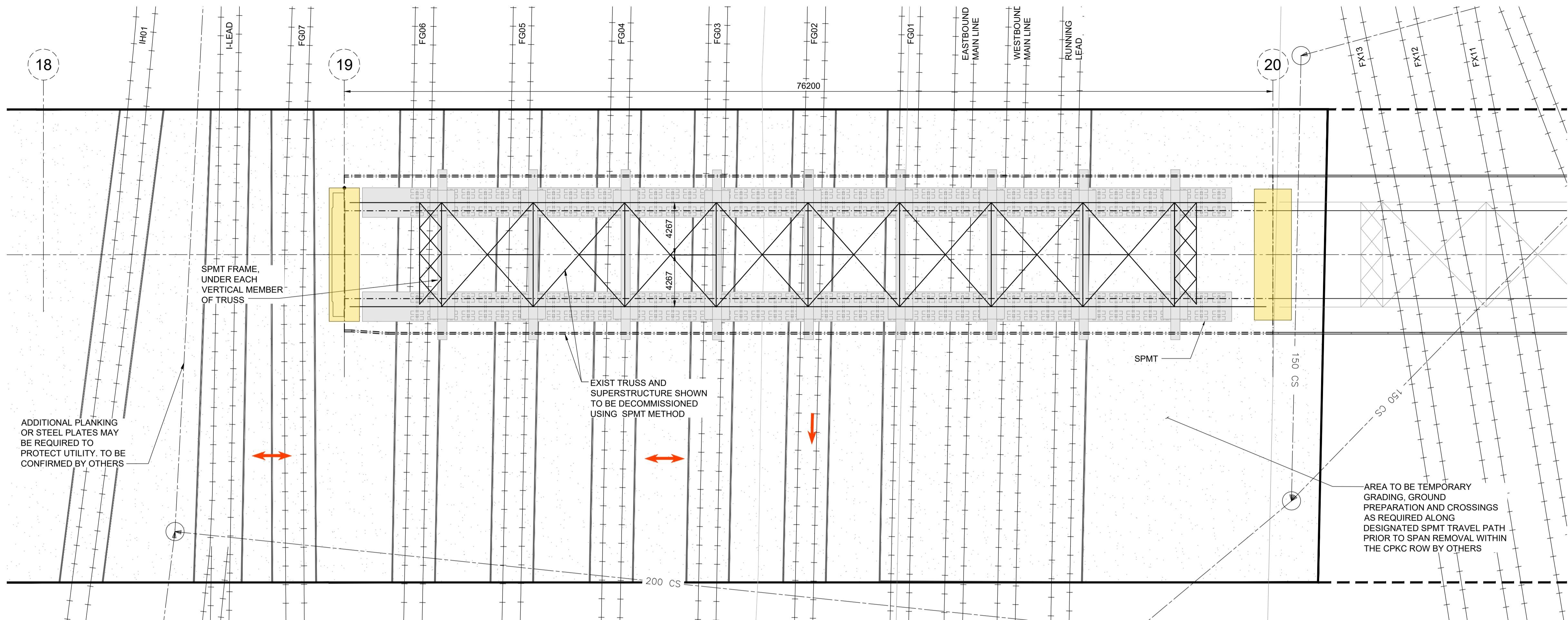
##### POST-WORKBLOCK DEMOLITION WORKS:

- REMOVE TEMPORARY GRADING AND CROSSINGS FOR SPMT RUNWAY. CLEAN AND RESTORE GROUND SURFACE TO PRE-DEMOLITION STATE AS REQUIRED

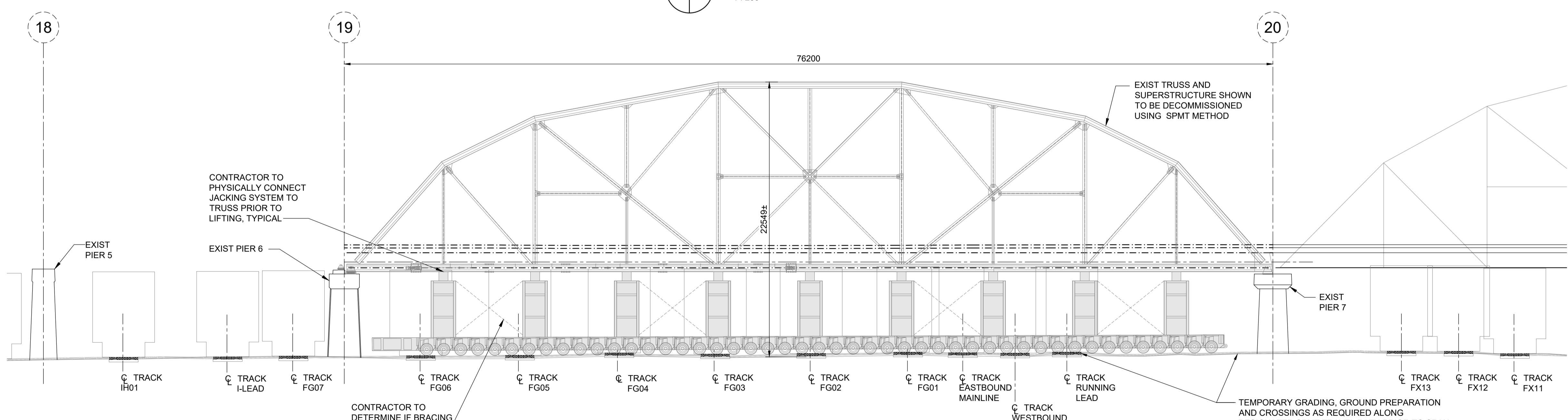
#### LEGEND:

- EXISTING PIER LOCATIONS
- ACCESS ROUTE FOR SPAN 19
- EXISTING AT GRADE CROSSING
- CONSTRUCTION STAGING AREA
- OVERALL WORK AREA
- SPMT TRAVEL PATH AREA
- SPMT TRAVEL PATH AREA NEXT SPAN

B.M. ELEV.		<b>TETRA TECH</b>		PRELIMINARY DRAWING NOT TO BE USED FOR CONSTRUCTION		THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT	
DESIGNED BY	ML	REVIEWED BY	MJB				
DRAWN BY	EV	APPROVED BY	MJB				
SCALE:	AS NOTED	ACCEPTED BY	DATE				
0 ISSUED FOR TENDER	25.12.01	MJB					
NO. REVISIONS	DATE	BY	DATE	2025.12.01			
CONSULTANT DRAWING NO. 704-INF.MB0304301-S0025				CITY DRAWING NUMBER 25 OF 42			
SPAN 19 ACCESS PLAN SPMT REMOVAL OPTION				25			



1 SPAN 19 - TRUSS PLAN  
1 : 200



2 SPAN 19 - EAST ELEVATION  
1 : 200

LEGEND:

- EXISTING PIER LOCATIONS
- SPMT TRAVEL PATH AREA
- SPMT TRAVEL PATH AREA NEXT SPAN

NOTE: THE STRUCTURAL CAPACITY OF SPAN 19 HAS NOT BEEN DEEMED ADEQUATE FOR REMOVAL VIA SPMT METHOD AS SHOWN

SPAN 19 TRUSS ESTIMATED MASS: 800,000 kg

ISSUED FOR INFORMATION ONLY  
NOT TO BE USED FOR CONSTRUCTION

ENGINERS  
GEOSCIENTISTS  
MANITOBA  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

B.M. ELEV.		TETRA TECH	
DESIGNED BY	ML	REVIEWED BY	MJB
DRAWN BY	EV	APPROVED BY	MJB
SCALE:	AS NOTED	ACCEPTED BY	DATE
0 ISSUED FOR TENDER	25.12.01	MJB	
NO. REVISIONS	DATE	BY	DATE 2025.12.01

PRELIMINARY DRAWING  
NOT TO BE USED FOR CONSTRUCTION  
CONSULTANT DRAWING NO.  
704-INF.MB10304301-S0026



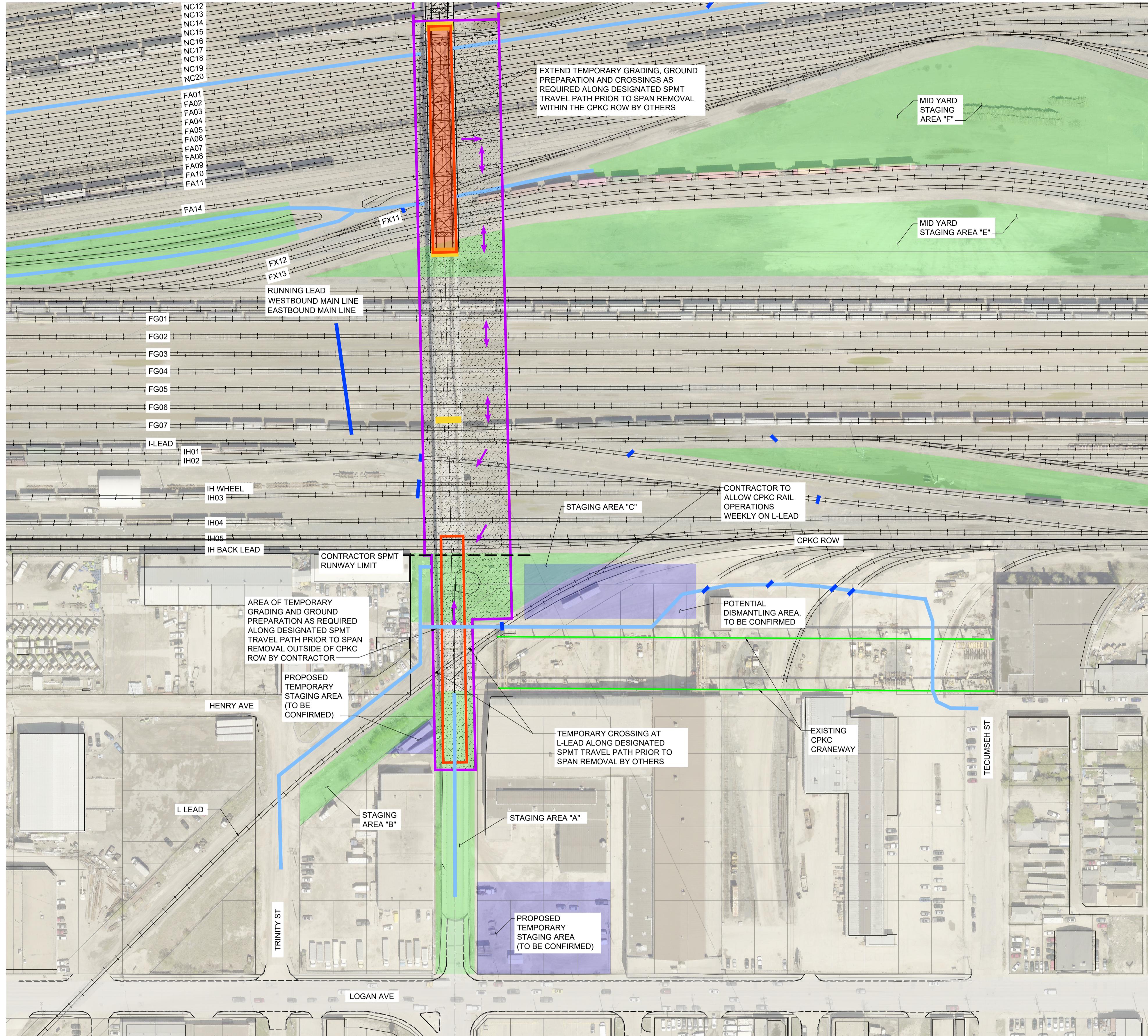
THE CITY OF WINNIPEG  
PUBLIC WORKS DEPARTMENT

ARLINGTON BRIDGE  
PHASE 4 - DECOMMISSIONING

CITY DRAWING NUMBER  
SHEET 26 OF 42

SPAN 19  
SPMT REMOVAL OPTION

26



1 SPAN 20 ACCESS PLAN  
1:1000

ISSUED FOR  
INFORMATION  
ONLY  
NOT TO BE  
USED FOR  
CONSTRUCTION

ENGINEERS  
GEOSCIENTISTS  
MANITOBA  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

B.M. ELEV.		TETRA TECH	
DESIGNED BY	ML	REVIEWED BY	MJB
DRAWN BY	EV	APPROVED BY	MJB
SCALE:	AS NOTED	ACCEPTED BY	DATE
0 ISSUED FOR TENDER	25.12.01	MJB	
NO. REVISIONS	DATE	BY	DATE 2025.12.01

#### SEQUENCE OF WORK

##### PRE-WORKBLOCK DEMOLITION WORKS:

- PERFORM LOCALIZED TRUSS MODIFICATIONS/REMOVALS TO FACILITATE TRUSS REMOVAL
- ERECT AND SETUP SELF-PROPELLED MODULAR TRANSPORTS (SPMT)
- PRIOR TO SPAN REMOVAL WORKBLOCK, ALL TRAINS SHALL BE REMOVED WITHIN SPMT TRAVEL PATH
- EXTEND TEMPORARY GRADING AND CROSSINGS ALONG DESIGNATED SPMT TRAVEL PATH

##### TRUSS REMOVAL WORKS:

- ENSURE NC18, NC19, NC20, FA01, FA02, FA03, FA04, FA05, FA06, FA07, FA08, FA09, FA10, FA11, FA14, FX11, FX12, FX13, RUNNING LEAD, WESTBOUND MAIN LINE, EASTBOUND MAIN LINE, FG01, FG02, FG03, FG04, FG05, FG06, FG07, IH01, IH WHEEL, IH03, IH04, IH05, IH BACKLEAD AND L-LEAD TRACKS TO BE UNDER WORKBLOCKS AS REQUIRED DURING SPMT MOVEMENT TO FINAL SPOT IN FRONT STAGING AREA "C"
- POSITION SPMT TO SPAN 20
- LIFT AND MOVE SPAN FROM BRIDGE LOCATION TO IN FRONT OF STAGING AREA "C"
- LOWER SPAN ONTO GROUND SURFACE
- RETURN SERVICE TO THE NC18, NC19, NC20, FA01, FA02, FA03, FA04, FA05, FA06, FA07, FA08, FA09, FA10, FA11, FA14, FX11, FX12, FX13, RUNNING LEAD, WESTBOUND MAIN LINE, EASTBOUND MAIN LINE, FG01, FG02, FG03, FG04, FG05, FG06, FG07, I-LEAD, IH01, IH WHEEL, IH03 AND IH04 ONCE SPMT CLEARS IH04 LEAD RIGHT OF WAY
- RETURN SERVICE TO THE IH05, IH BACKLEAD AND L-LEAD TRACK ONCE SPAN 20 DEMOLITION IS CLEARS THAT TRACKS RIGHT OF WAYS

##### DURING WORKBLOCK DEMOLITION WORKS:

- DISMANTLE/DEMOLISH TRUSS COMPONENTS ON GROUND, WHILE SIMULTANEOUSLY HAULING THEM OFF SITE
- HAULING EQUIPMENT TO REMOVE TRUSS COMPONENTS FROM STAGING AREAS "C" VIA LOGAN AVE
- TRACK BLOCKS AND SERVICE REINSTATEMENTS CAN BE STAGED TO REDUCE OVERALL OUT OF SERVICE IMPACTS
- ONCE SPMT GETS TO SPAN 20, IH BACK LEAD TO RUNNING LEAD CAN BE RETURNED TO SERVICE (UPON CPKC DIRECTION) UNTIL SPMT IS READY TO MOVE
- ONCE SPMT STARTS MOVING, TRACKS THAT HAVE BEEN CLEARED CAN BE PUT BACK IN SERVICE IN A PHASED APPROACH

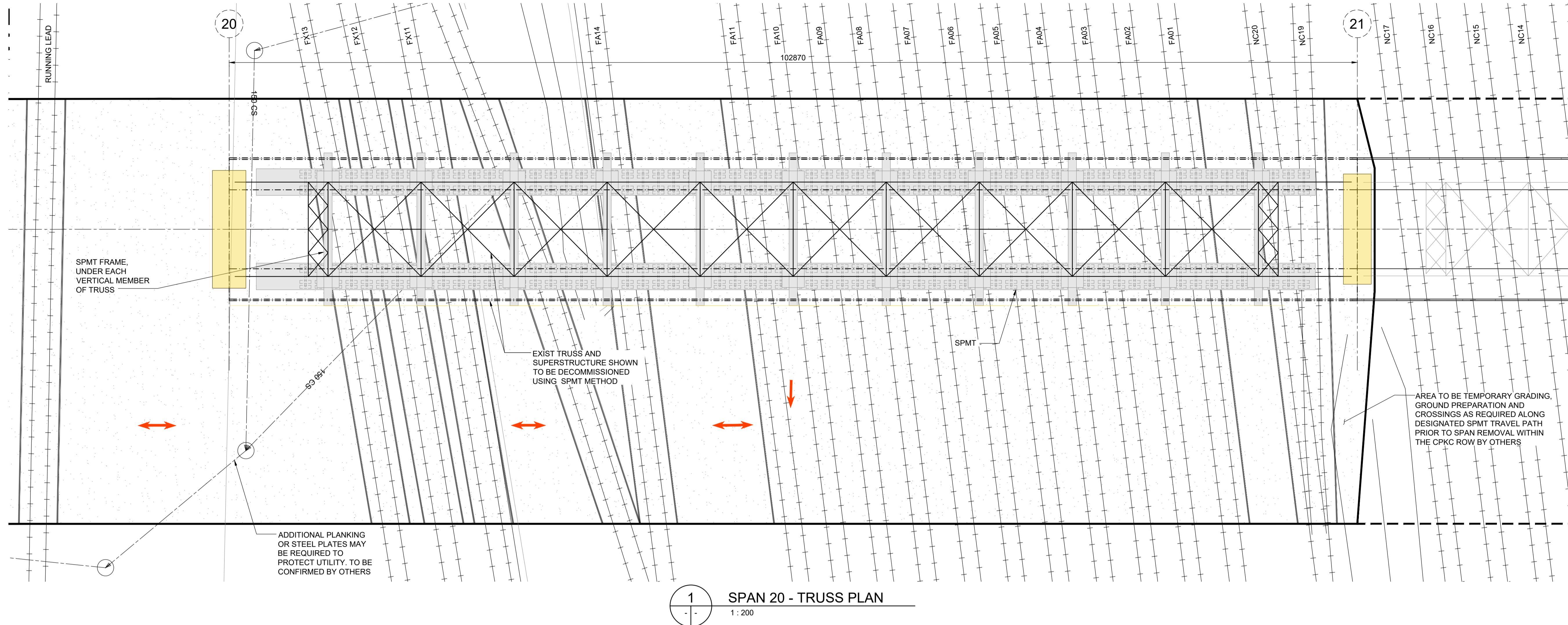
##### POST-WORKBLOCK DEMOLITION WORKS:

- REMOVE TEMPORARY GRADING AND CROSSINGS FOR SPMT RUNWAY. CLEAN AND RESTORE GROUND SURFACE TO PRE-DEMOLITION STATE AS REQUIRED

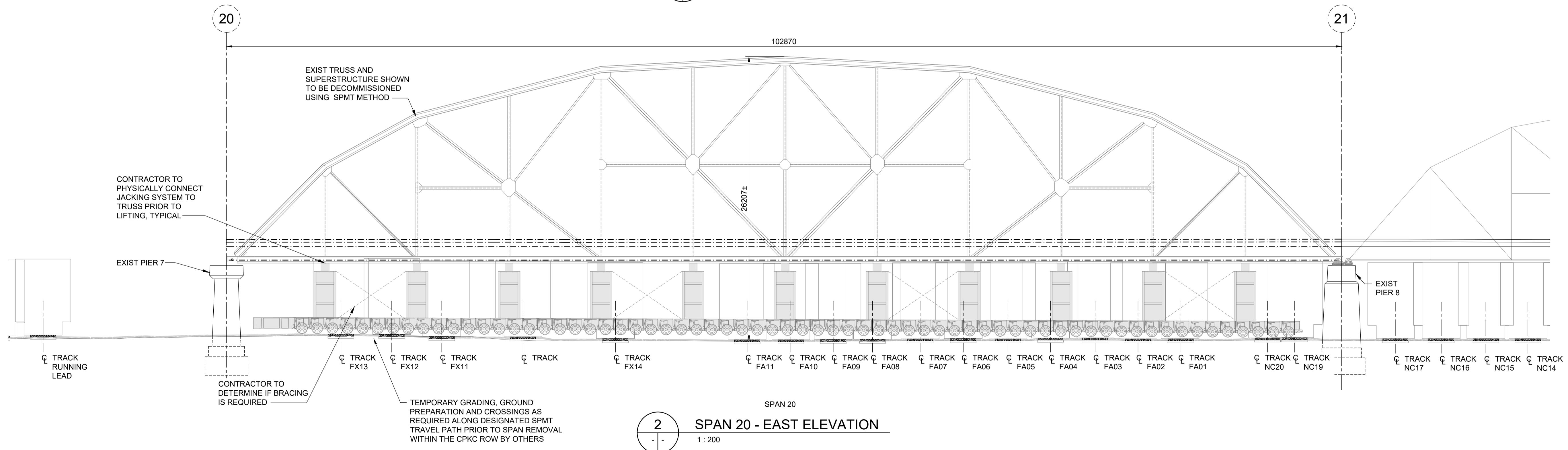
#### LEGEND:

- EXISTING PIER LOCATIONS
- ACCESS ROUTE FOR SPAN 20
- EXISTING AT GRADE CROSSING
- CONSTRUCTION STAGING AREA
- OVERALL WORK AREA
- SPMT TRAVEL PATH
- SPMT TRAVEL PATH AREA NEXT SPAN

**THE CITY OF WINNIPEG**  
PUBLIC WORKS DEPARTMENT  
ARLINGTON BRIDGE  
PHASE 4 - DECOMMISSIONING  
SPAN 20 ACCESS PLAN  
SPMT REMOVAL OPTION  
CONSULTANT DRAWING NO.  
704-INF.MB0304301-S0027  
CITY DRAWING NUMBER  
SHEET 27 OF 42  
27



1 SPAN 20 - TRUSS PLAN  
1:200



2 SPAN 20 - EAST ELEVATION  
1:200

LEGEND:

- EXISTING PIER LOCATIONS
- SPMT TRAVEL PATH AREA
- SPMT TRAVEL PATH AREA NEXT SPAN

NOTE:  
THE STRUCTURAL CAPACITY OF SPAN 20 HAS NOT  
BEEN DEEMED ADEQUATE FOR REMOVAL VIA SPMT  
METHOD AS SHOWN

SPAN 20 TRUSS ESTIMATED  
MASS: 1,400,000 kg

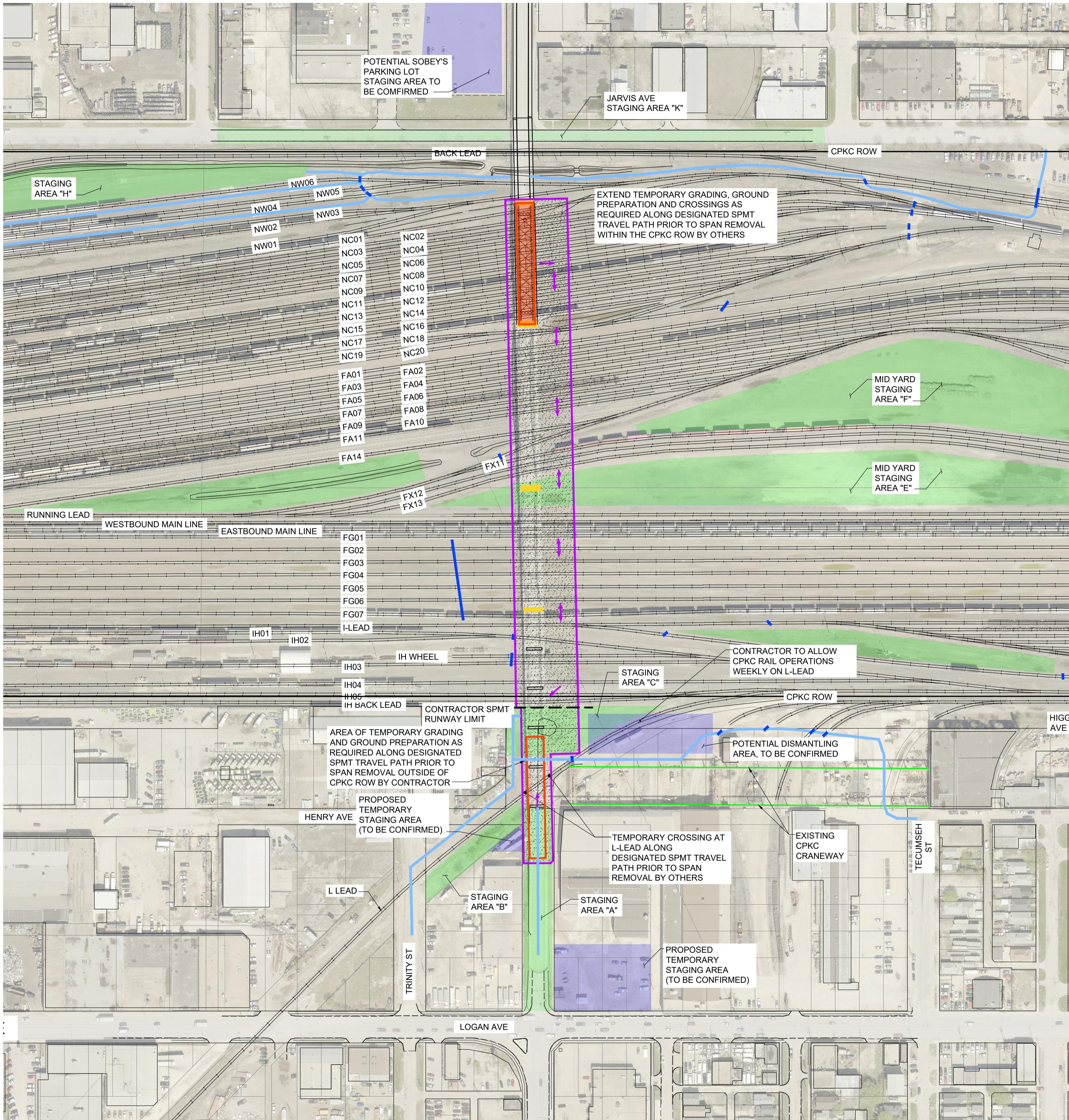
ISSUED FOR  
INFORMATION  
ONLY  
NOT TO BE  
USED FOR  
CONSTRUCTION

**ENGINEERS  
GEOSCIENTISTS  
MANITOBA**  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

B.M. ELEV.		TETRA TECH	
DESIGNED BY	ML	REVIEWED BY	MJB
DRAWN BY	EV	APPROVED BY	MJB
SCALE:	AS NOTED	ACCEPTED BY	DATE
0 ISSUED FOR TENDER	25.12.01	MJB	
NO. REVISIONS	DATE	BY	DATE 2025.12.01

PRELIMINARY  
DRAWING  
NOT TO BE  
USED FOR  
CONSTRUCTION  
CONSULTANT DRAWING NO.  
704-INF.MB0304301-S0028

**THE CITY OF WINNIPEG**  
PUBLIC WORKS DEPARTMENT  
ARLINGTON BRIDGE  
PHASE 4 - DECOMMISSIONING  
SPAN 20  
SPMT REMOVAL OPTION  
CITY DRAWING NUMBER  
SHEET 28 OF 42  
28



1 SPAN 21 ACCESS PLAN  
1:1250

ISSUED FOR  
INFORMATION  
ONLY  
NOT TO BE  
USED FOR  
CONSTRUCTION

ENGINERS  
GEOSCIENTISTS  
MANITOBA  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

B.M. ELEV.		TETRA TECH	
DESIGNED BY	ML	REVIEWED BY	MJB
DRAWN BY	EV	APPROVED BY	MJB
SCALE:	AS NOTED	ACCEPTED BY	DATE
0 ISSUED FOR TENDER	25.12.01	MJB	
NO. REVISIONS	DATE	BY	DATE 2025.12.01

#### SEQUENCE OF WORK

##### PRE-WORKBLOCK DEMOLITION WORKS:

- PERFORM LOCALIZED TRUSS MODIFICATIONS/REMOVALS TO FACILITATE TRUSS REMOVAL
- ERECT AND SETUP SELF-PROPELLED MODULAR TRANSPORTS (SPMT)
- PRIOR TO SPAN REMOVAL WORKBLOCK, ALL TRAINS SHALL BE REMOVED WITHIN SPMT TRAVEL PATH
- EXTEND TEMPORARY GRADING AND CROSSINGS ALONG DESIGNATED SPMT TRAVEL PATH

##### TRUSS REMOVAL WORKS:

- ENSURE NC01, NC02, NC03, NC04, NC05, NC06, NC07, NC08, NC09, NC10, NC11, NC12, NC13, NC14, NC15, NC16, NC17, NC19, NC 20 AND NW01 TRACKS ARE REMOVED FROM SERVICE
- FA01, FA02, FA03, FA04, FA05, FA06, FA07, FA08, FA09, FA10, FA11, FA14, FX11, FX12, FX13, RUNNING LEAD, WESTBOUND MAINLINE, EASTBOUND MAINLINE, FG01, FG02, FG03, FG04, FG05, FG06, FG07, I-LEAD, IH01, IH02, IH WHEEL, IH03, IH04, IH05, IH BACKLEAD AND L-LEAD TRACKS TO BE UNDER WORKBLOCKS AS REQUIRED DURING SPMT MOVEMENT TO FINAL SPOT IN FRONT STAGING AREA "C"
- POSITION SPMT TO SPAN 21
- LIFT AND MOVE SPAN FROM BRIDGE LOCATION TO INFRONT OF STAGING AREA "C"
- LOWER SPAN ONTO GROUND SURFACE
- RETURN SERVICE TO THE NW01, NC01, NC02, NC03, NC04, NC05, NC06, NC07, NC08, NC09, NC10, NC11, NC12, NC13, NC14, NC15, NC16, NC17, NC19, NC 20, FA01, FA02, FA03, FA04, FA05, FA06, FA07, FA08, FA09, FA10, FA11, FA14, FX11, FX12, FX13, RUNNING LEAD, WESTBOUND MAINLINE, EASTBOUND MAINLINE, FG01, FG02, FG03, FG04, FG05, FG06, FG07, I-LEAD, IH01, IH02, IH WHEEL, IH03, IH04, IH05, AND IH BACKLEAD TRACKS ONCE SPMT CLEARS IH BACKLEAD RIGHT OF WAY

##### DURING WORKBLOCK DEMOLITION WORKS:

- DISMANTLE/DEMOLISH TRUSS COMPONENTS ON GROUND, WHILE SIMULTANEOUSLY HAULING THEM OFF SITE
- HAULING EQUIPMENT TO REMOVE TRUSS COMPONENTS FROM STAGING AREAS "C" VIA LOGAN AVE
- TRACK BLOCKS AND SERVICE REINSTATEMENTS CAN BE STAGED TO REDUCE OVERALL OUT OF SERVICE IMPACTS
- ONCE SPMT GETS TO SPAN 21, IH BACK LEAD TO NC20 CAN BE RETURNED TO SERVICE (UPON CPKC DIRECTION) UNTIL SPMT IS READY TO MOVE
- ONCE SPMT STARTS MOVING, TRACKS THAT HAVE BEEN CLEARED CAN BE PUT BACK IN SERVICE IN A PHASED APPROACH

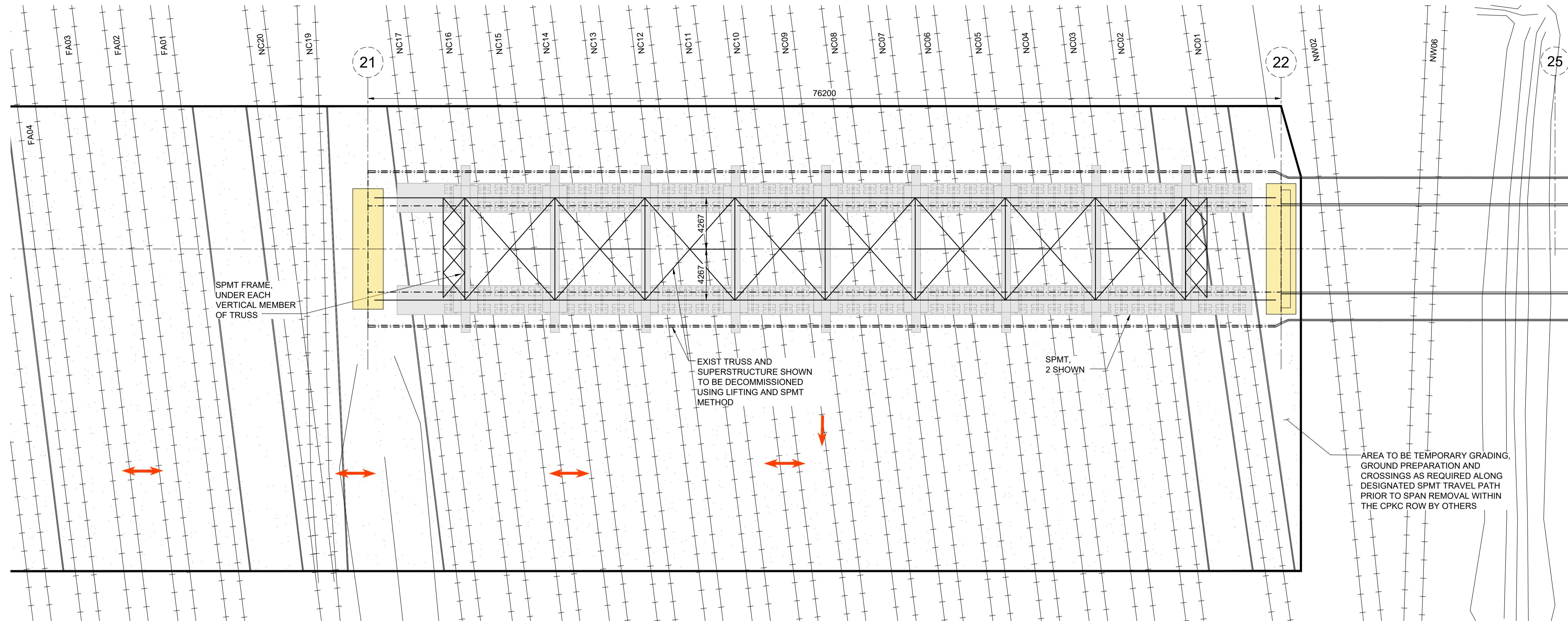
##### POST-WORKBLOCK DEMOLITION WORKS:

- REMOVE TEMPORARY GRADING AND CROSSINGS FOR SPMT RUNWAY. CLEAN AND RESTORE GROUND SURFACE TO PRE-DEMOLITION STATE AS REQUIRED

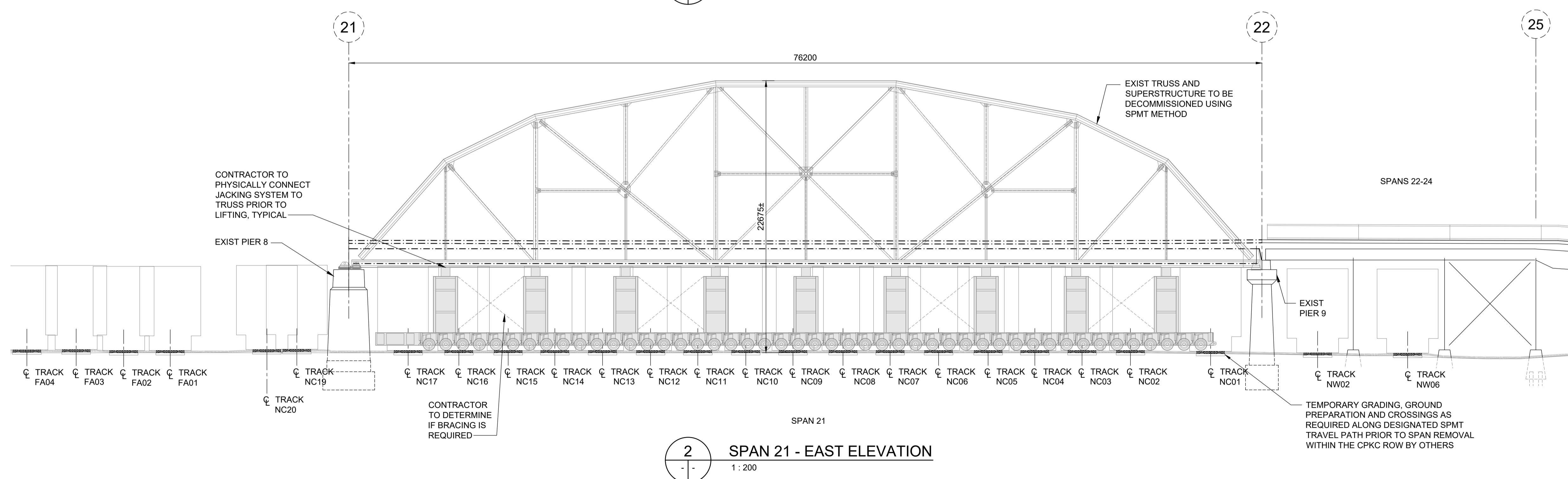
#### LEGEND:

- EXISTING PIER LOCATIONS
- EXISTING PIERS TO BE DEMOLISHED
- ACCESS ROUTE FOR SPAN 21
- EXISTING AT GRADE CROSSING
- CONSTRUCTION STAGING AREA
- OVERALL WORK AREA
- SPMT TRAVEL PATH

**THE CITY OF WINNIPEG**  
PUBLIC WORKS DEPARTMENT  
ARLINGTON BRIDGE  
PHASE 4 - DECOMMISSIONING  
SPAN 21 ACCESS PLAN  
SPMT REMOVAL OPTION  
SHEET 29 OF 42



1 SPAN 21 - TRUSS PLAN  
1 : 200



LEGEND:

- EXISTING PIER LOCATIONS
- SPMT TRAVEL PATH AREA

NOTE:  
THE STRUCTURAL CAPACITY OF SPAN 21 HAS NOT  
BEEN DEEMED ADEQUATE FOR REMOVAL VIA SPMT  
METHOD AS SHOWN

SPAN 21 TRUSS ESTIMATED  
MASS: 800,000 kg

ISSUED FOR  
INFORMATION  
ONLY  
NOT TO BE  
USED FOR  
CONSTRUCTION

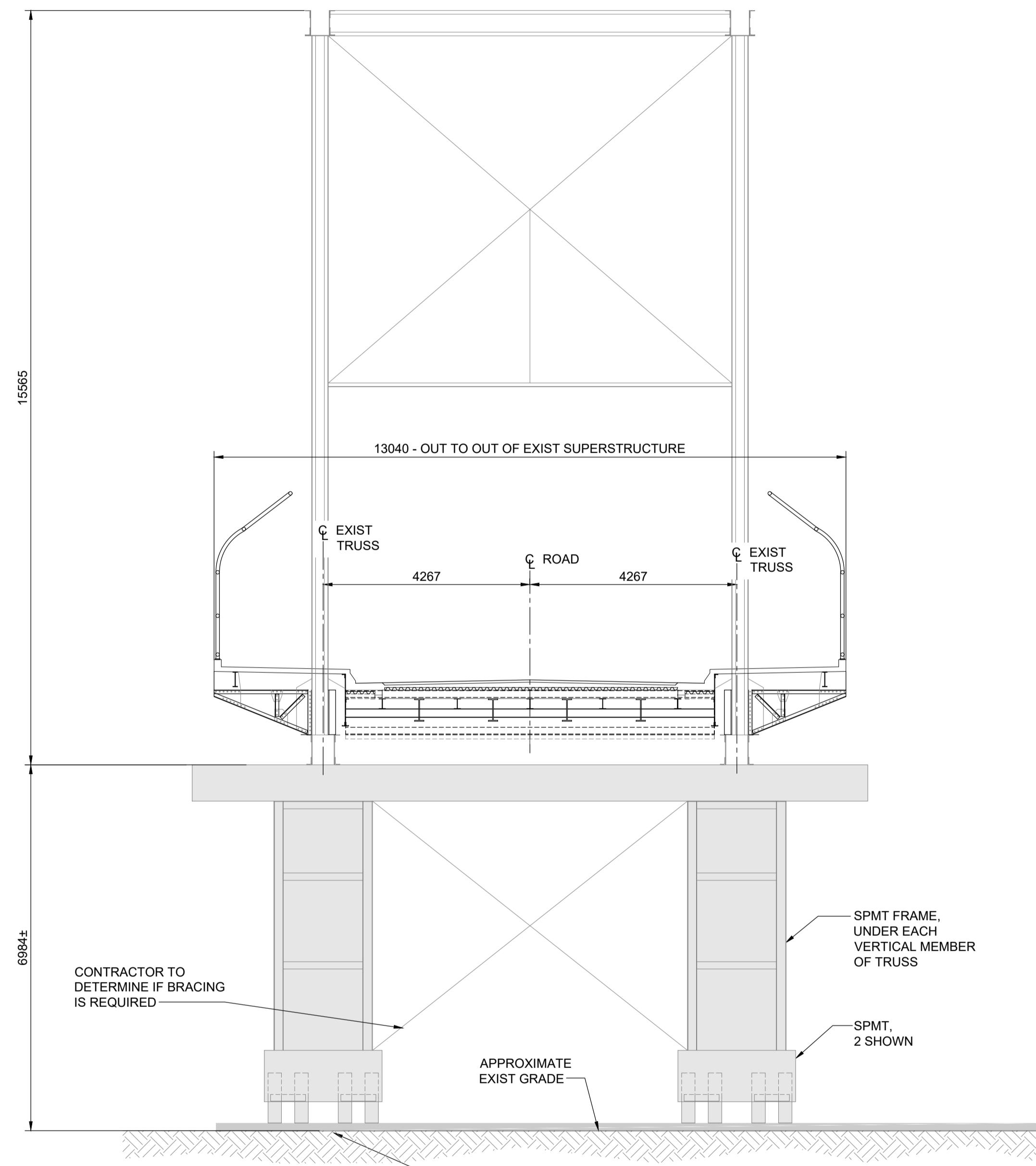
**TETRA TECH**  
ENGINEERS  
GEOSCIENTISTS  
MANITOBA  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

B.M. ELEV.		TETRA TECH	
DESIGNED BY	ML	REVIEWED BY	MJB
DRAWN BY	EV	APPROVED BY	MJB
SCALE:	AS NOTED	ACCEPTED BY	DATE
0 ISSUED FOR TENDER	25.12.01	MJB	
NO. REVISIONS	DATE	BY	DATE 2025.12.01

PRELIMINARY  
DRAWING  
NOT TO BE  
USED FOR  
CONSTRUCTION  
CONSULTANT DRAWING NO.  
704-INF.MB10304301-S0030

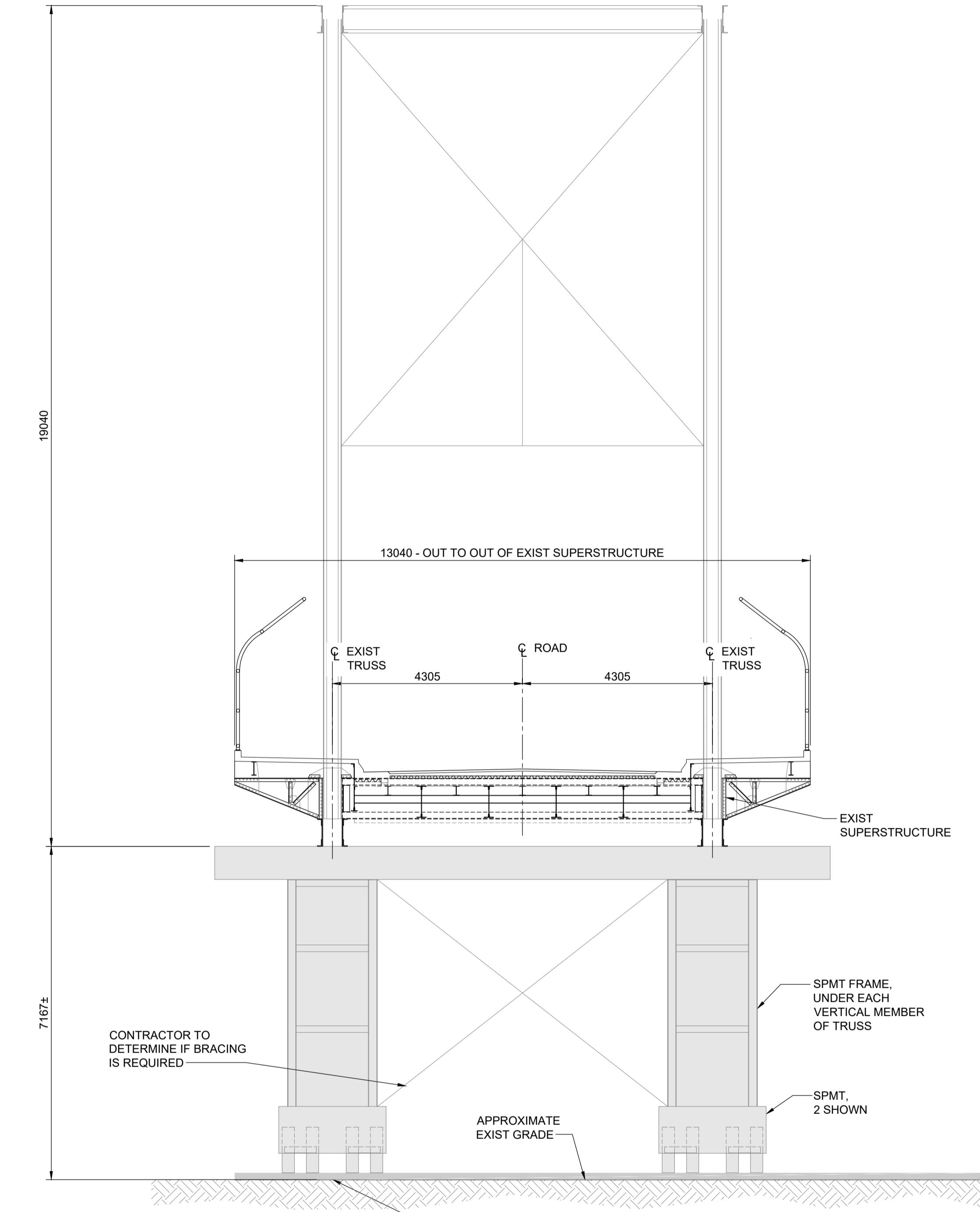


THE CITY OF WINNIPEG  
PUBLIC WORKS DEPARTMENT  
ARLINGTON BRIDGE  
PHASE 4 - DECOMMISSIONING  
SPAN 21  
SPMT REMOVAL OPTION  
CITY DRAWING NUMBER  
SHEET 30 OF 42  
30



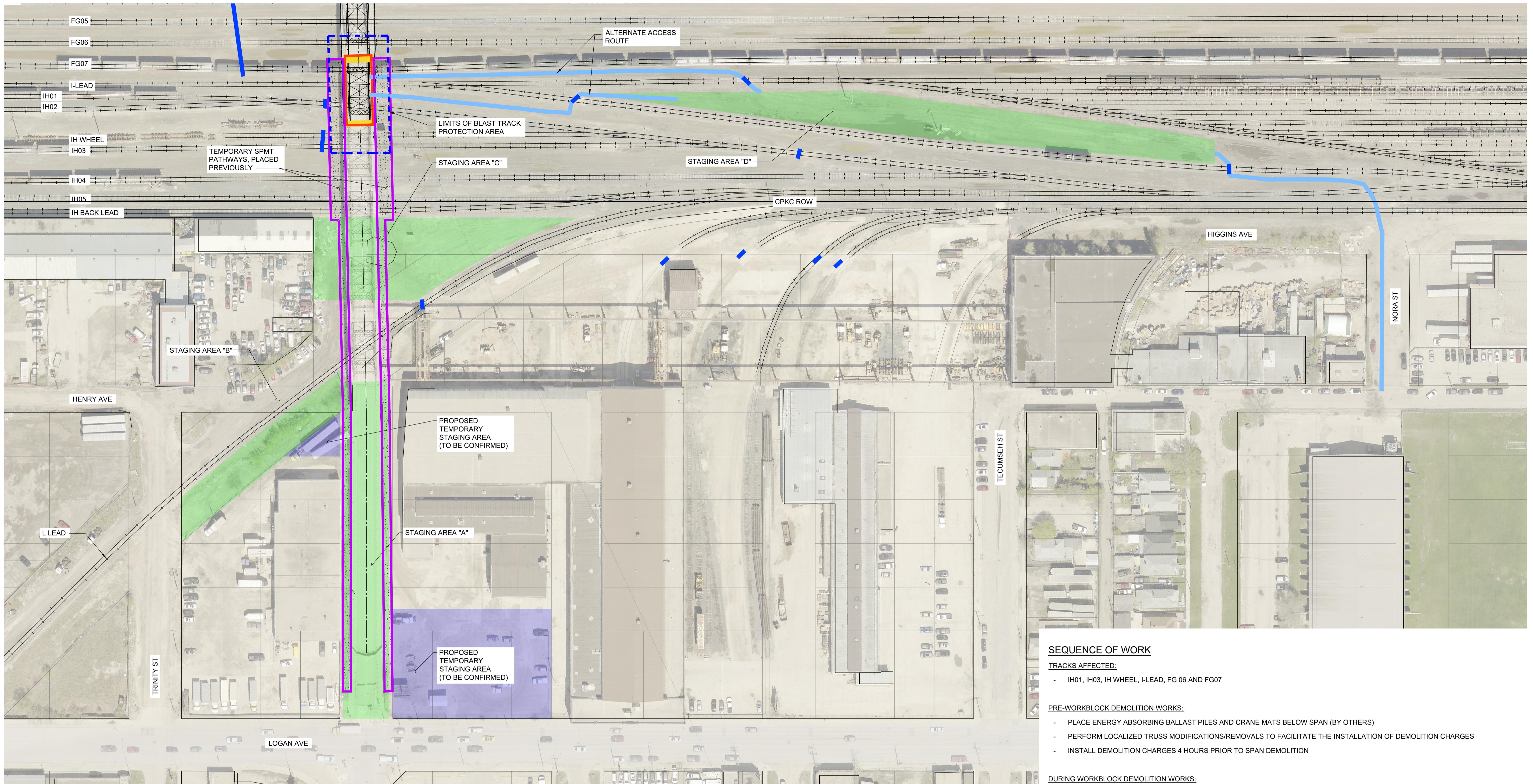
1 SPAN 19 & 21 - CROSS SECTION 1 : 75

ISSUED FOR  
**INFORMATION  
ONLY**  
NOT TO BE  
USED FOR  
CONSTRUCTION



2 SPAN 20 - CROSS SECTION  
- - 75

B.M. ELEV.				 <b>TETRA TECH</b>	PRELIMINARY DRAWING		 <b>THE CITY OF WINNIPEG</b> PUBLIC WORKS DEPARTMENT
DESIGNED BY		ML	REVIEWED BY		MJB		
DRAWN BY		EV	APPROVED BY		MJB		
SCALE:		AS NOTED	ACCEPTED BY	DATE	NOT TO BE USED FOR CONSTRUCTION		
0	ISSUED FOR TENDER			25.12.01	MJB	CITY DRAWING NUMBER	
NO.	REVISIONS	DATE	PX	DATE	2005-12-01	SHEET 31 OF 42	
CONSULTANT DRAWING NO. 704-INF.MBI0304301-S0031							
ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING							
SPAN 19, 20 AND 21 CROSS SECTIONS SPMT REMOVAL OPTION							
31							



## SPAN 18 ACCESS PLAN

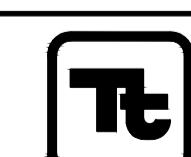
1 : 750

## LEGEND

- EXISTING PIER LOCATIONS
- ACCESS ROUTE FOR SPAN 18
- EXISTING AT GRADE CROSSING
- REQUIRED AT GRADE CROSSING
- CONSTRUCTION STAGING AREA
- OVERALL WORK AREA
- BLAST TRACK PROTECTION AREA
- SPMT TRAVEL PATH

ISSUED FOR  
**INFORMATION  
ONLY**

NOT TO BE  
USED FOR  
CONSTRUCTION



**TETRA TECH**

## PRELIMINARY DRAWING

 Winnipeg

# THE CITY OF WINNIPEG

## PUBLIC WORKS DEPARTMENT

## ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING

ITY DRAWING NUMBER

## SPAN 18 ACCESS PLAN BLAST AND DROP REMOVAL OPTION

32

## SEQUENCE OF WORK

#### TRACKS AFFECTED:

- IH01, IH03, IH WHEEL, I-LEAD, FG 06 AND FG07

#### PRE-WORKBLOCK DEMOLITION WORKS:

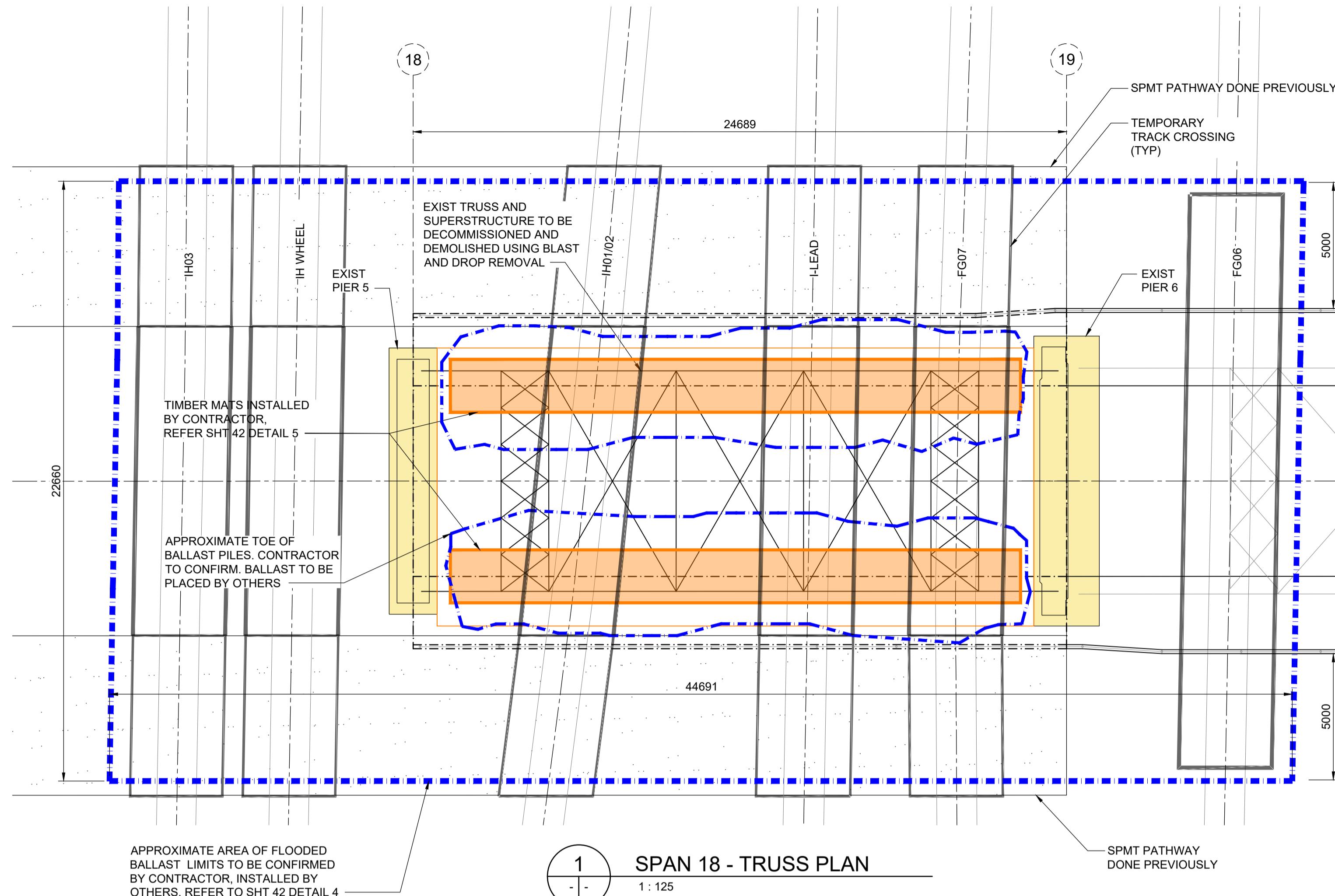
- PLACE ENERGY ABSORBING BALLAST PILES AND CRANE MATS BELOW SPAN (BY OTHERS)
- PERFORM LOCALIZED TRUSS MODIFICATIONS/REMOVALS TO FACILITATE THE INSTALLATION OF DEMOLITION CHARGES
- INSTALL DEMOLITION CHARGES 4 HOURS PRIOR TO SPAN DEMOLITION

#### DURING WORKBLOCK DEMOLITION WORKS:

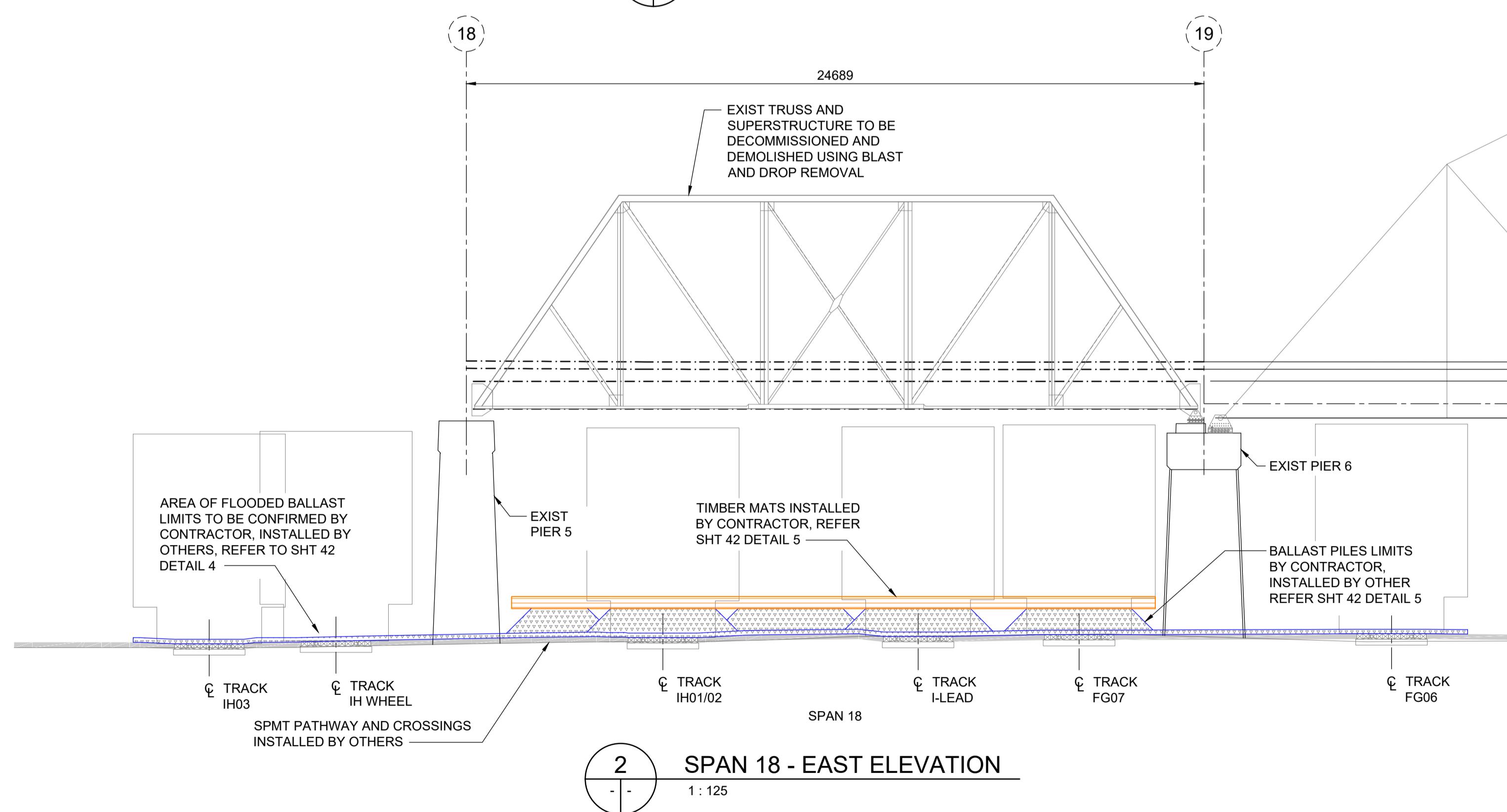
- SET UP BLAST PERIMETER DELINEATION AND CLEAR ALL PERSONNEL WITHIN BLAST ZONE
- DETONATE DEMOLITION CHARGES AND DROP SPAN
- REMOVE BLAST PERIMETER FENCING
- DISMANTLE TRUSS COMPONENTS ON GROUND, WHILE SIMULTANEOUSLY HAULING SCRAP MATERIAL OFF SITE
- HAULING EQUIPMENT TO REMOVE TRUSS COMPONENTS VIA TRUCK TO STAGING AREA "D" TEMPORARILY THEN OFF SITE VIA NORA STREET OR TO STAGING AREA "C" THEN OFF SITE VIA LOGAN AVE.

## POST-WORKBLOCK DEMOLITION WORKS:

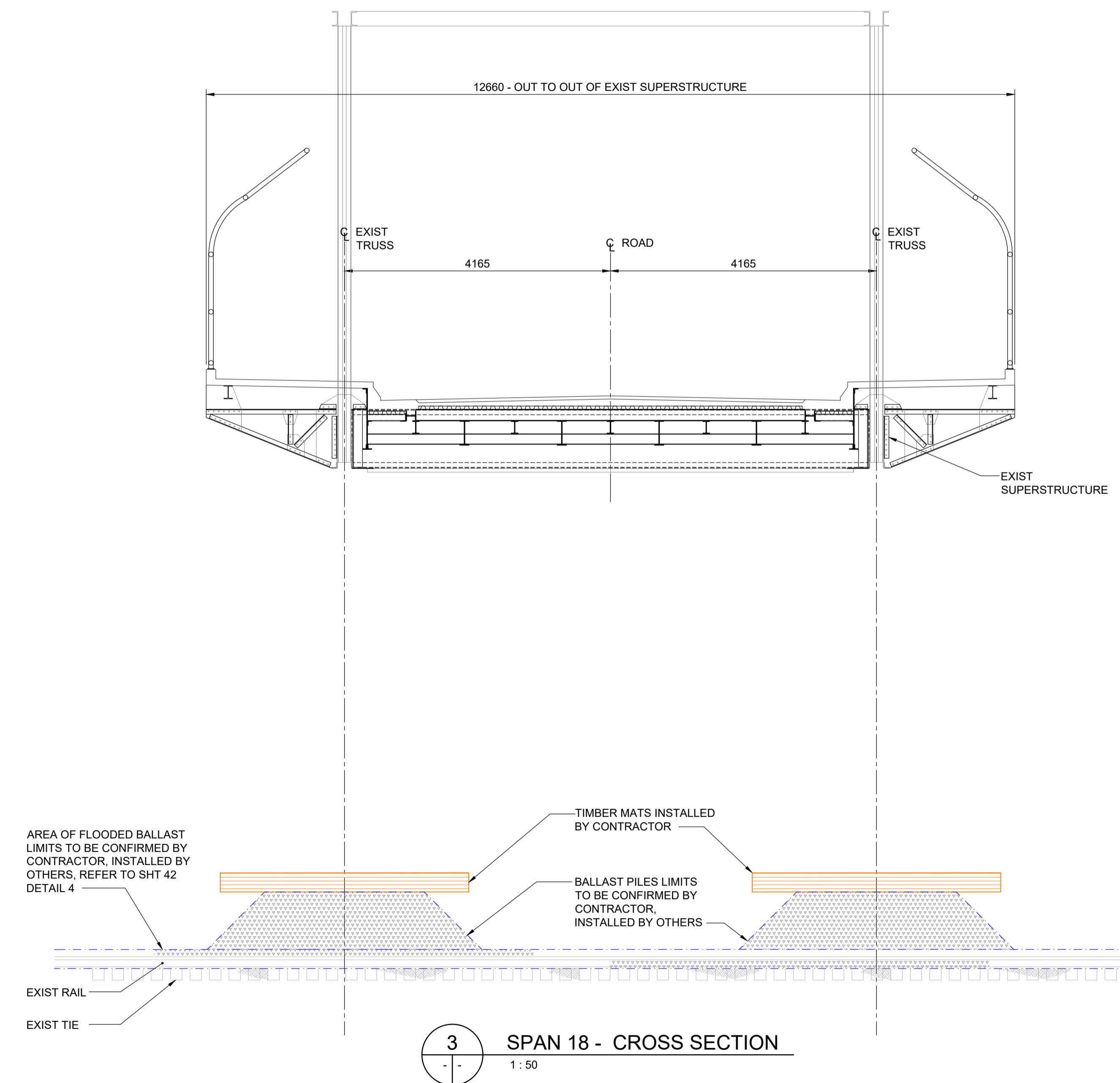
- CLEAN AND REMOVE BALLAST PILES TO BE USED AT NEXT DEMOLITION LOCATION (BY OTHERS)



1 SPAN 18 - TRUSS PLAN  
1:125



2 SPAN 18 - EAST ELEVATION  
1:125



3 SPAN 18 - CROSS SECTION  
1:50

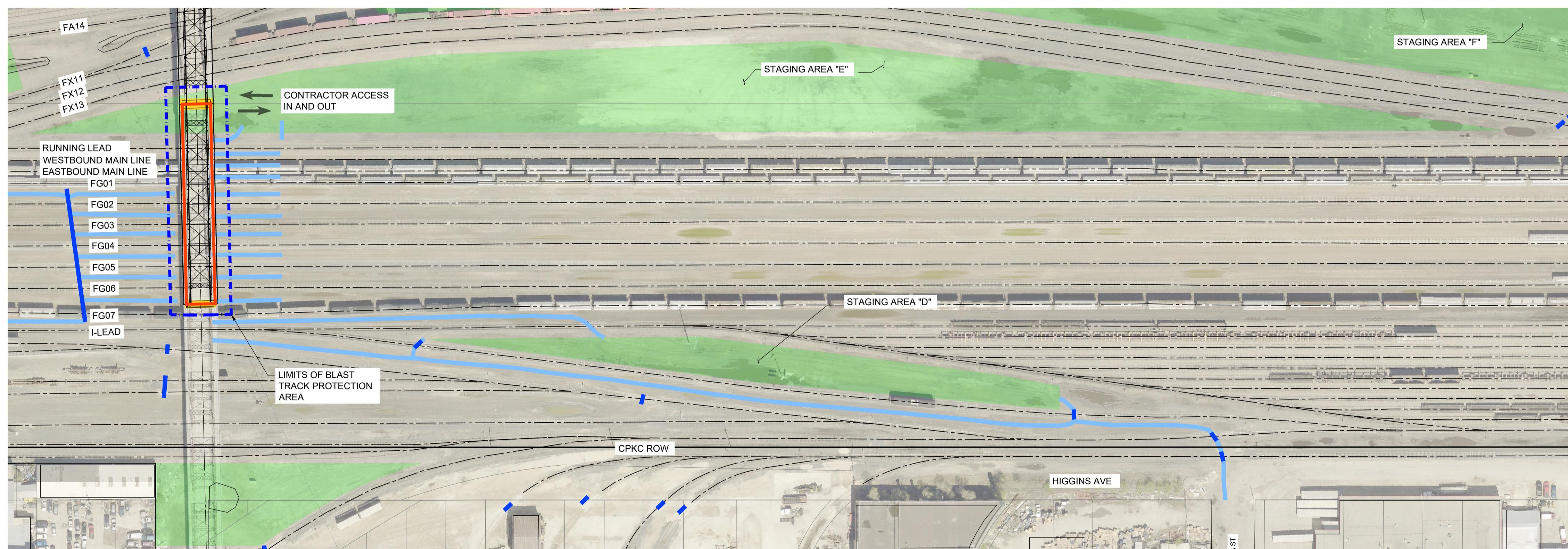
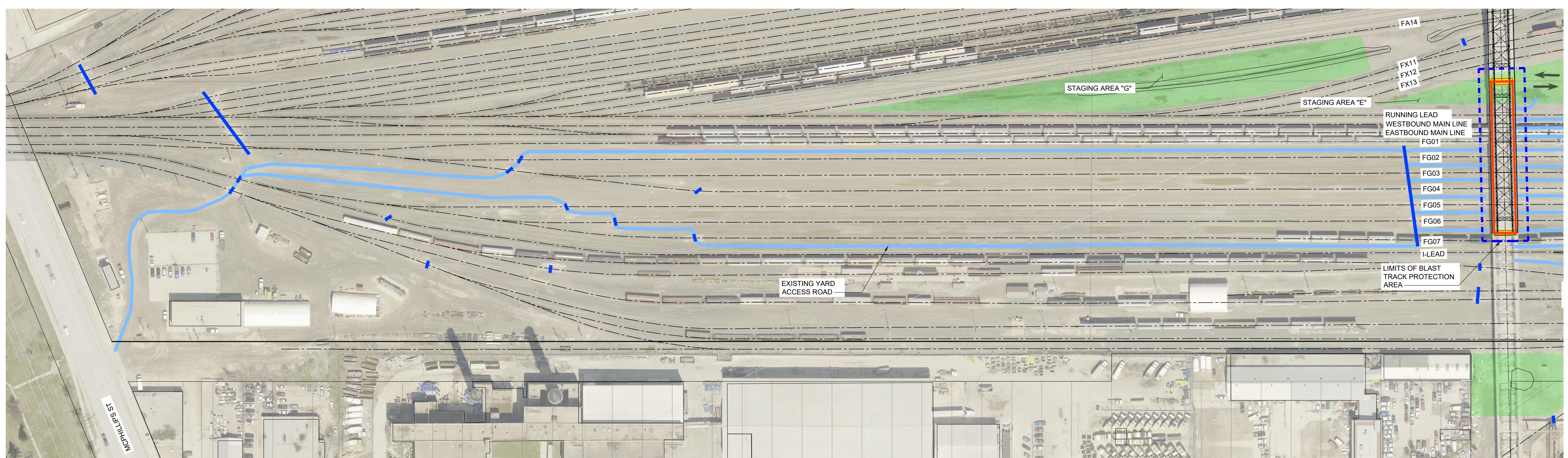
ISSUED FOR  
INFORMATION  
ONLY  
NOT TO BE  
USED FOR  
CONSTRUCTION

**ENGINEERS  
GEOSCIENTISTS**  
MANITOBA  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

B.M. ELEV.		TETRA TECH		PRELIMINARY DRAWING		THE CITY OF WINNIPEG	
DESIGNED BY	ML	REVIEWED BY	MJB	NOT TO BE USED FOR CONSTRUCTION		ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	CITY DRAWING NUMBER
DRAWN BY	EV	APPROVED BY	MJB				
SCALE:	AS NOTED	ACCEPTED BY	DATE				
0 ISSUED FOR TENDER	25.12.01	MJB					
NO. REVISIONS	DATE	BY	DATE	2025.12.01		SPAN 18 BLAST AND DROP REMOVAL OPTION	
							33
							33 OF 42

CONSULTANT DRAWING NO.  
704-INF.MB10304301-S0033

THE CITY OF WINNIPEG  
PUBLIC WORKS DEPARTMENT



1  
- - -

### SPAN 19 - ACCESS PLAN

1 : 1000

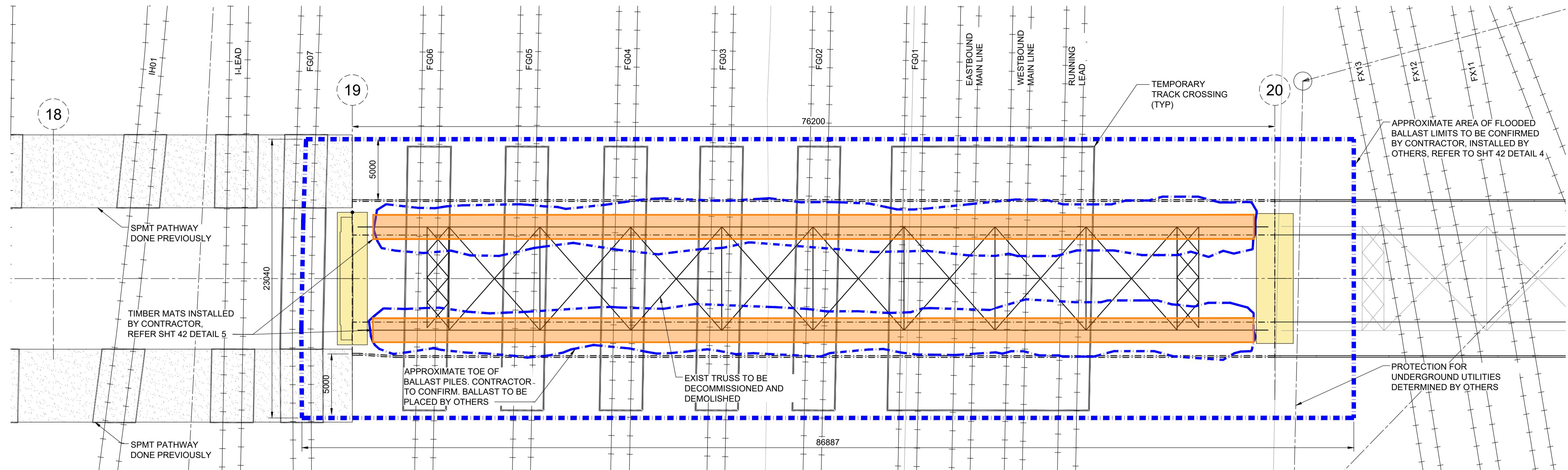
#### LEGEND:

- EXISTING PIER LOCATIONS
- ACCESS ROUTE FOR SPAN 19
- EXISTING AT GRADE CROSSING
- CONSTRUCTION STAGING AREA
- OVERALL WORK AREA
- BLAST TRACK PROTECTION AREA

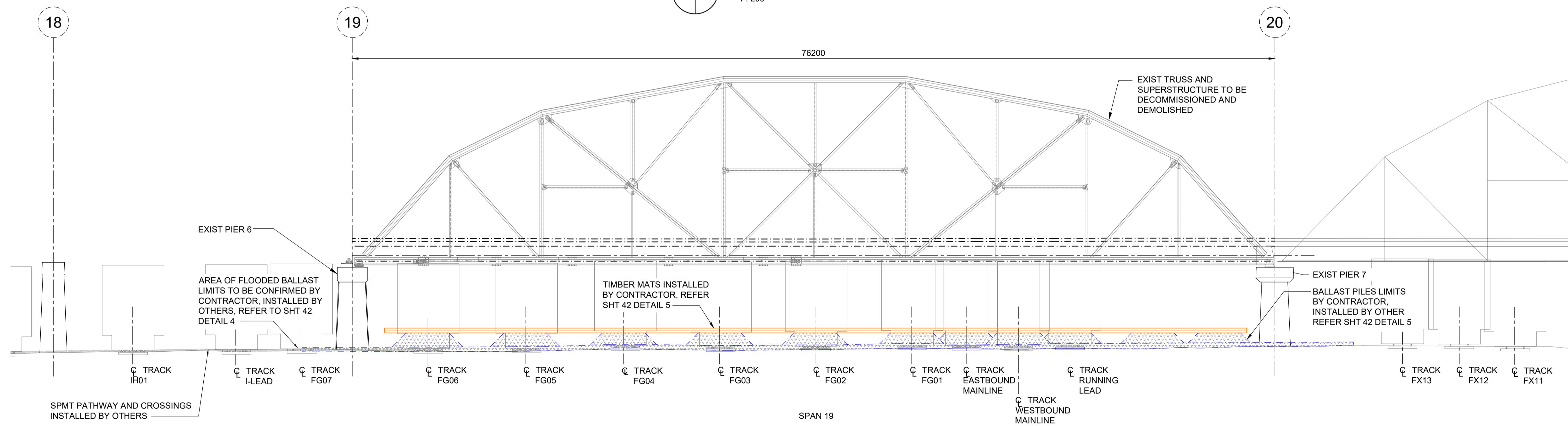
ISSUED FOR  
INFORMATION  
ONLY  
NOT TO BE  
USED FOR  
CONSTRUCTION

**ENGINEERS  
GEOSCIENTISTS  
MANITOBA**  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

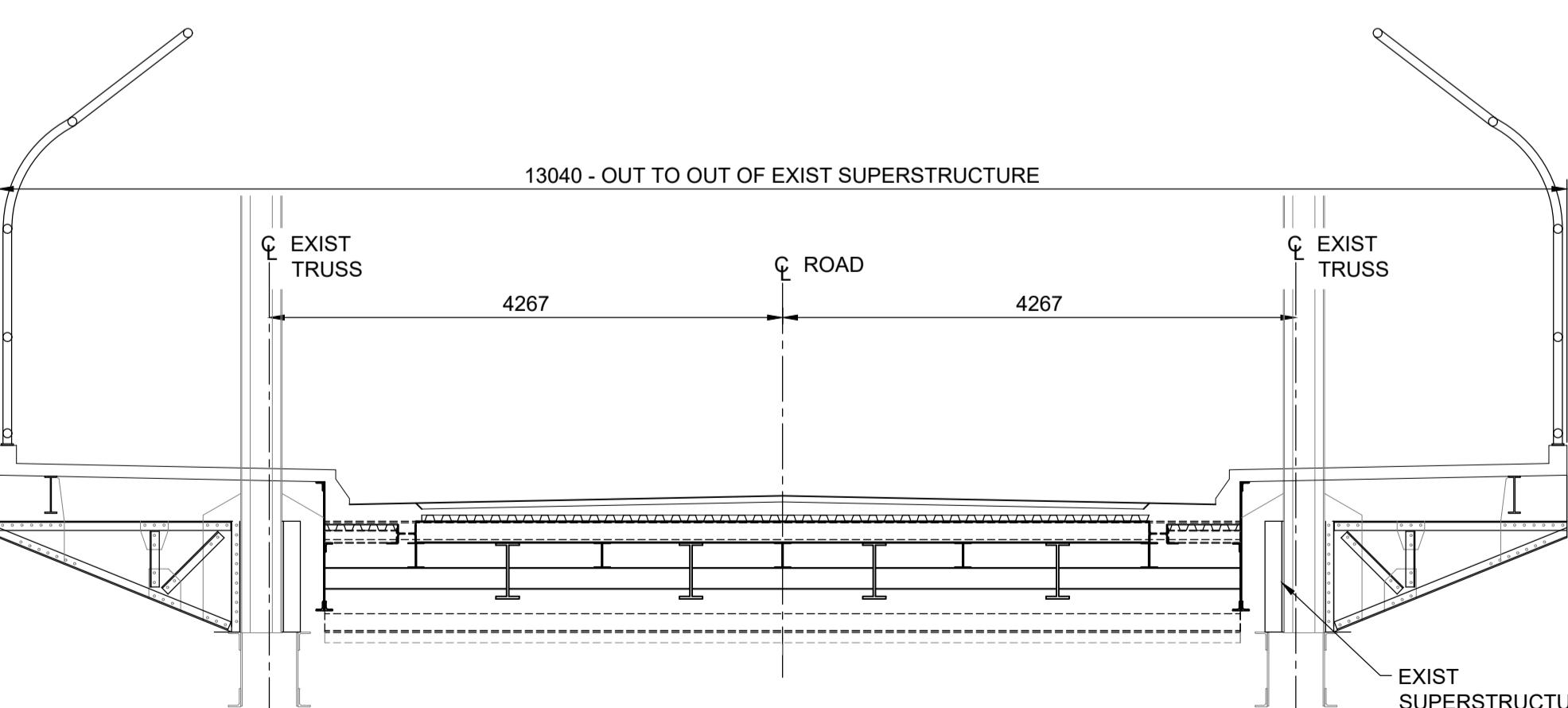
B.M. ELEV.		TETRA TECH		PRELIMINARY DRAWING		THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT	
DESIGNED BY	ML	REVIEWED BY	MJB				
DRAWN BY	EV	APPROVED BY	MJB				
SCALE:	AS NOTED	ACCEPTED BY	DATE				
0 ISSUED FOR TENDER	25.12.01	MJB					
NO. REVISIONS	DATE	BY	DATE	2025.12.01			
CONSULTANT DRAWING NO. 704-INF.MB10304301-S0034				CITY DRAWING NUMBER 34 OF 42			
SPAN 19 ACCESS PLAN BLAST AND DROP REMOVAL OPTION				34			



1 SPAN 19 - TRUSS PLAN  
1 : 200



2 SPAN 19 - EAST ELEVATION  
1 : 200

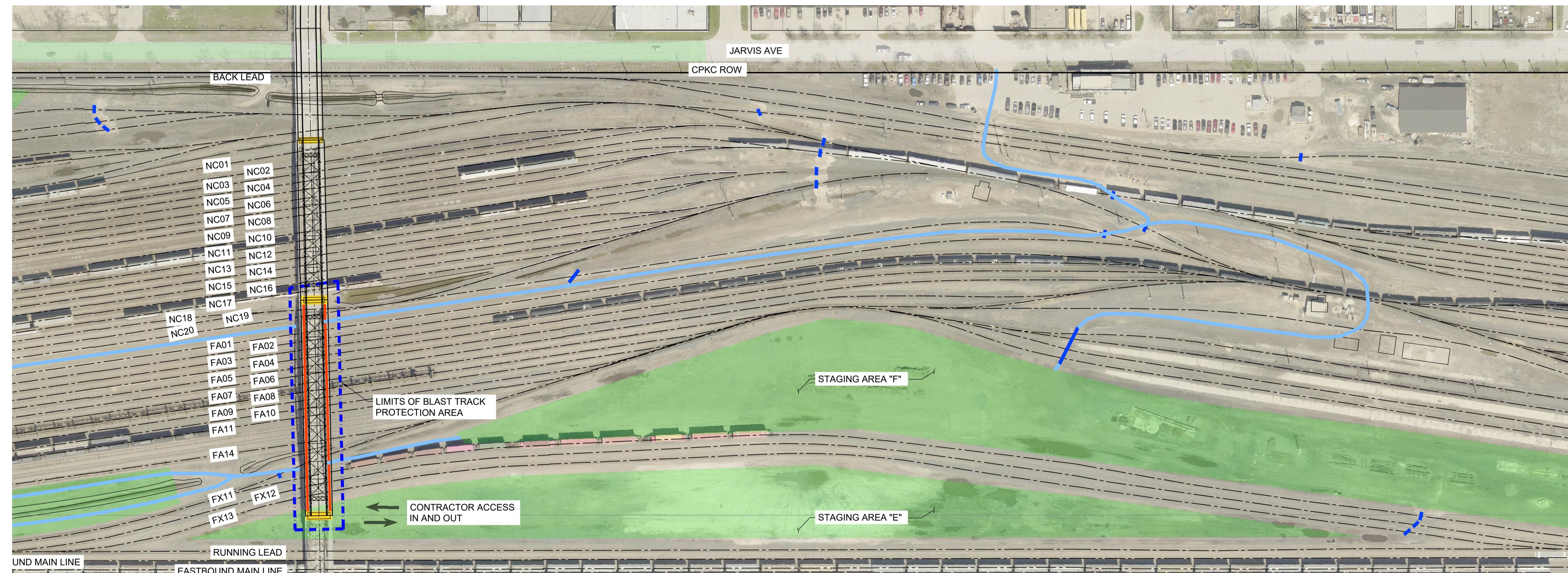
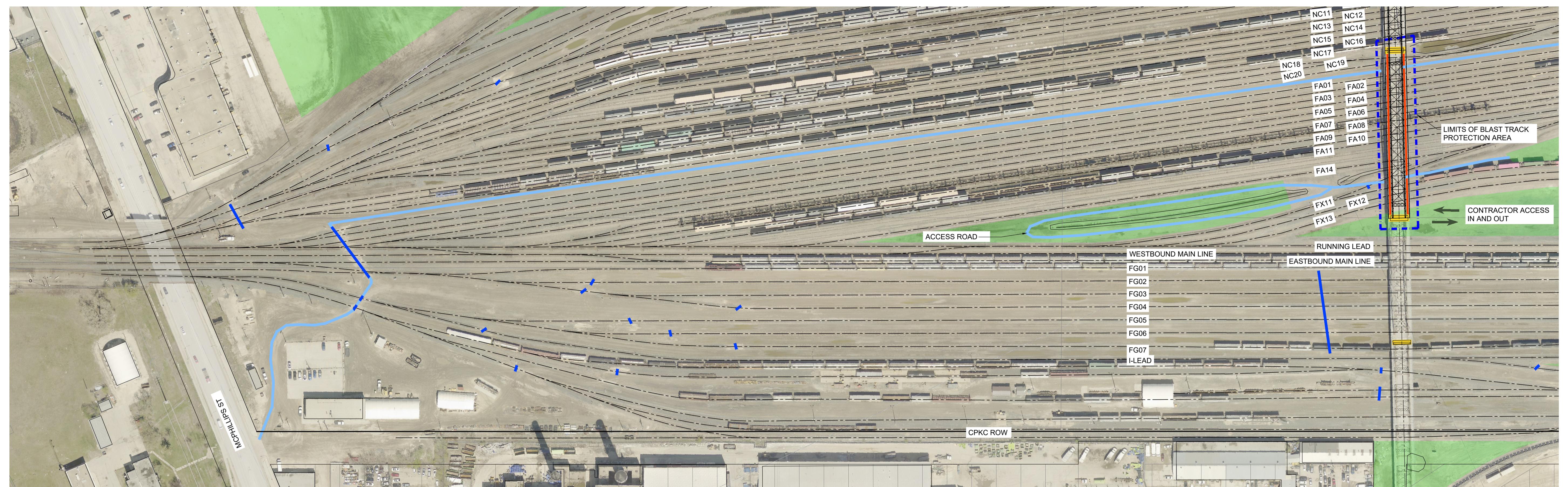


3 SPAN 19 - CROSS SECTION  
1 : 50

 **ENGINEERS  
GEOSCIENTISTS**  
MANITOBA  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

B.M. ELEV.		<b>TETRA TECH</b>		PRELIMINARY DRAWING		THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT	
DESIGNED BY	ML	REVIEWED BY	MJB				
DRAWN BY	EV	APPROVED BY	MJB				
SCALE:	AS NOTED	ACCEPTED BY	DATE				
0 ISSUED FOR TENDER	25.12.01	MJB					
NO. REVISIONS	DATE	BY	DATE	2025.12.01			
CONSULTANT DRAWING NO. 704-INF.MB10304301-S0035		CITY DRAWING NUMBER		ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING		SHEET 35 OF 42	
SPAN 19 BLAST AND DROP OPTION						35	

ISSUED FOR  
INFORMATION  
ONLY  
NOT TO BE  
USED FOR  
CONSTRUCTION



1  
1 : 1250

### SPAN 20 - ACCESS PLAN

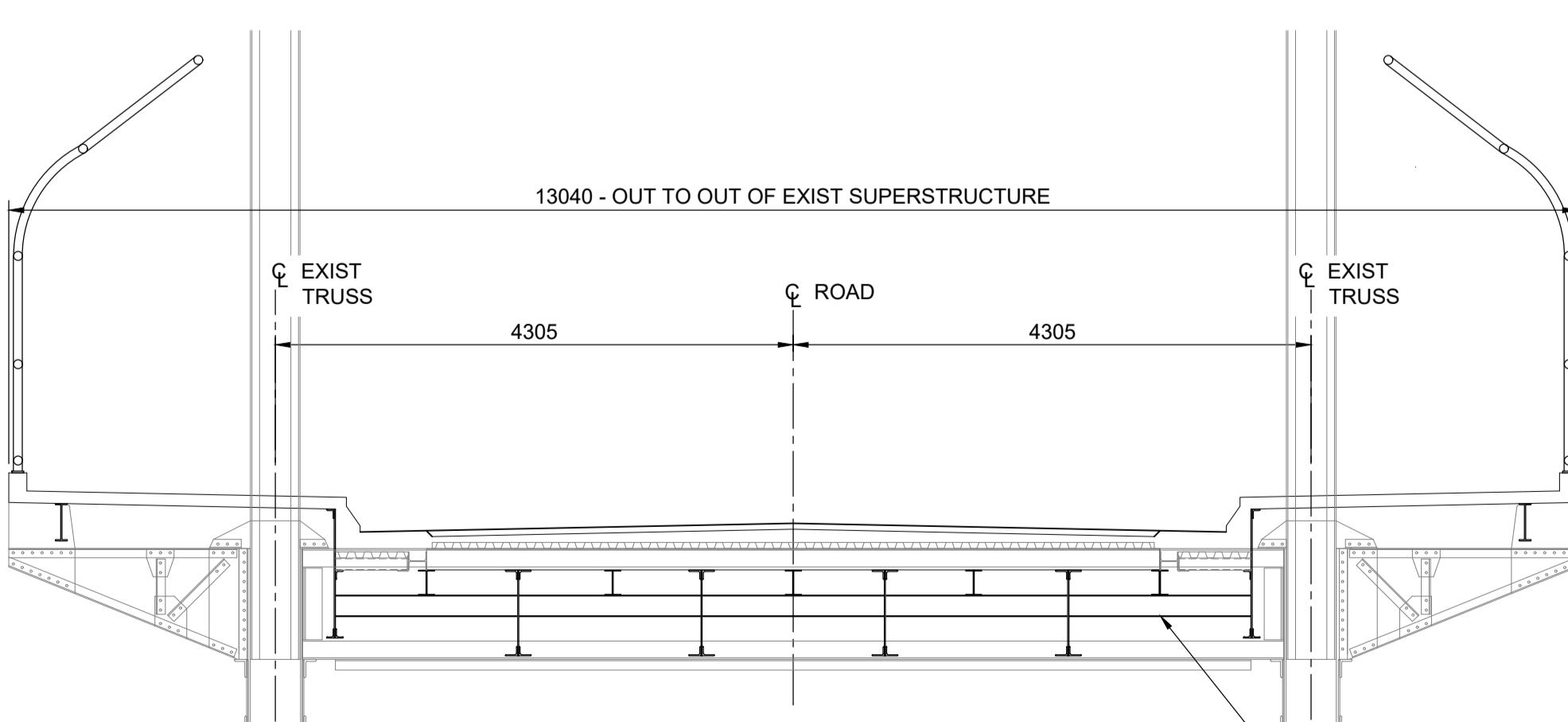
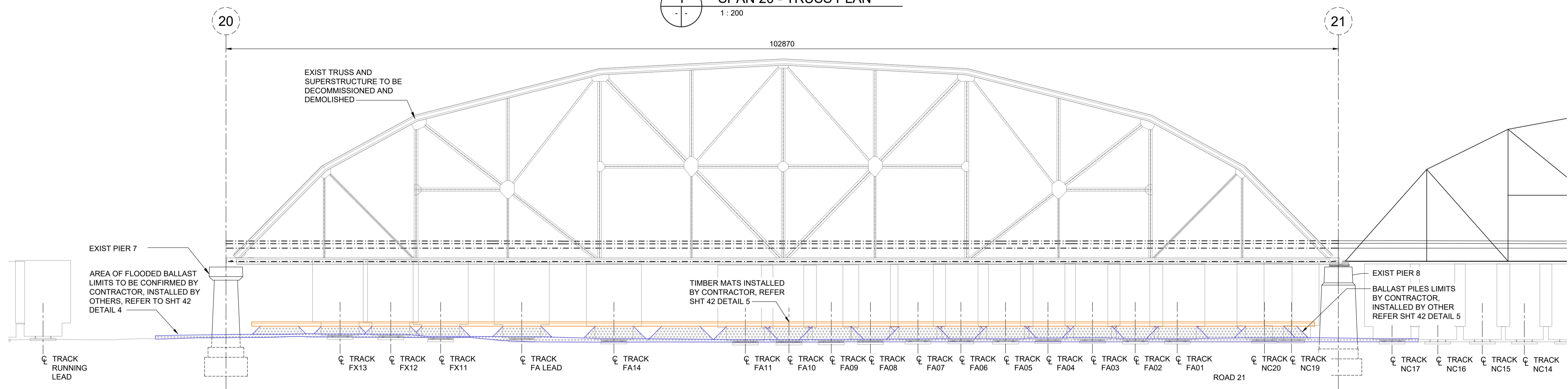
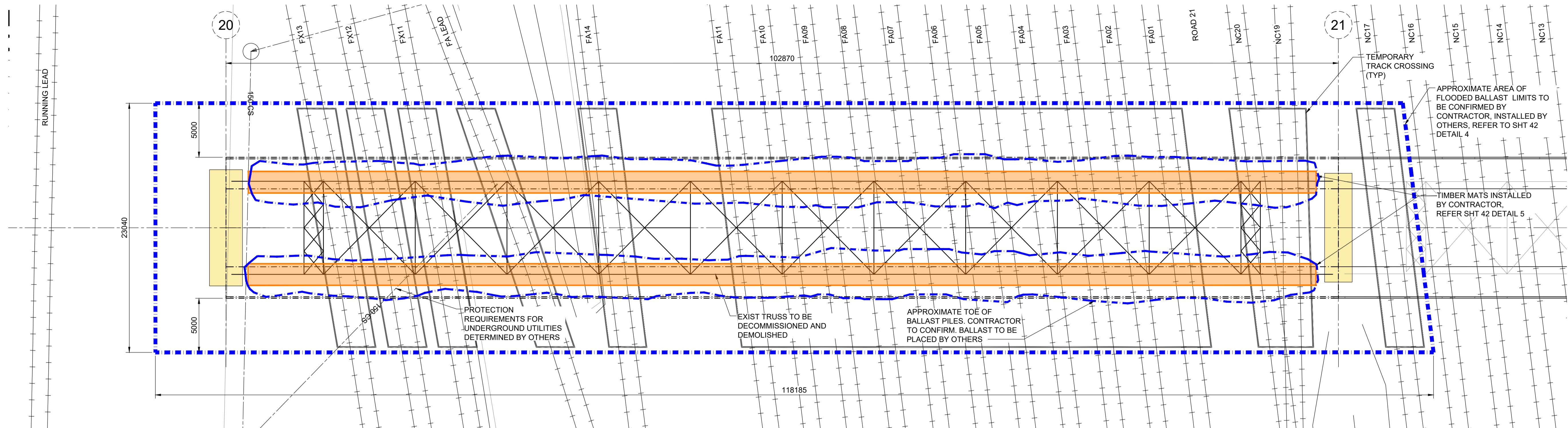
#### LEGEND:

- EXISTING PIER LOCATIONS
- ACCESS ROUTE FOR SPAN 20
- EXISTING AT GRADE CROSSING
- CONSTRUCTION STAGING AREA
- OVERALL WORK AREA
- BLAST TRACK PROTECTION AREA

ISSUED FOR  
INFORMATION  
ONLY  
NOT TO BE  
USED FOR  
CONSTRUCTION

ENGINERS  
GEOSCIENTISTS  
MANITOBA  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

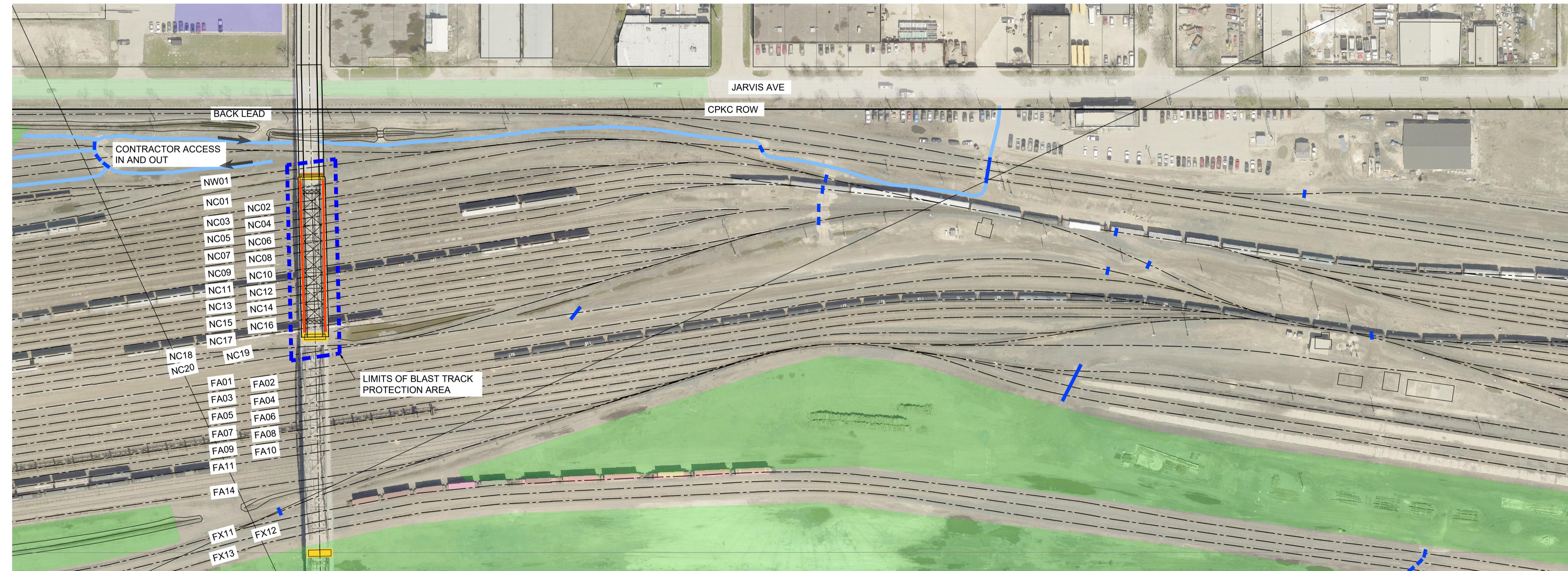
B.M. ELEV.		TETRA TECH		PRELIMINARY DRAWING		THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT	
DESIGNED BY	ML	REVIEWED BY	MJB	NOT TO BE USED FOR CONSTRUCTION		CITY DRAWING NUMBER	
DRAWN BY	EV	APPROVED BY	MJB				
SCALE:	AS NOTED	ACCEPTED BY	DATE				
0 ISSUED FOR TENDER	25.12.01	MJB					
NO. REVISIONS	DATE	BY	DATE	704-INF.MB10304301-S0036		SHEET 36 OF 42	
SPAN 20 ACCESS PLAN BLAST AND DROP REMOVAL OPTION							



3 SPAN 20 - CROSS SECTION  
- - 1 : 50



B.M. ELEV.				 <b>TETRA TECH</b>	<b>PRELIMINARY DRAWING</b>  NOT TO BE USED FOR CONSTRUCTION	 <b>THE CITY OF WINNIPEG</b> PUBLIC WORKS DEPARTMENT
DESIGNED BY	ML	REVIEWED BY	MJB			
DRAWN BY	EV	APPROVED BY	MJB	ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING		
SCALE:	AS NOTED	ACCEPTED BY	DATE	SPAN 20 BLAST AND DROP REMOVAL OPTION		
0	ISSUED FOR TENDER	25.12.01	MJB	CITY DRAWING NUMBER SHEET 37 OF 42		
NO.	REVISIONS	DATE	BY	704-INF.MBI0304301-S0037		
				37		



LEGEND:

- EXISTING PIER LOCATIONS
- ACCESS ROUTE FOR SPAN 21
- EXISTING AT GRADE CROSSING
- CONSTRUCTION STAGING AREA
- OVERALL WORK AREA
- BLAST TRACK PROTECTION AREA

ISSUED FOR  
INFORMATION  
ONLY  
NOT TO BE  
USED FOR  
CONSTRUCTION

**ENGINEERS  
GEOSCIENTISTS  
MANITOBA**  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

1  
1 : 1250

SPAN 21 - ACCESS PLAN

B.M. ELEV.		TETRA TECH	
DESIGNED BY	ML	REVIEWED BY	MJB
DRAWN BY	EV	APPROVED BY	MJB
SCALE:	AS NOTED	ACCEPTED BY	DATE
0 ISSUED FOR TENDER	25.12.01	MJB	
NO. REVISIONS	DATE	BY	DATE 2025.12.01

PRELIMINARY  
DRAWING  
NOT TO BE  
USED FOR  
CONSTRUCTION  
CONSULTANT DRAWING NO.  
704-INF.MB10304301-S0038

THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT	CITY DRAWING NUMBER
ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	SHEET 38 OF 42
SPAN 21 ACCESS PLAN BLAST AND DROP REMOVAL OPTION	38

SEQUENCE OF WORK

TRACKS AFFECTED:

- NC01, NC02, NC03, NC04, NC05, NC06, NC07, NC08, NC09, NC10, NC11, NC12, NC13, NC14, NC15, NC16, NC17, NC19, NC20, NW01, NW02, NW03, NW04, NW05, NW06 AND BACK LEAD

PRE-WORKBLOCK DEMOLITION WORKS:

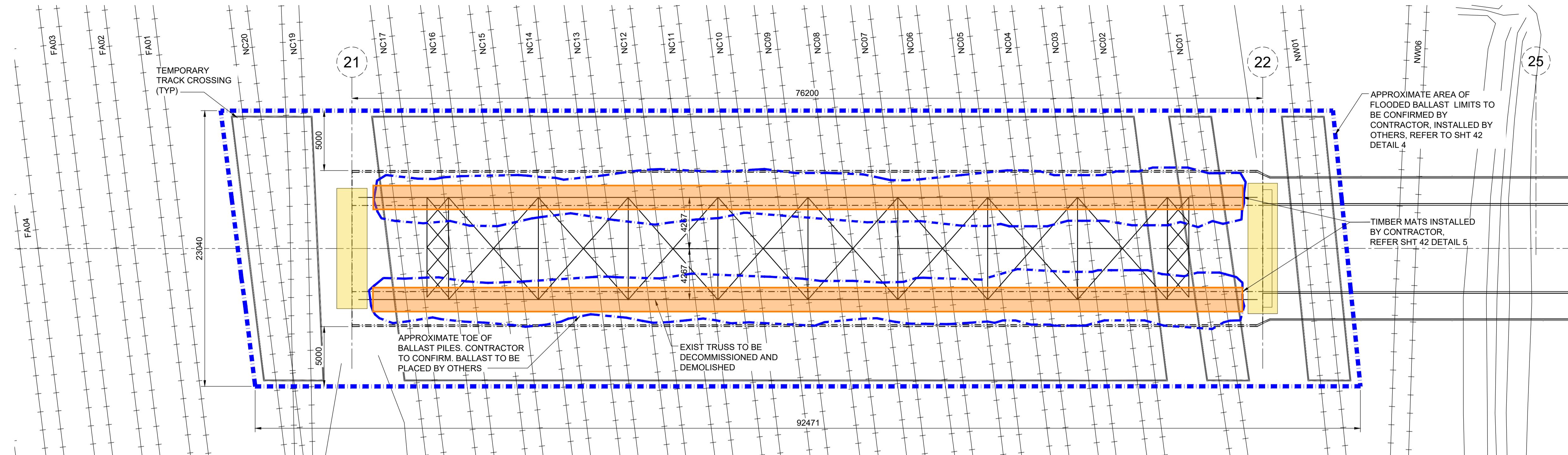
- REMOVE ALL ANCILLARY BRIDGE COMPONENTS.
- PERFORM LOCALIZED TRUSS MODIFICATIONS/REMOVALS TO FACILITATE THE INSTALLATION OF DEMOLITION CHARGES
- INSTALL DEMOLITION CHARGES 24 HRS PRIOR TO SPAN DEMOLITION

DURING WORKBLOCK DEMOLITION WORKS:

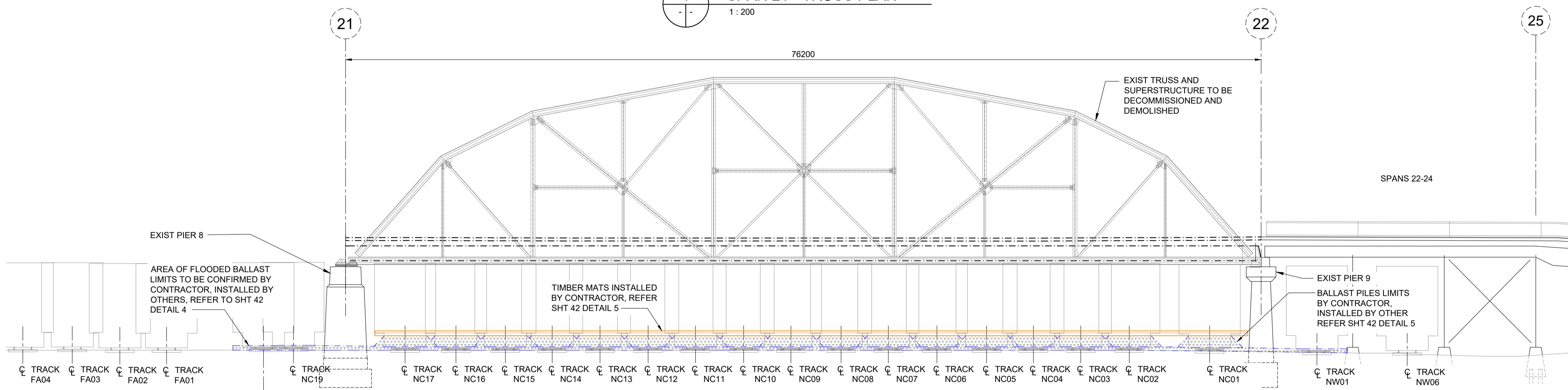
- CPKC TO SETUP TRACK PROTECTION TO TAKE AFFECTED TRACKS OUT OF SERVICE
- PLACE ENERGY ABSORBING BALLAST PILES AND CRANE MATS BELOW SPAN (BY OTHERS)
- SETUP BLAST PERIMETER FENCING AND CLEAR ALL PERSONNEL WITHIN BLAST ZONE
- DETONATE DEMOLITION CHARGES AND DROP SPAN
- REMOVE BLAST PERIMETER FENCING
- DISMANTLE TRUSS COMPONENTS ON GROUND WHILE SIMULTANEOUSLY HAULING DEMOLITION DEBRIS TO MID YARD STAGING AREA "F".
- CLEAN AND REMOVE BALLAST PILES TO BE USED AT NEXT DEMOLITION LOCATION (BY OTHERS)
- CPKC REMOVES TRACK PROTECTION. ALL AFFECTED TRACK CLEAR

POST-WORKBLOCK DEMOLITION WORKS:

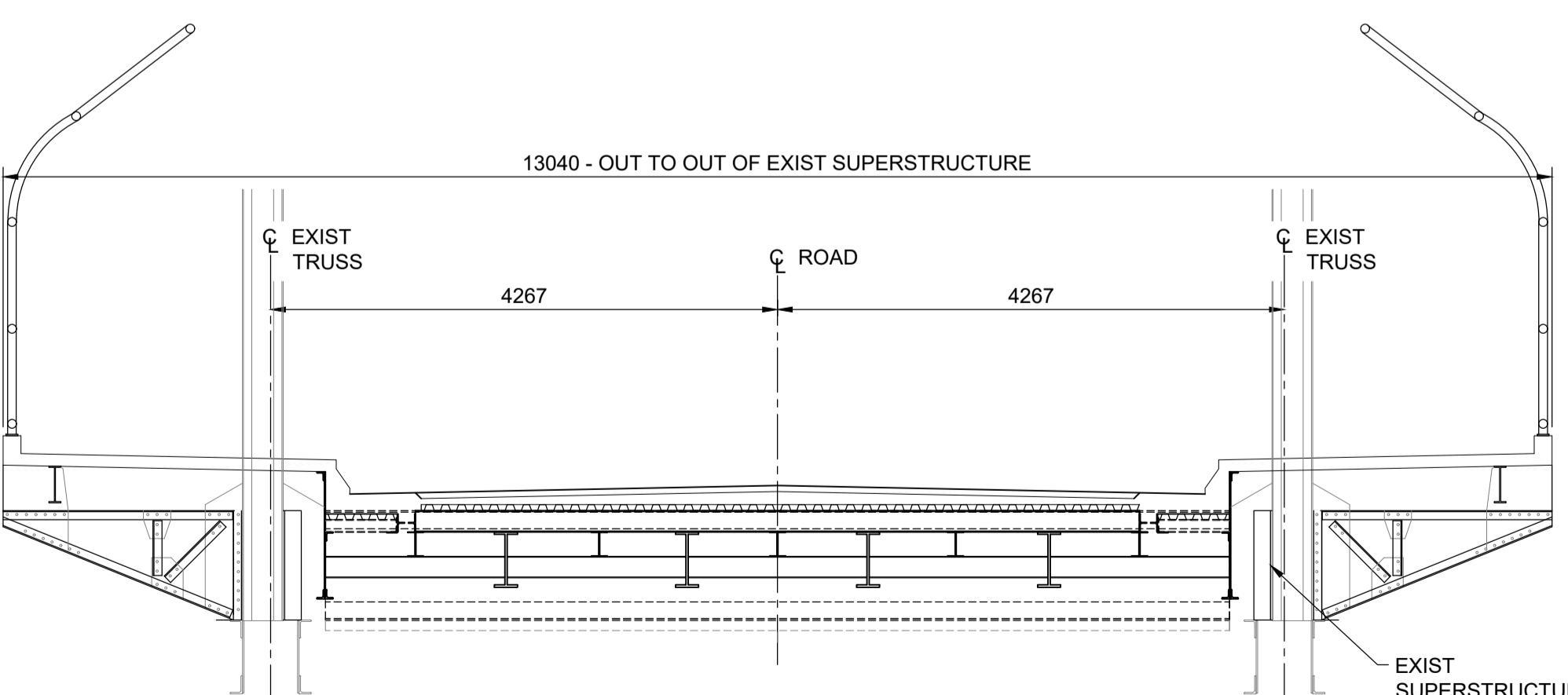
- DEMOLITION DEBRIS TO BE REMOVED OFF SITE ACROSS THE BACK LEAD AND ONTO JARVIS AVE.



1 SPAN 21 - TRUSS PLAN



2 SPAN 21 - EAST ELEVATION

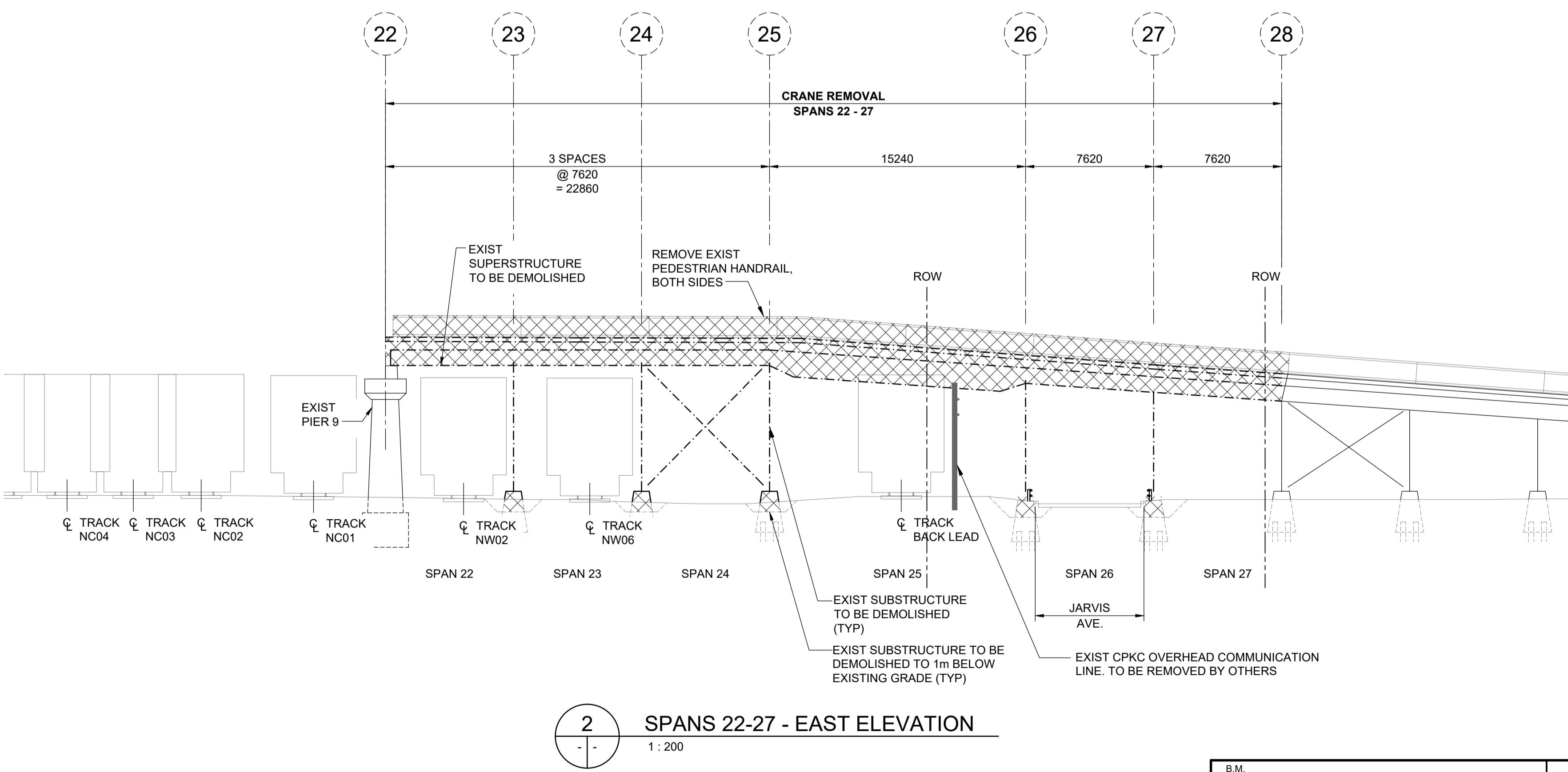
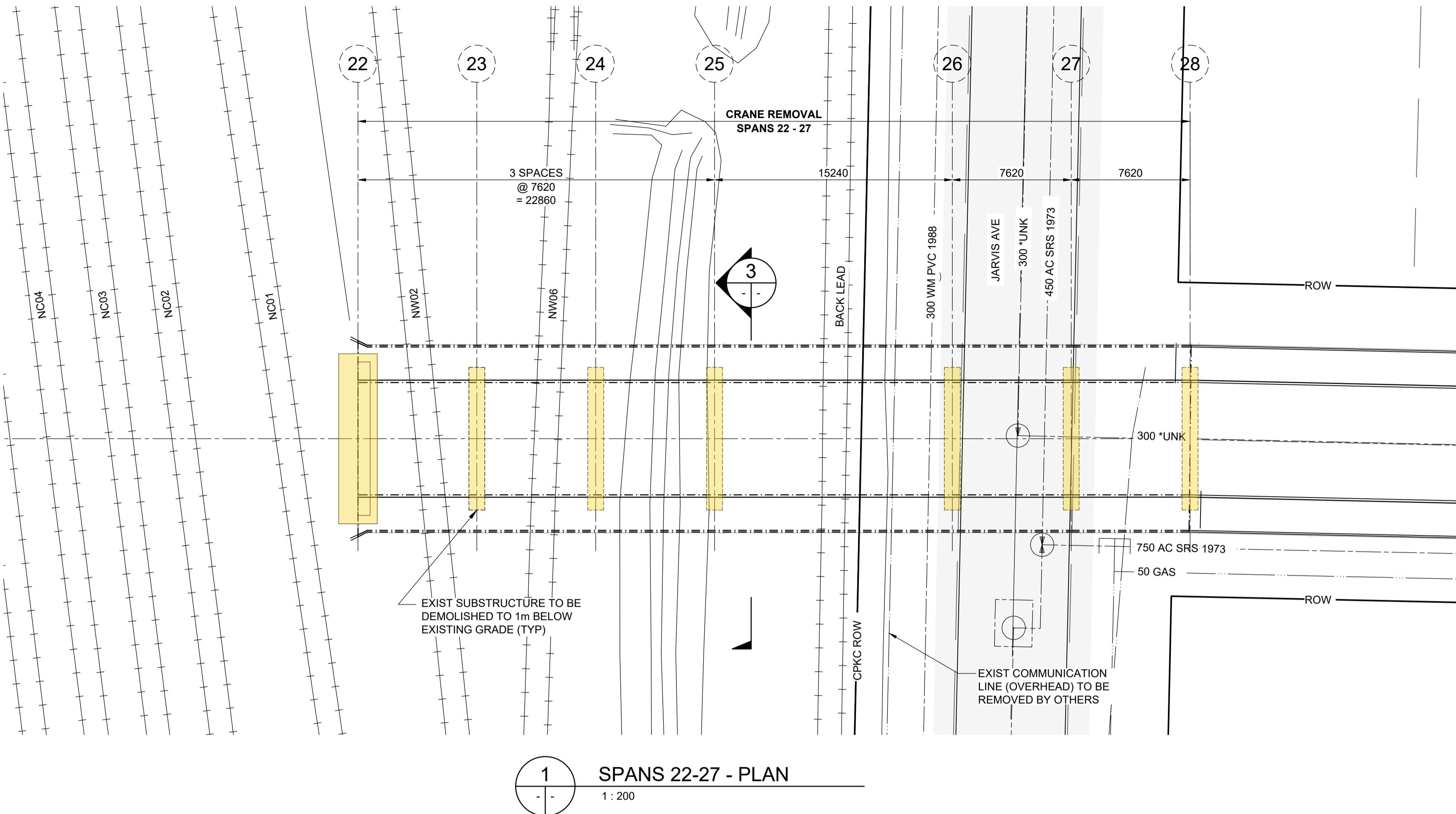


3 SPAN 21 - CROSS SECTION

**ENGINEERS  
GEOSCIENTISTS  
MANITOBA**  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

B.M. ELEV.		TETRA TECH		PRELIMINARY DRAWING		THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT	
DESIGNED BY	ML	REVIEWED BY	MJB				
DRAWN BY	EV	APPROVED BY	MJB				
SCALE:	AS NOTED	ACCEPTED BY	DATE				
0 ISSUED FOR TENDER	25.12.01	MJB				ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	CITY DRAWING NUMBER
NO. REVISIONS	DATE	BY	DATE	2025.12.01			SHEET 39 OF 42
						SPAN 21 BLAST AND DROP REMOVAL OPTION	39
						704-INF.MB10304301-S0039	

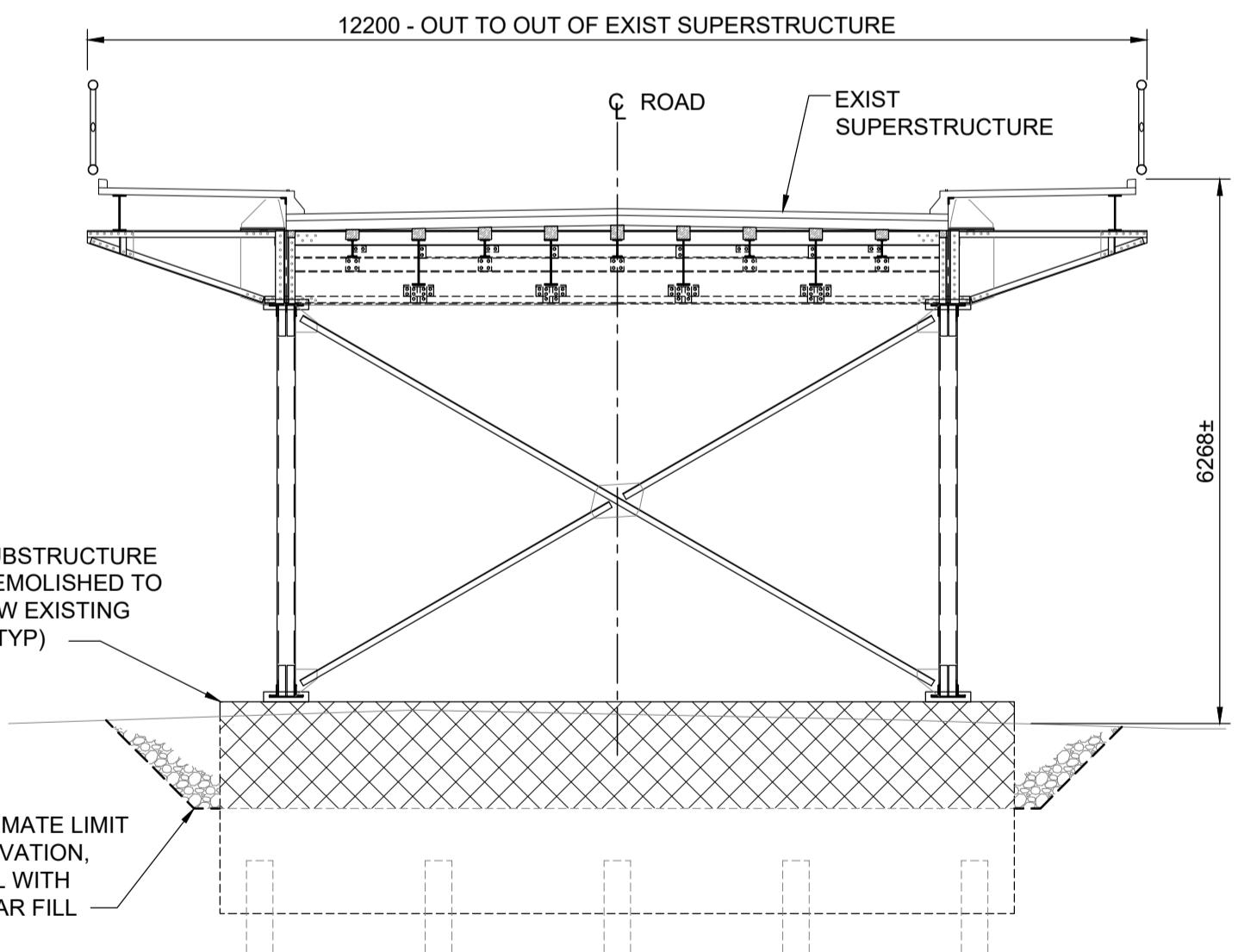
ISSUED FOR  
INFORMATION  
ONLY  
NOT TO BE  
USED FOR  
CONSTRUCTION



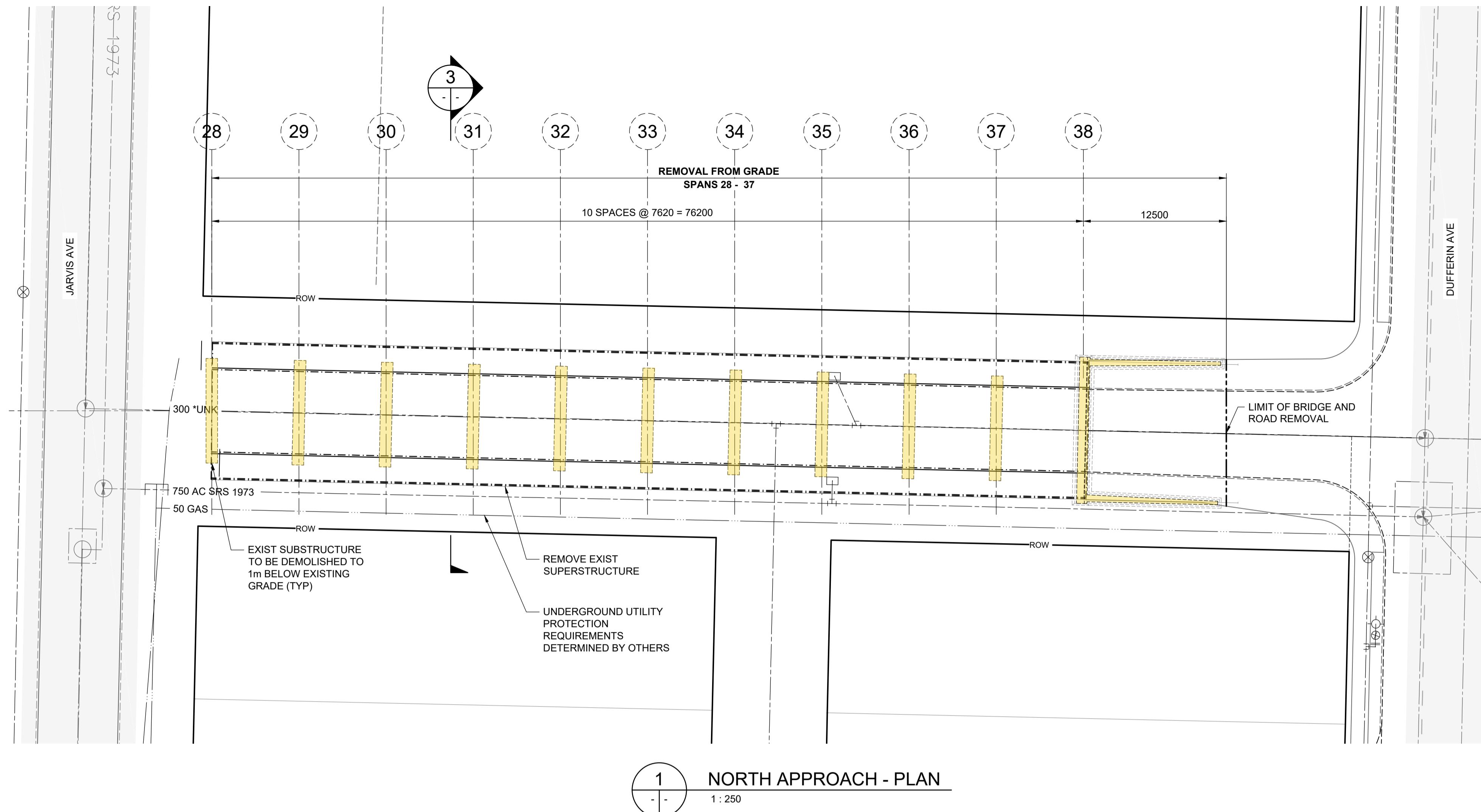
ESTIMATED MASS:  
SPANS 22-24 - 55,000 kg  
SPAN 25 - 105,000 kg  
SPANS 26-27 - 55,000 kg

ISSUED FOR  
INFORMATION  
ONLY  
NOT TO BE  
USED FOR  
CONSTRUCTION

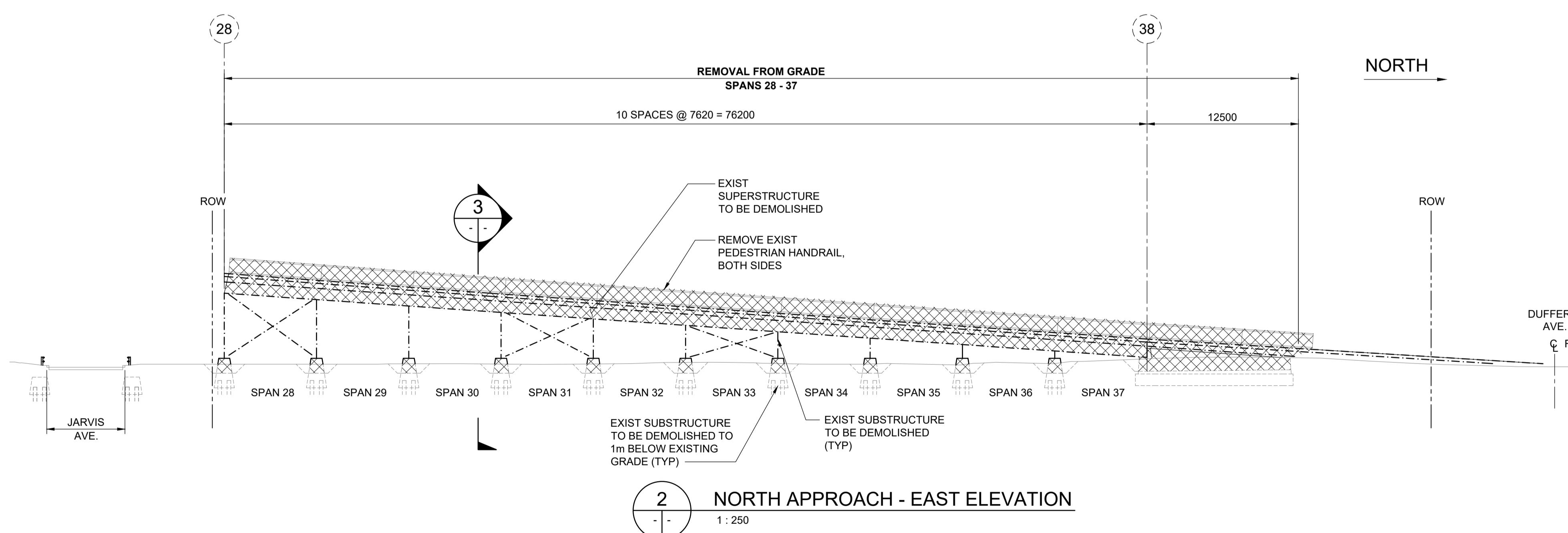
**ENGINEERS  
GEOSCIENTISTS**  
MANITOBA  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499



B.M. ELEV.		TETRA TECH		PRELIMINARY DRAWING		THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT	
DESIGNED BY	ML	REVIEWED BY	MJB				
DRAWN BY	EV	APPROVED BY	MJB				
SCALE:	AS NOTED	ACCEPTED BY	DATE				
0 ISSUED FOR TENDER	25.12.01	MJB					
NO. REVISIONS	DATE	BY	DATE	2025.12.01			
CONSULTANT DRAWING NO. 704-INF.MB10304301-S0040		CITY DRAWING NUMBER		ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING		SHEET 40 OF 42	
SPANS 22-27 CRANE REMOVAL OPTION				40			



1 NORTH APPROACH - PLAN  
- - 1 : 250

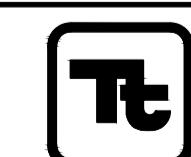


2 NORTH APPROACH - EAST ELEVATION  
- - 1 : 250

# NORTH APPROACH - CROSS SECTION

ISSUED FOR  
**INFORMATION  
ONLY**

NOT TO BE  
USED FOR  
CONSTRUCTION



# **TETRA TECH**

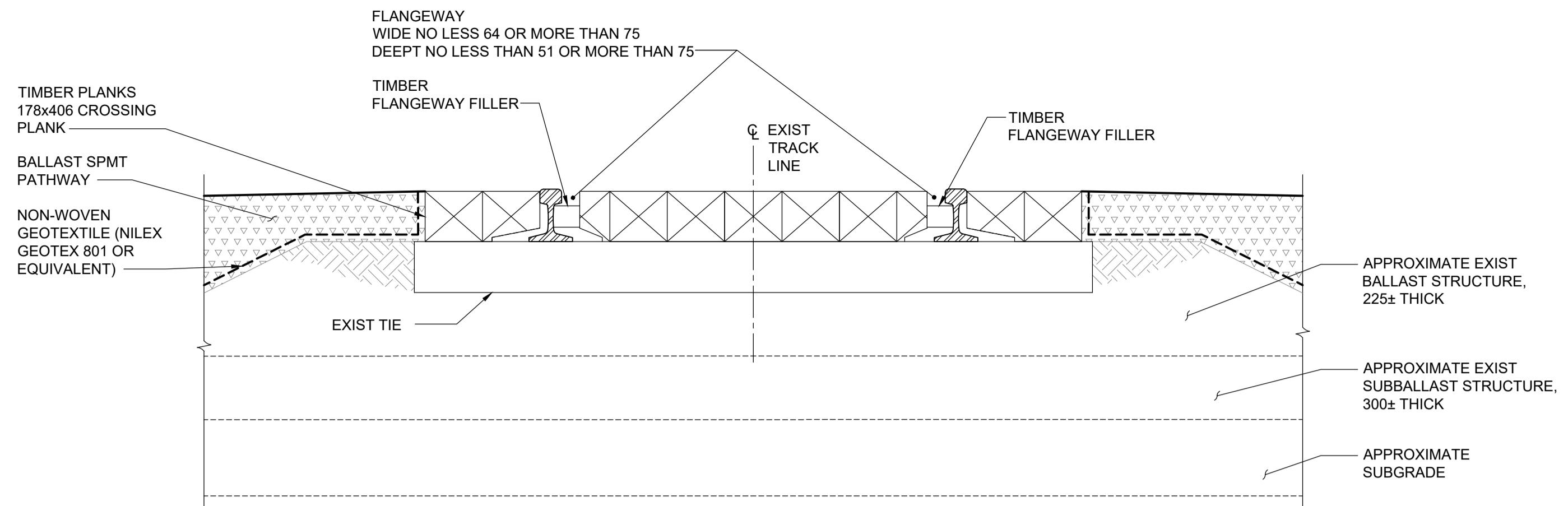
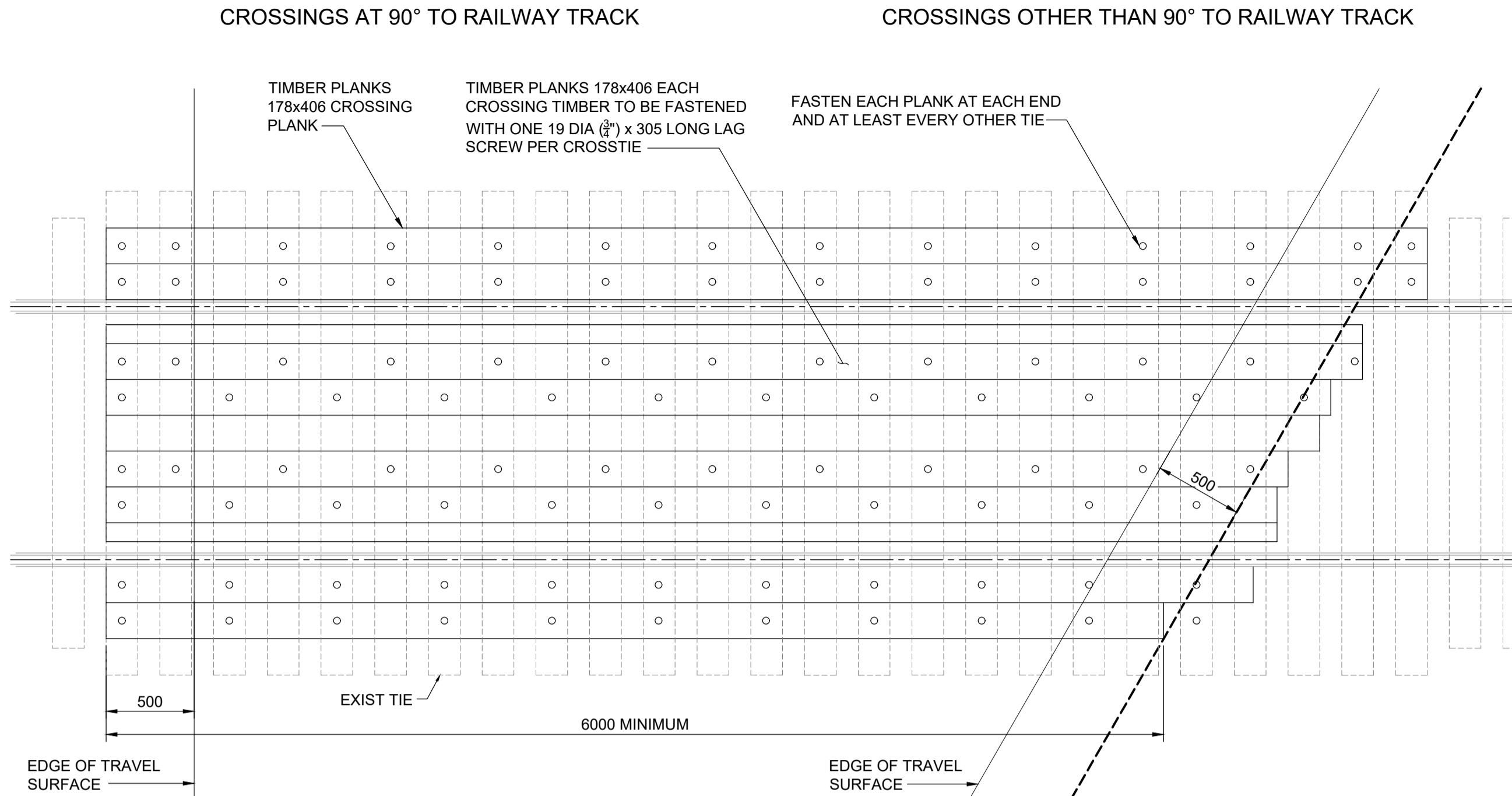
DATE	
	PRELIMINARY DRAWING
	NOT TO BE USED FOR CONSTRUCTION
	CONSULTANT DRAWING NO.
	704-INF.MBI0304301-S0041



# THE CITY OF WINNIPEG

## PUBLIC WORKS DEPARTMENT

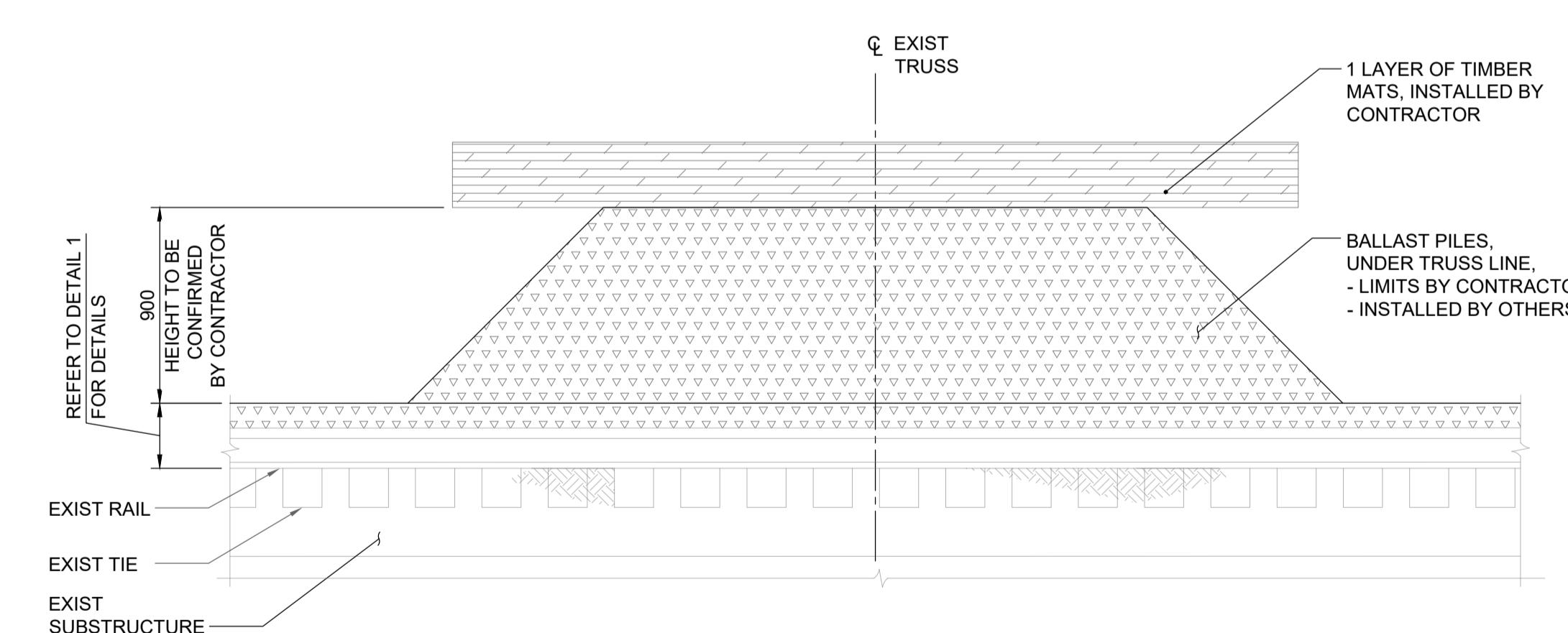
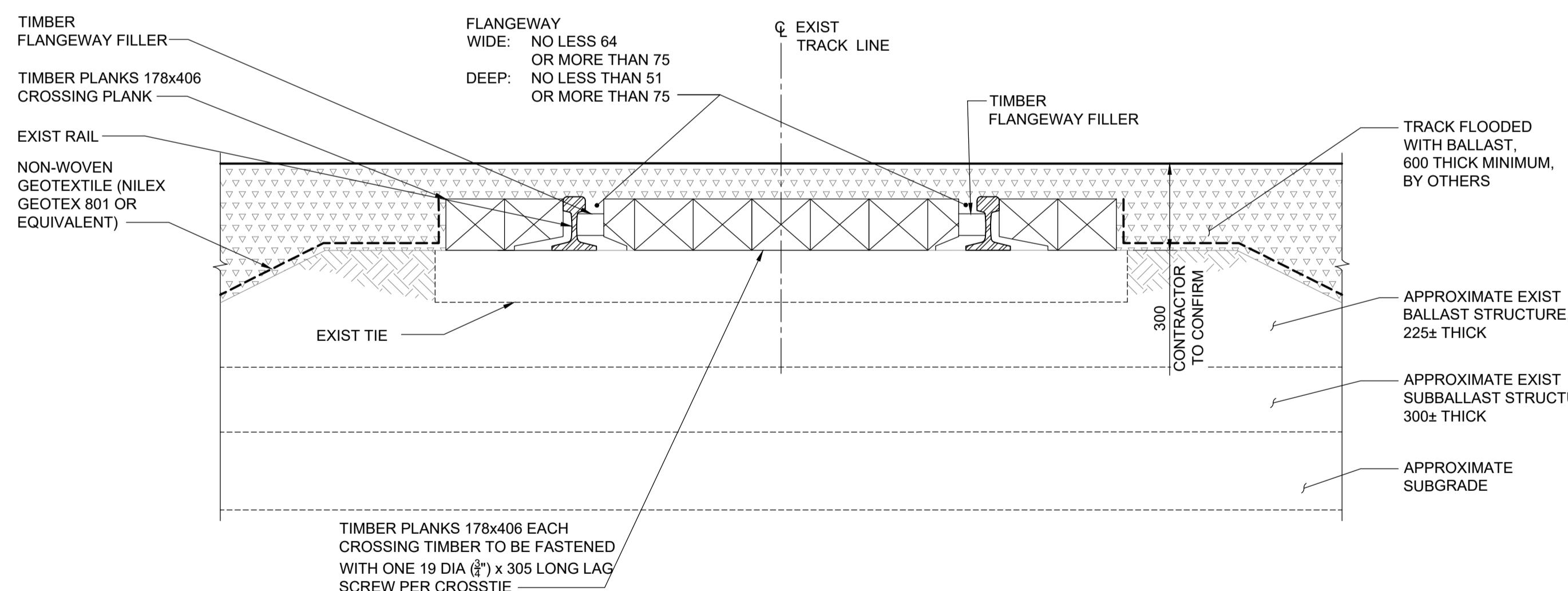
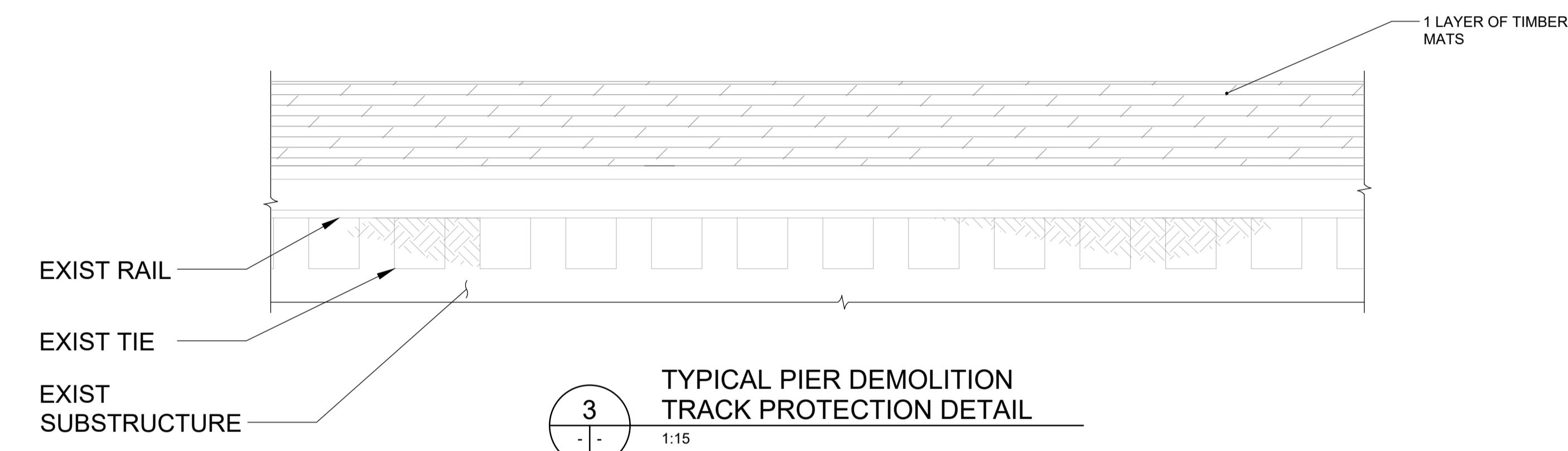
<b>ARLINGTON BRIDGE</b> <b>PHASE 4 - DECOMMISSIONING</b>	<b>CITY DRAWING NUMBER</b>    
	<b>SHEET</b> <b>OF</b> <b>41</b> <b>42</b>
<b>NORTH APPROACH SPANS 28-37</b> <b>REMOVAL FROM GRADE OPTION</b>	<b>41</b>



**TYPICAL TRACK CROSSING DETAIL - PLAN VIEW**

1:25

**TYPICAL TEMPORARY CROSSING DETAIL**



**TYPICAL TRACK PROTECTION BLAST AND DROP METHOD DETAIL**

1:15

REFER TO SHEETS 32-39 FOR LIMITS

**ADDITIONAL TRACK PROTECTION FOR BLAST AND DROP METHOD DETAIL**

1:25

REFER TO SHEETS 32-39 FOR LIMITS

ISSUED FOR INFORMATION ONLY  
NOT TO BE USED FOR CONSTRUCTION

**ENGINEERS GEOSCIENTISTS MANITOBA**  
Certificate of Authorization  
Tetra Tech Canada Inc.  
No. 6499

B.M. ELEV.		TETRA TECH		PRELIMINARY DRAWING		THE CITY OF WINNIPEG	
DESIGNED BY	ML	REVIEWED BY	MJB	NOT TO BE USED FOR CONSTRUCTION		Winnipeg	PUBLIC WORKS DEPARTMENT
DRAWN BY	EV	APPROVED BY	MJB			ARLINGTON BRIDGE	CITY DRAWING NUMBER
SCALE:	AS NOTED	ACCEPTED BY	DATE			PHASE 4 - DECOMMISSIONING	
0 ISSUED FOR TENDER	25.12.01	MJB		CONSULTANT DRAWING NO.			
NO. REVISIONS	DATE	BY	DATE	704-INF.MB0304301-S0042		TYPICAL PROTECTION DETAILS	42