



THE CITY OF WINNIPEG

**PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION**

ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING

TENDER No. 746-2025



LOCATION PLAN



ACCEPTED BY:

BRIDGE PROJECTS ENGINEER

CITY DRAWING NUMBER:

DATE

SHEET 1

DRAWING LIST

SHEET NUMBER	DRAWING DESCRIPTION
1	COVER SHEET
2	DRAWING LIST AND GENERAL NOTES
3	OVERALL SITE PLAN
4	PARTIAL SITE PLAN
5	PARTIAL SITE PLAN
6	PARTIAL SITE PLAN
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8	SOUTH APPROACH ACCESS PLAN - REMOVAL FROM GRADE OPTION
9	SOUTH APPROACH SPANS 1-12 & 13 - REMOVAL FROM GRADE OPTION
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40	SPANS 22-27 - CRANE REMOVAL OPTION
41	NORTH APPROACH SPANS 28-37 - REMOVAL FROM GRADE OPTION
42	TYPICAL PROTECTION DETAILS

GENERAL NOTES:

- THESE DRAWINGS SHALL BE READ IN CONJUNCTION WITH THE CONTRACT SPECIFICATIONS.
- THE GEOMETRY, LAYOUT, AND ANY DETAILS DEPICTING THE EXISTING STRUCTURE ARE BASED ON EXISTING DRAWINGS AND LIMITED FIELD SURVEY DATA. THE CONTRACTOR SHALL REFER TO RECORD DRAWINGS FOR ADDITIONAL STRUCTURE INFORMATION. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO FIELD VERIFY ALL NECESSARY DIMENSIONS SUCH THAT THE WORK CAN BE CONSTRUCTED AS SHOWN ON THESE DRAWINGS. THE CONTRACTOR SHALL REPORT ANY DISCREPANCIES TO THE CONTRACT ADMINISTRATOR PRIOR TO DECOMMISSIONING.
- METHODS AND OPTIONS SHOWN ON THESE DRAWINGS ARE CONCEPTUAL AND ARE NOT FINALIZED.
- DO NOT SCALE OFF DRAWINGS.

DECOMMISSIONING OPTIONS:

OPTION A -

PART A: 2026 WORKS

- REMOVE SOUTH APPROACH SPANS (1-13) FROM GRADE BY CONVENTIONAL DEMOLITION METHODS.
- REMOVE SOUTH APPROACH SUBSTRUCTURE UNITS AND ABUTMENT AS SHOWN ON THE DRAWINGS.
- REMOVE PRATT SPANS (14-18) BY SELF PROPELLED MODULAR TRANSPORTERS (SPMT) GANTRY METHOD.
- REMOVE PIERS 1-5 AS SHOWN ON THE DRAWINGS.

PART B: 2027 WORKS

- REMOVE CAMELBACK SPANS (19-21) VIA SELF PROPELLED MODULAR TRANSPORTERS (SPMT) METHOD.
- REMOVE NORTH SPANS (22-27) BY CRANE POSITIONED ON JARVIS AVENUE.
- REMOVE NORTH APPROACH SPANS (28-37) FROM GRADE BY CONVENTIONAL DEMOLITION METHODS.
- REMOVE NORTH APPROACH SUBSTRUCTURE UNITS AND ABUTMENT AS SHOWN ON THE DRAWINGS.

OPTION B -

PART A: 2026 WORKS

- REMOVE SOUTH APPROACH SPANS (1-13) FROM GRADE BY CONVENTIONAL DEMOLITION METHODS.
- REMOVE SOUTH APPROACH SUBSTRUCTURE UNITS AND ABUTMENT AS SHOWN ON THE DRAWINGS.
- REMOVE PRATT SPANS (14-17) BY SELF PROPELLED MODULAR TRANSPORTERS (SPMT) GANTRY METHOD.
- REMOVE PIERS 1-5 AS SHOWN ON THE DRAWINGS.

PART B: 2027 WORKS

- REMOVE PRATT TRUSS SPAN 18 BY BLAST AND DROP METHOD.
- REMOVE CAMELBACK SPANS (19-21) BY BLAST AND DROP METHOD.
- REMOVE NORTH SPANS (22-27) BY CRANE POSITIONED ON JARVIS AVENUE.
- REMOVE NORTH APPROACH SPANS (28-37) FROM GRADE BY CONVENTIONAL DEMOLITION METHODS.
- REMOVE NORTH APPROACH SUBSTRUCTURE UNITS AND ABUTMENT AS SHOWN ON THE DRAWINGS.

DECOMMISSIONING NOTES:

- SUBSTRUCTURE UNITS TO BE REMOVED SHALL BE REMOVED 1m BELOW GRADE. CRUSHED 20mm DOWN LIMESTONE TO BE PLACED IN EXCAVATION, COMPACTED IN 300mm LIFTS.
- ALL DECOMMISSIONING MATERIALS TO BE REMOVED OFF SITE AND DISPOSED OF PROPERLY.
- ARLINGTON RAMP ROADWAY SHALL BE REMOVED AS PER NOTED LIMITS.
- CPKC WILL PREPARE SPMT PATHWAYS WITHIN THE YARD AND WILL INSTALL CROSSING PLANKS AS SHOWN WITHIN THIS DRAWING SET. LIMITS OF PLANKING/SPMT PATHWAYS TO BE DETERMINED BY CONTRACTOR.
- CONTRACTOR TO DESIGN SPMT PATHWAYS AND MATERIALS REQUIREMENTS.
- CONTRACTOR RESPONSIBLE FOR CONFIRMING UNDERGROUND UTILITIES.
- CPKC WILL PLACE BLAST AND DROP PROTECTION MATERIALS WITHIN THE YARD.
- CONTRACTOR TO DESIGN BLAST AND DROP PROTECTION SYSTEM. CPKC TO PLACE BALLAST WITHIN YARD.
- CONTRACTOR RESPONSIBLE FOR ALL PRELIMINARY ENGINEERING TO LIFT AND MOVE TRUSS SPANS INCLUDING ANY REQUIRED STRENGTHENING AND TEMPORARY BRACING.
- CITY OF WINNIPEG TO REMOVE ALUMINUM RAILING SYSTEM PRIOR TO DECOMMISSIONING.
- REMAINING PIERS ARE PROPOSED TO BE REMOVED DURING RECONSTRUCTION OF THE REPLACEMENT BRIDGE, BY OTHERS.
- SIZE OF SUBSTRUCTURE UNITS BELOW GRADE MAY VARY FROM RECORD DRAWINGS.
- NEEDLES AND OTHER HAZARDS OBSERVED ON BRIDGE AND SHALL BE REMOVED PRIOR TO DECOMMISSIONING FOR SAFETY REASONS.
- VEHICLES USE OF THE EXISTING BRIDGE LIMITED TO PASSENGER VEHICLES AND QUARTER TON TRUCKS. LARGE VEHICLES OR EQUIPMENT WILL NEED TO BE REVIEWED BY THE CONTRACT ADMINISTRATOR FOR ACCEPTANCE.
- LIMITED ORIGINAL DRAWINGS OF THE EXISTING STRUCTURE AND PREVIOUS REPAIRS WILL BE MADE AVAILABLE TO THE CONTRACTOR.
- LOAD RATING ANALYSIS FINITE ELEMENT MODEL WILL BE MADE AVAILABLE TO THE CONTRACTOR. USE OF LOAD RATING MODEL AT CONTRACTOR'S RISK.
- RECENT INSPECTION REPORTS AND OTHER IMPORTANT DOCUMENTS WILL BE MADE AVAILABLE TO THE CONTRACTOR.
- STATED MASS FOR EACH SPAN INCLUDES THE CONCRETE DECK AND SIDEWALK.
- MASS STATED IS APPROXIMATE.
- CONTRACTOR LOAD RATING ANALYSIS TO BE IN ACCORDANCE TO CSA CHBDC 2025.
- TRUSS ASSESSMENT FOR SPMT MOVE TO BE IN ACCORDANCE WITH FEDERAL HIGHWAY ADMINISTRATION MANUAL TO USE OF SELF PROPELLED MODULAR TRANSPORTERS (SPMT) TO REMOVE AND REPLACE BRIDGES JUNE 2007.
- SPMT UNITS AND CONFIGURATIONS SHOWN ARE CONCEPTUAL. CONTRACTOR TO DEVELOP SPMT CONCEPT AND LIFTING APPARATUS CAPABLE OF SAFELY LIFTING AND MOVING THE TRUSS SPANS.

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MANITOBA

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B.M. ELEV.			
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NO.	REVISIONS	DATE	BY

 TETRA TECH			
DESIGNED BY	ML	REVIEWED BY	MJB
DRAWN BY	EV	APPROVED BY	MJB
SCALE:	AS NOTED		ACCEPTED BY DATE
	DATE	2025.12.01	

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704-INF-MBIO304301-S0002



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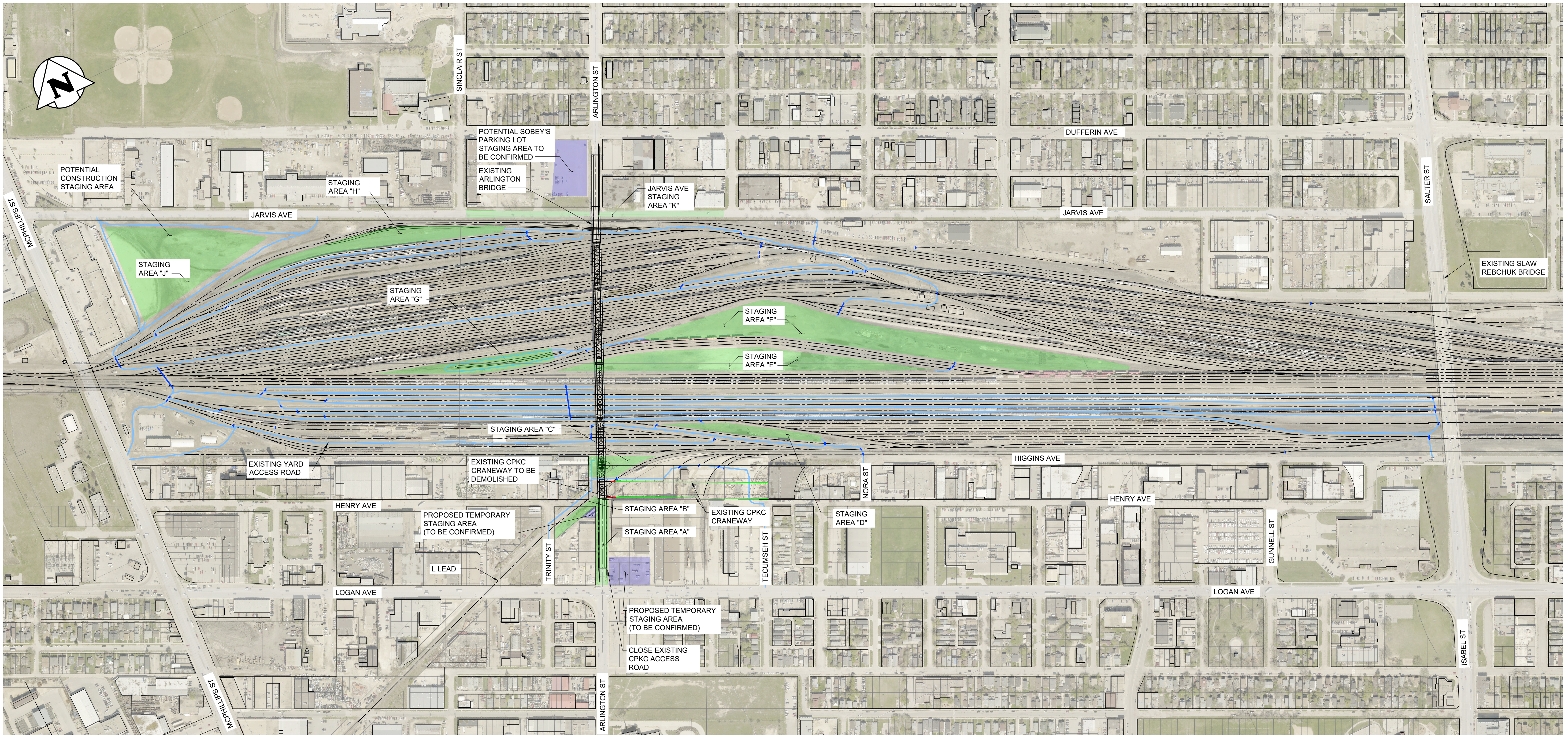
ARLINGTON BRIDGE
PHASE 4 - DECOMMISSIONING

CITY DRAWING NUMBER

SHEET 2 OF 42

DRAWING LIST AND GENERAL NOTES

2



- LEGEND:
- EXISTING YARD ACCESS ROADS
 - EXISTING AT GRADE CROSSING
 - CONSTRUCTION STAGING AREA
 - POTENTIAL STAGING AREA TO BE CONFIRMED WITH PROPERTY OWNER

NOTES:
- WORK AREAS AND WORK PLAN FOR EACH ITEM IS DESCRIBED WITHIN THIS DRAWING SET

1 OVERALL SITE PLAN
1 : 3000

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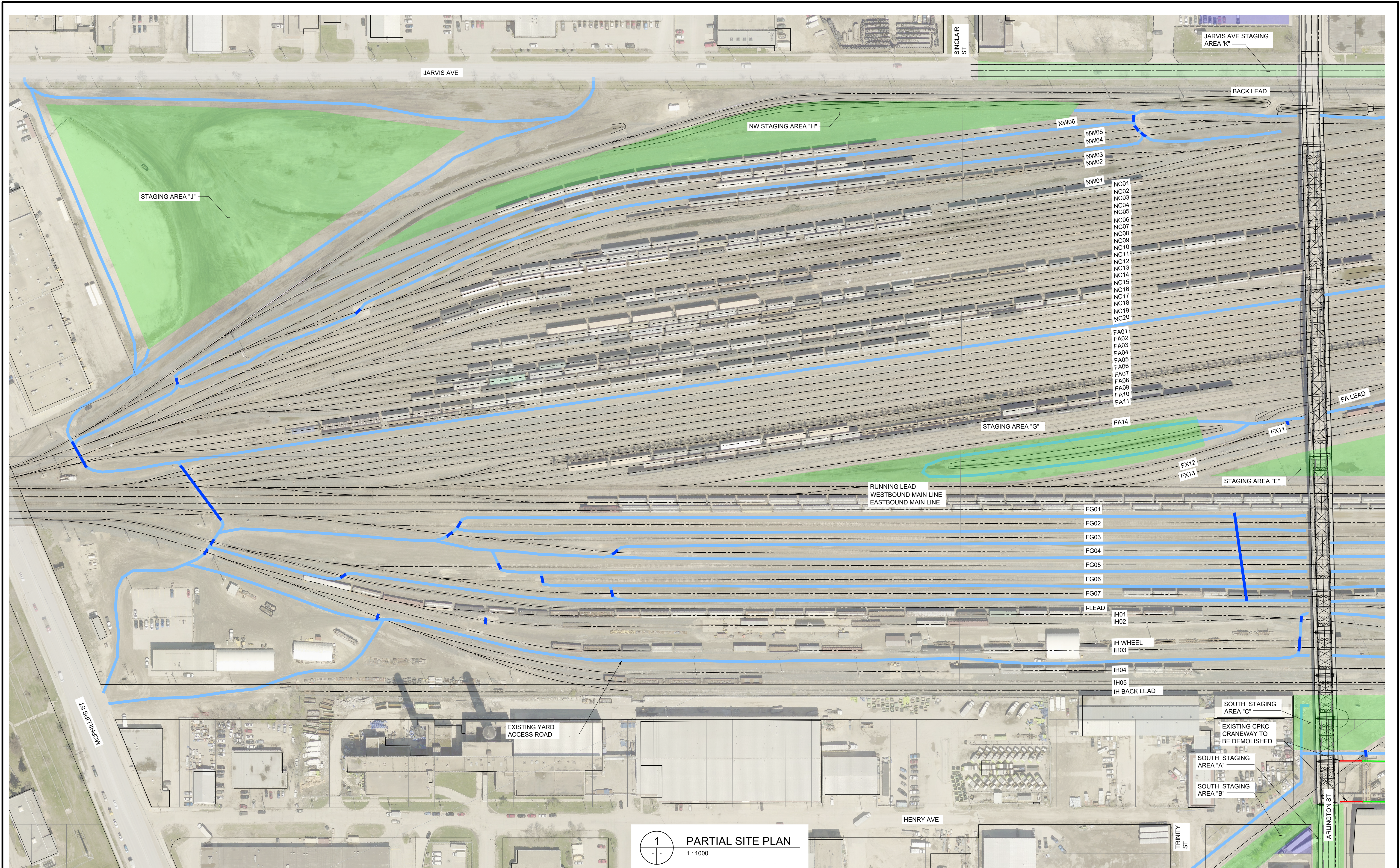
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THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT	
ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	CITY DRAWING NUMBER
OVERALL SITE PLAN	SHEET 3 OF 42
	3



LEGEND:

- EXISTING YARD ACCESS ROADS
- EXISTING AT GRADE CROSSING
- CONSTRUCTION STAGING AREA
- POTENTIAL STAGING AREA TO BE CONFIRMED WITH PROPERTY OWNER

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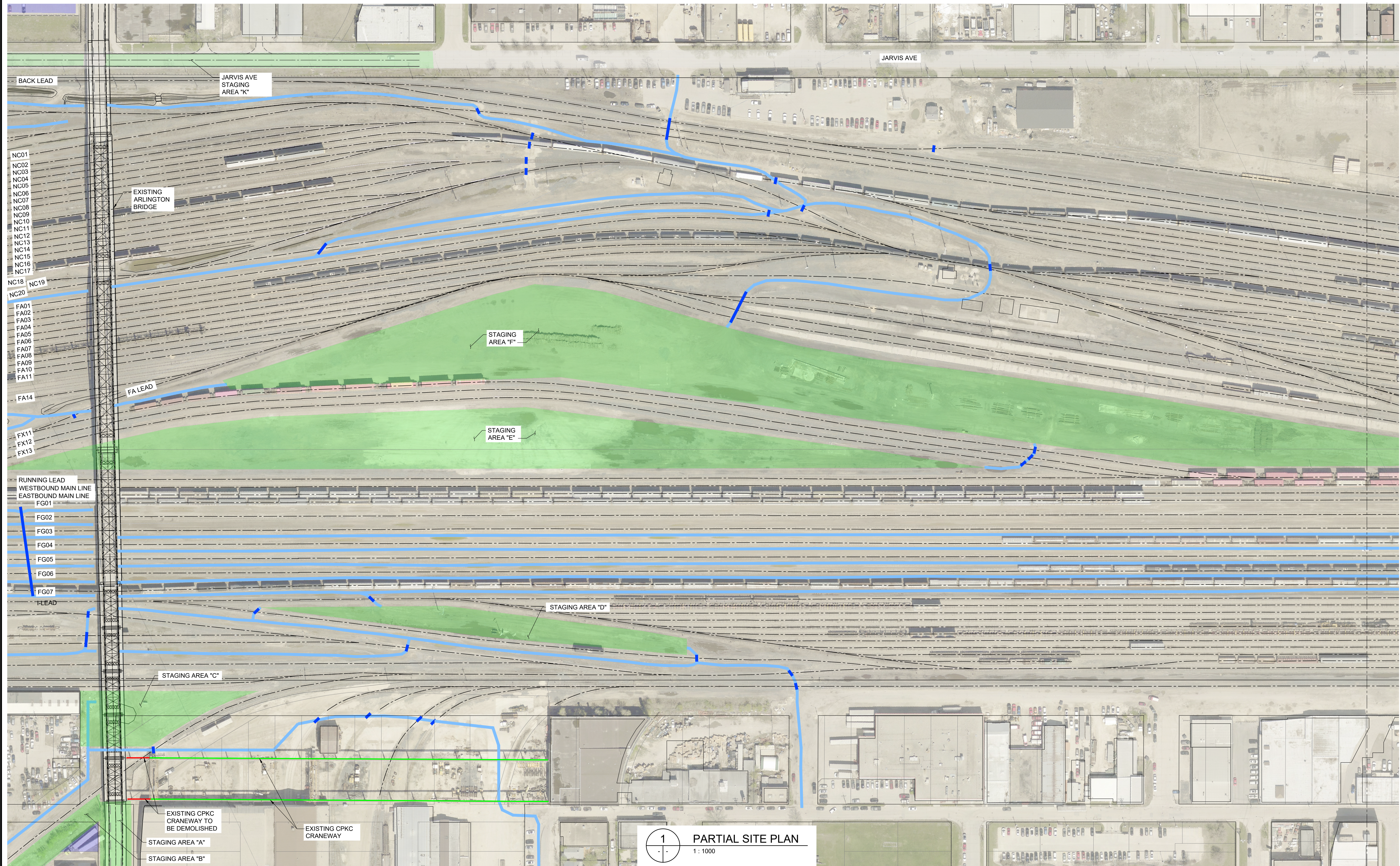
PARTIAL SITE PLAN
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ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	CITY DRAWING NUMBER
PARTIAL SITE PLAN	SHEET 4 OF 42
	4



LEGEND:

- EXISTING YARD ACCESS ROADS
- EXISTING AT GRADE CROSSING
- CONSTRUCTION STAGING AREA
- POTENTIAL STAGING AREA TO BE CONFIRMED WITH PROPERTY OWNER

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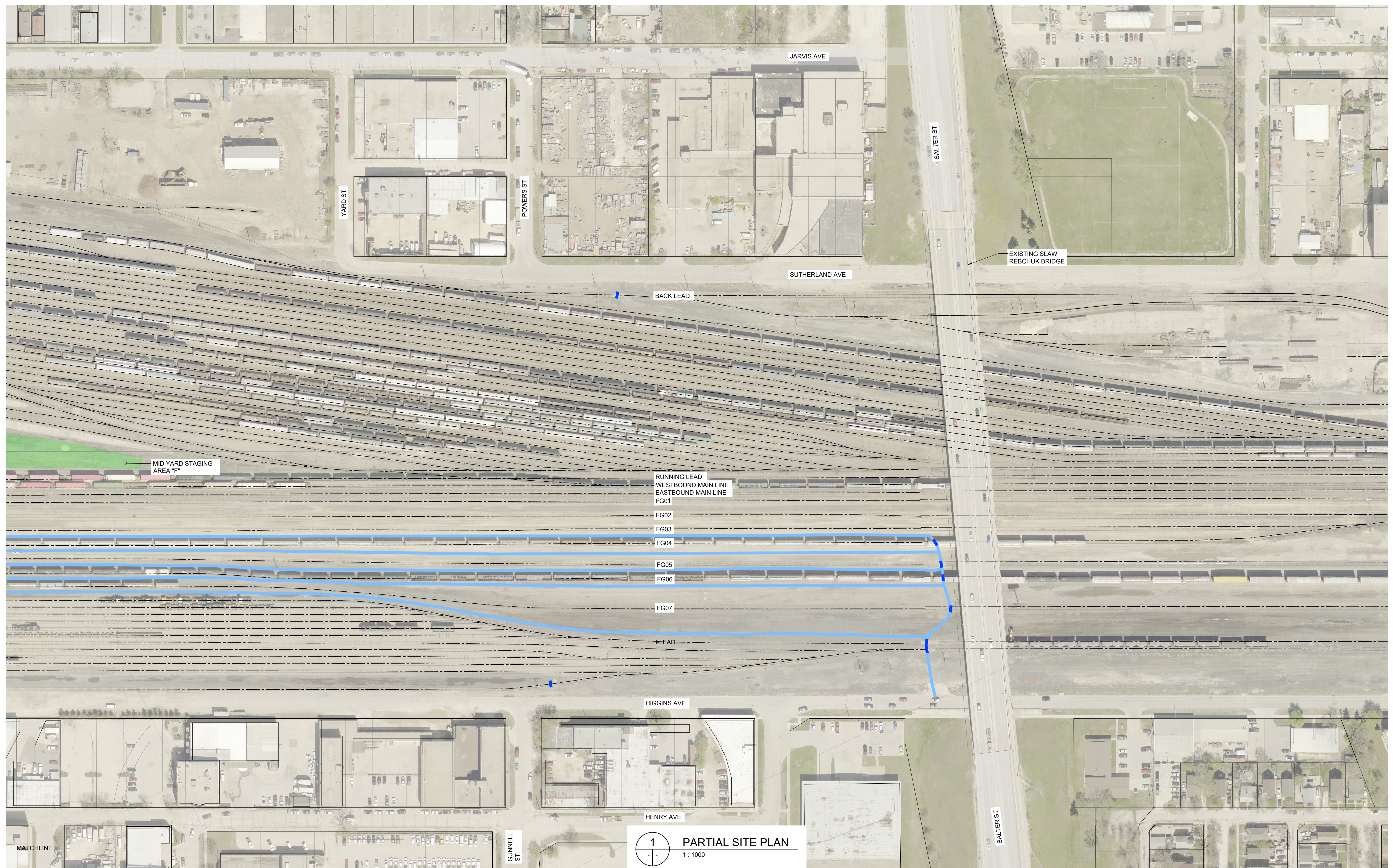
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PARTIAL SITE PLAN
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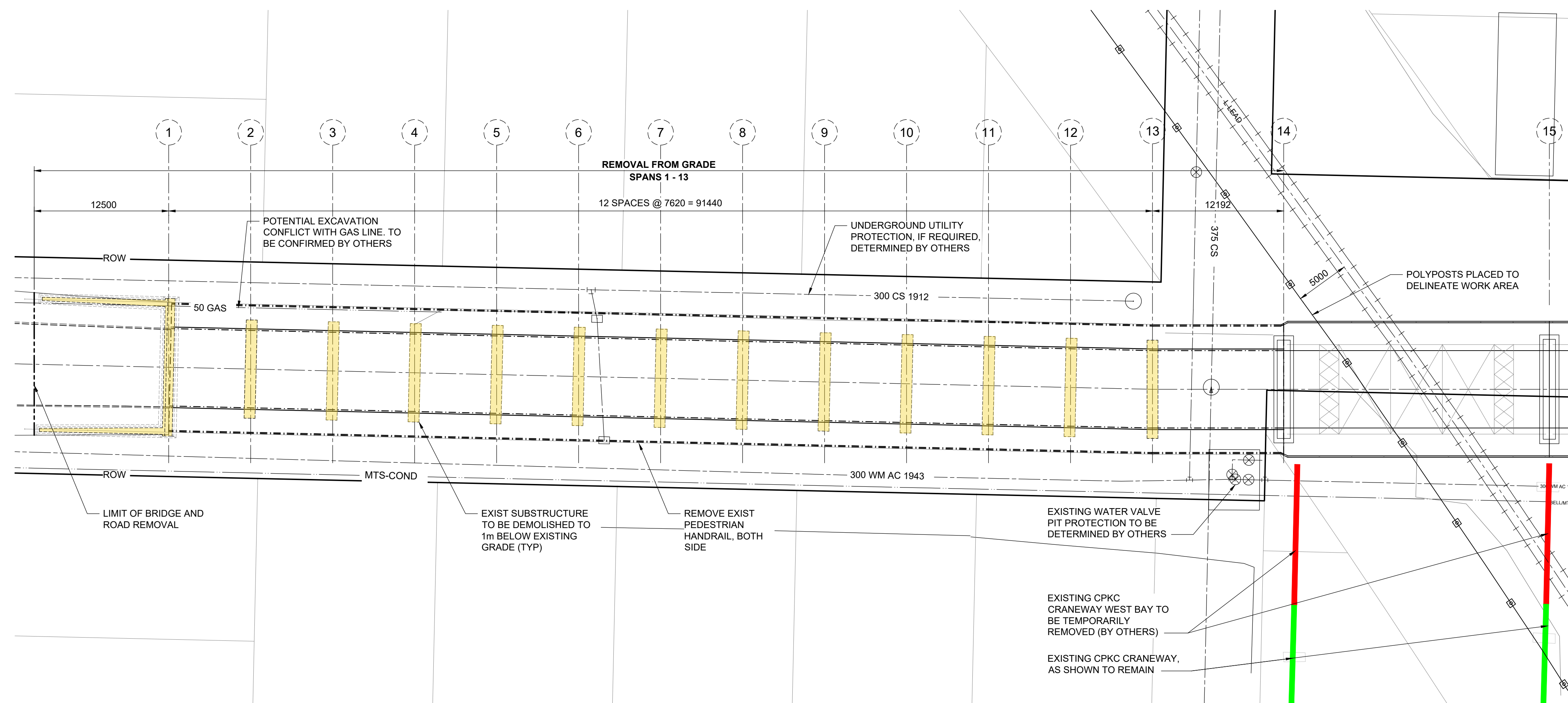
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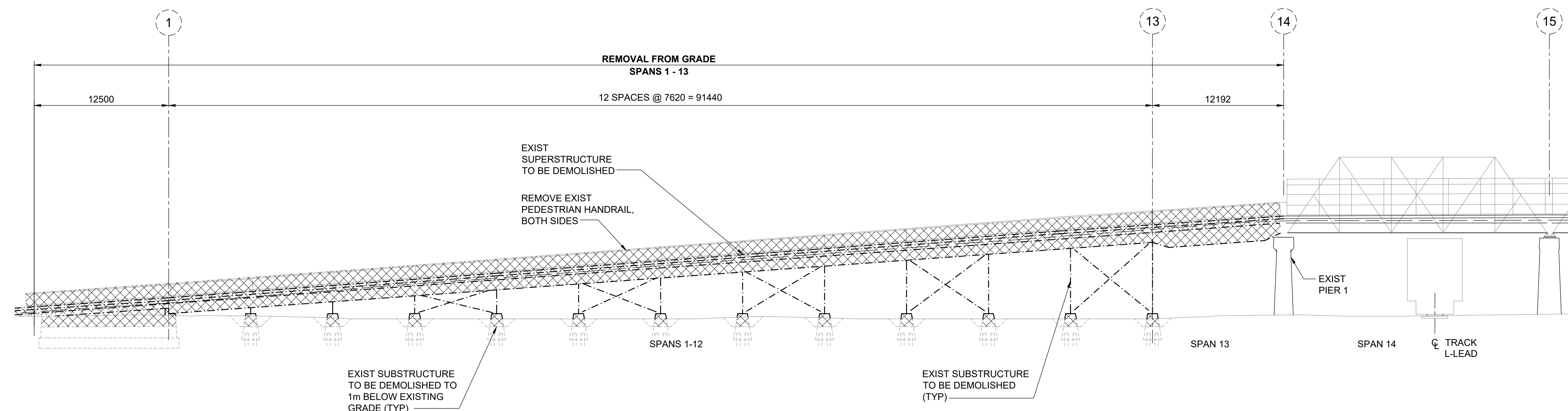
THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT	
ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	
PARTIAL SITE PLAN	
CITY DRAWING NUMBER	
SHEET 5 OF 42	
5	



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ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	CITY DRAWING NUMBER
	SHEET 6 OF 42
PARTIAL SITE PLAN	6



1 SOUTH APPROACH - PLAN
1 : 250



2 SOUTH APPROACH - EAST ELEVATION
1 : 250

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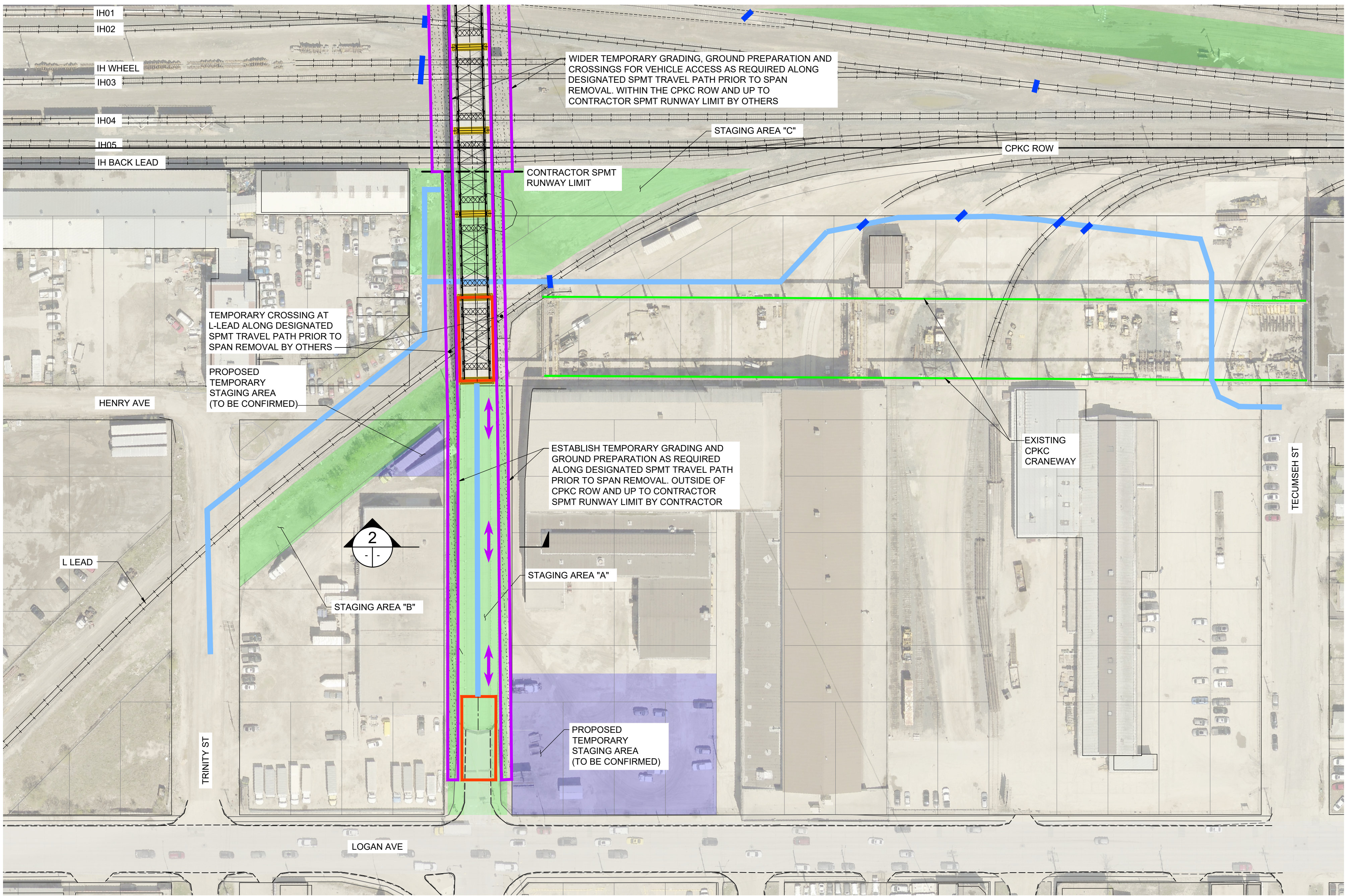
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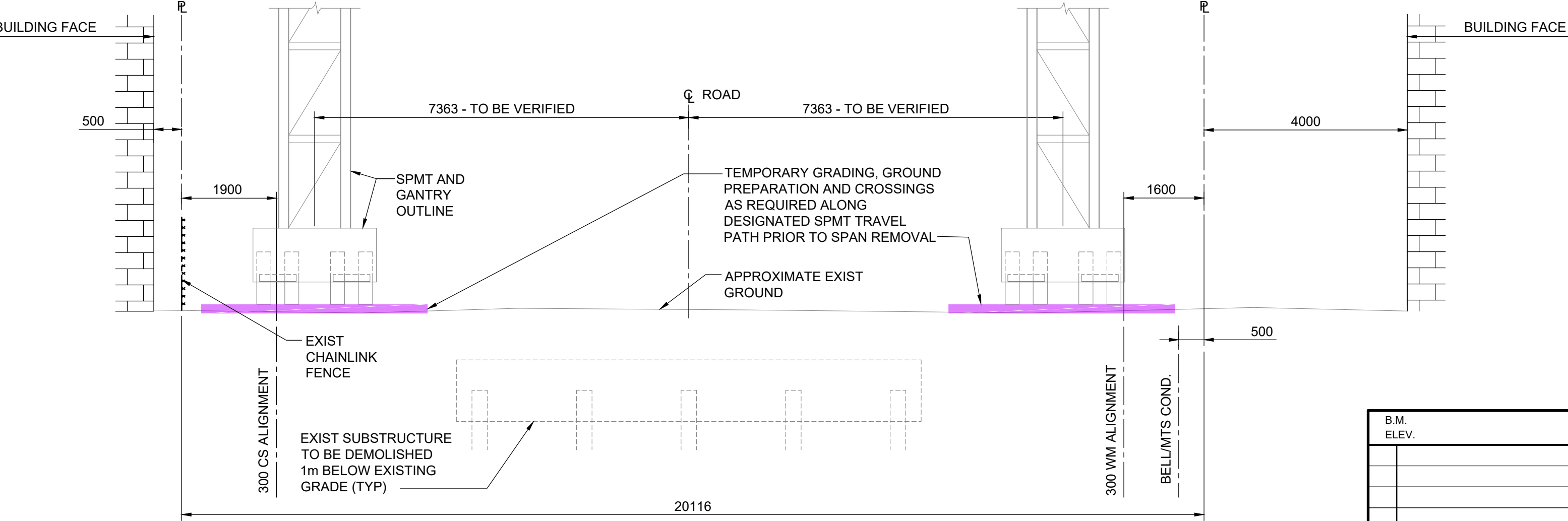
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THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT	
ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	CITY DRAWING NUMBER
SOUTH APPROACH SPANS 1- 12 & 13 REMOVAL FROM GRADE OPTION	SHEET 9 OF 42
	9



1 SPAN 14 ACCESS PLAN
1 : 750



2 SECTION
1 : 75

SEQUENCE OF WORK

PRE-DEMOLITION WORKS:

- INSTALL TEMPORARY GRADING AND CROSSINGS FROM LOGAN AVENUE TO PIER 6 ALONG DESIGNATED SPMT TRAVEL
- PERFORM LOCALIZED TRUSS MODIFICATIONS/REMOVALS TO FACILITATE TRUSS REMOVAL
- ERECT AND SETUP SELF-PROPELLED MODULAR TRANSPORTS (SPMT) AND GANTRY CRANES
- PRIOR TO SPAN REMOVAL WORKBLOCK, ALL TRAINS SHALL BE REMOVED WITHIN SPMT TRAVEL PATH

TRUSS REMOVAL WORKS:

- ENSURE L-LEAD IS REMOVED FROM SERVICE
- POSITION SPMT TO SPAN 14
- LIFT AND MOVE SPAN FROM BRIDGE LOCATION TO STAGING AREA "A"
- LOWER SPAN ONTO GROUND SURFACE
- RETURN SERVICE TO THE L-LEAD TRACK ONCE CLEAR OF RIGHT OF WAY

DEMOLITION WORKS:

- DISMANTLE/DEMOLISH TRUSS COMPONENTS ON GROUND, WHILE SIMULTANEOUSLY HAULING THEM OFF SITE
- HAULING EQUIPMENT TO REMOVE TRUSS COMPONENTS FROM STAGING AREAS "A" VIA LOGAN AVE

POST-DEMOLITION WORKS:

- CLEAN AND RESTORE GROUND SURFACE TO PRE-DEMOLITION STATE IF/AS REQUIRED

LEGEND:

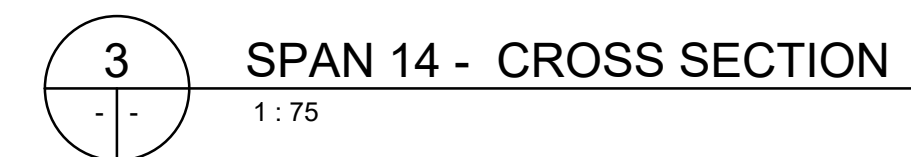
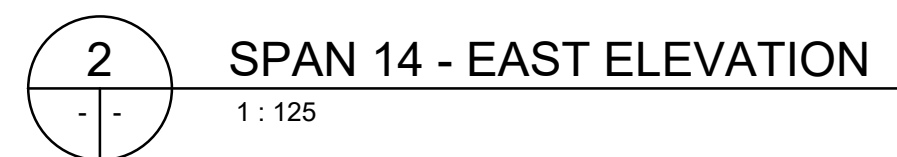
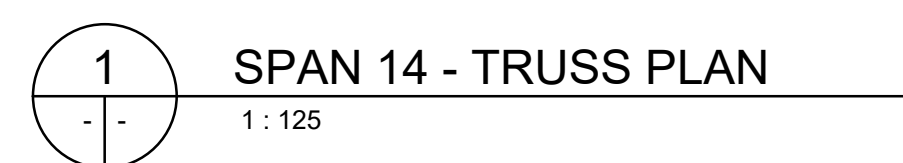
- EXISTING PIER LOCATIONS
- ACCESS ROUTE FOR SPAN 14
- EXISTING AT GRADE CROSSING
- CONSTRUCTION STAGING AREA
- OVERALL WORK AREA
- SPMT TRAVEL PATH


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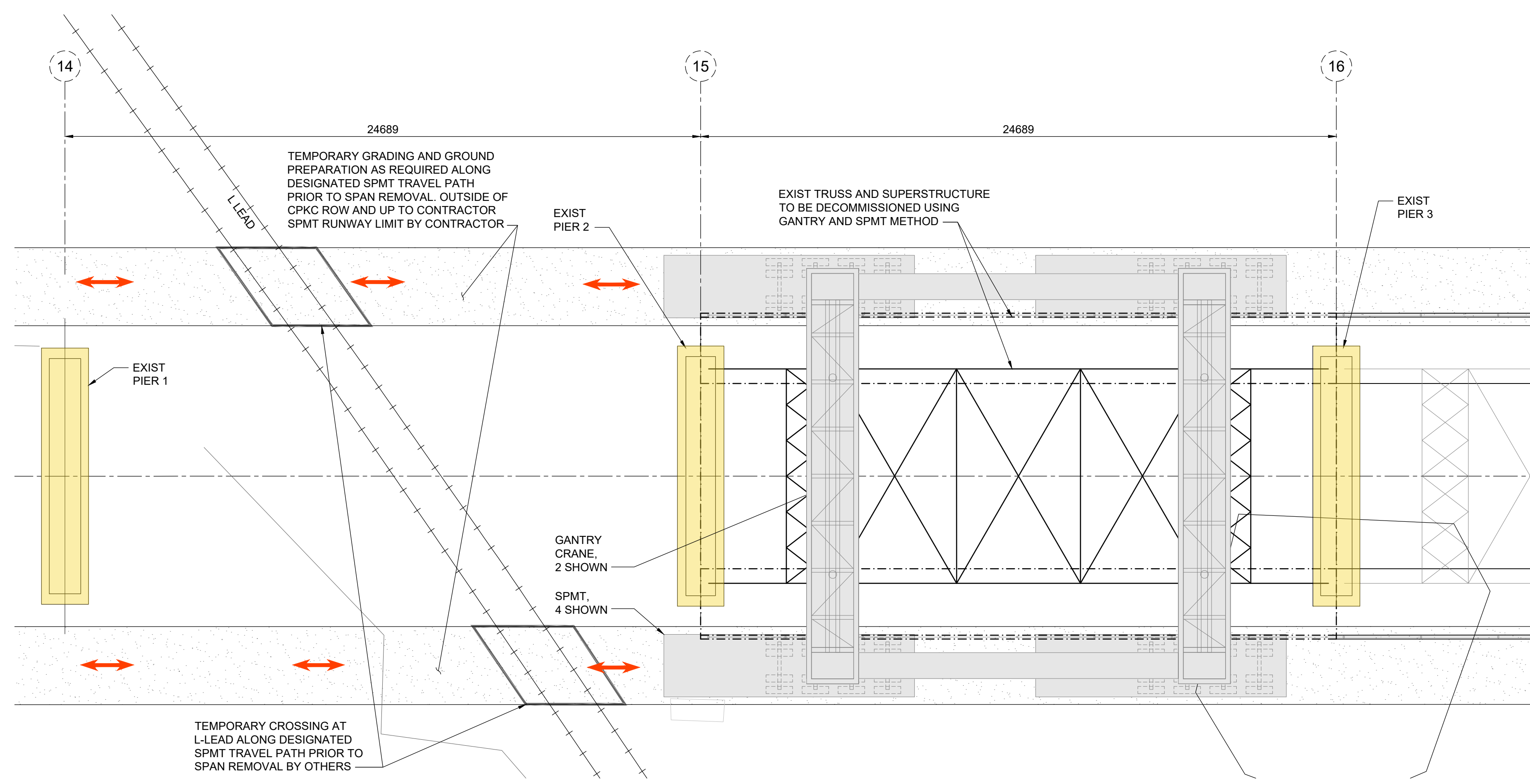
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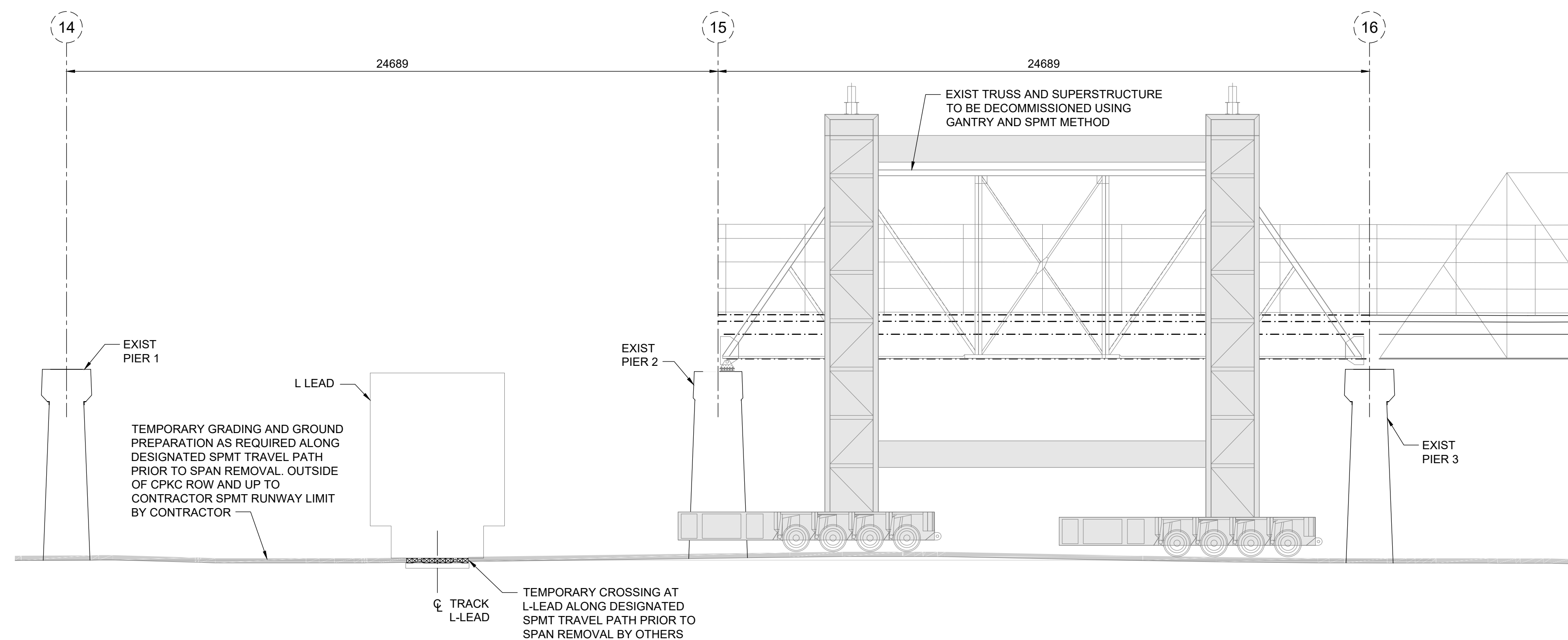
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			SCALE: AS NOTED	ACCEPTED BY	DATE	CONSULTANT DRAWING NO. 704-INF-MB10304301-S0010		SHEET 10 OF 42
0	ISSUED FOR TENDER	25.12.01	MJB			SPAN 14 ACCESS PLAN SPMT AND GANTRY REMOVAL OPTION		10
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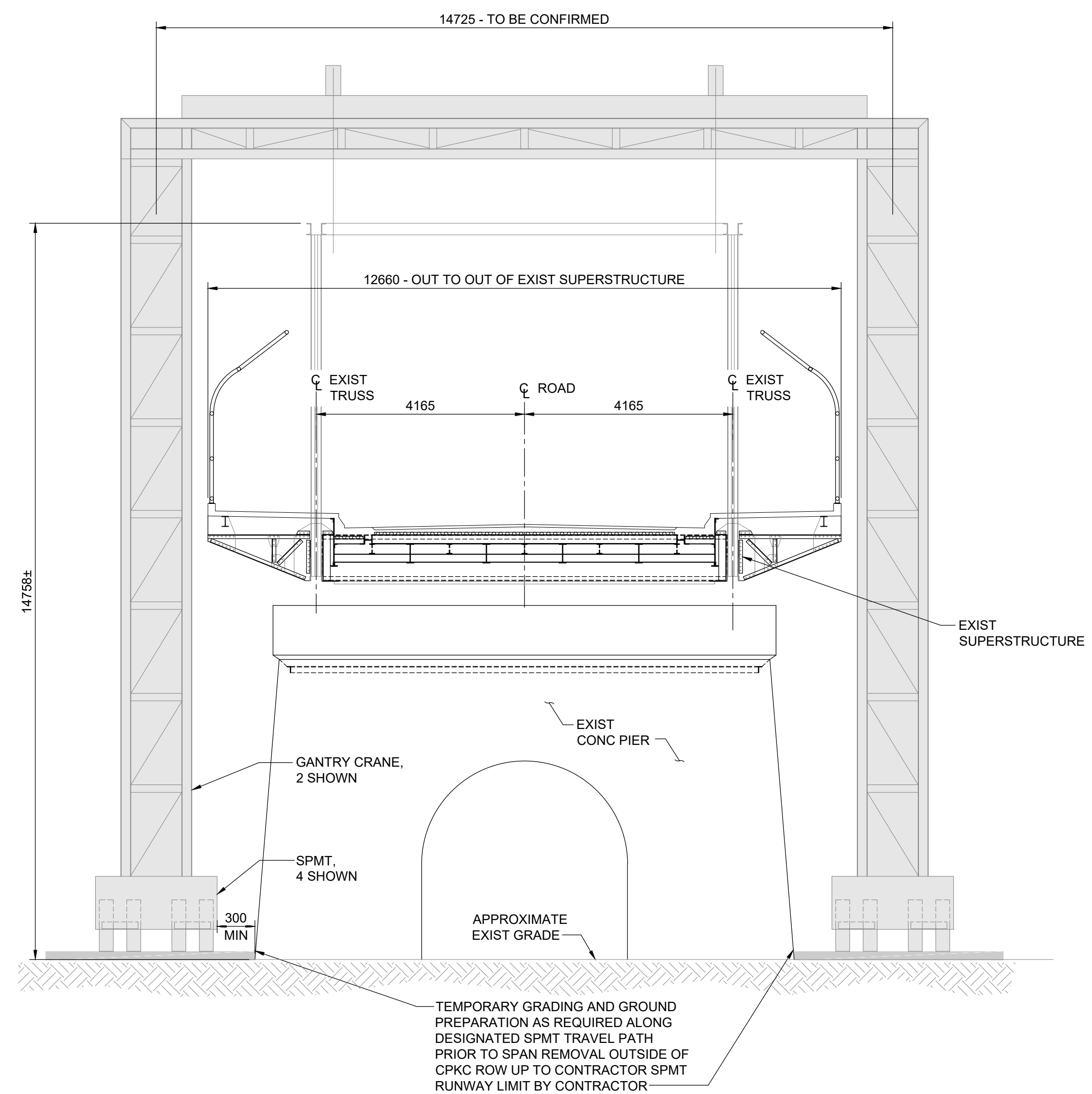
 <div style="text-align: center;"> <h1 style="margin: 0;">THE CITY OF WINNIPEG</h1> <h2 style="margin: 0;">PUBLIC WORKS DEPARTMENT</h2> </div>	
ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	CITY DRAWING NUMBER
	SHEET 11 OF 42
SPAN 14 SPMT AND GANTRY REMOVAL OPTION	11



1 SPAN 15 - TRUSS PLAN
1 : 125



2 SPAN 15 - EAST ELEVATION
1 : 125



3 SPAN 15 - CROSS SECTION
1 : 75

PRATT TRUSS ESTIMATED
MASS: 200,000 kg

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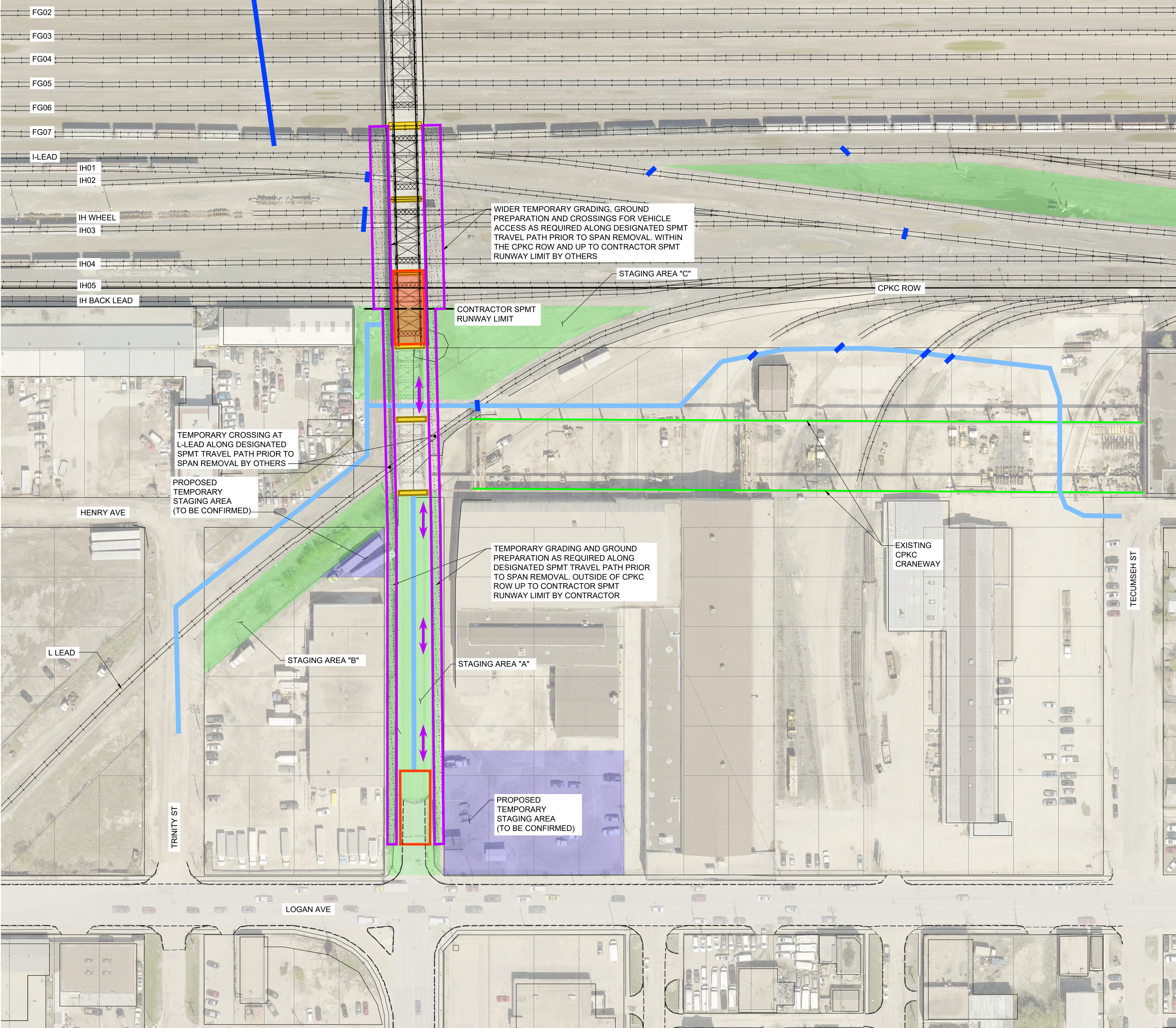
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THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT	
ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	CITY DRAWING NUMBER
SPAN 15 SPMT AND GANTRY REMOVAL OPTION	SHEET 13 OF 42
	13



SEQUENCE OF WORK

PRE-WORKBLOCK DEMOLITION WORKS:

- PERFORM LOCALIZED TRUSS MODIFICATIONS/REMOVALS TO FACILITATE TRUSS REMOVAL
- ERECT AND SETUP SELF-PROPELLED MODULAR TRANSPORTS (SPMT) AND GANTRY CRANES
- PRIOR TO SPAN REMOVAL WORKBLOCK, ALL TRAINS SHALL BE REMOVED WITHIN SPMT TRAVEL PATH

TRUSS REMOVAL WORKS:

- ENSURE L-LEAD, IH BACK LEAD, IH05, IH04 TRACKS ARE REMOVED FROM SERVICE
- POSITION SPMT TO SPAN 16
- LIFT AND MOVE SPAN FROM BRIDGE LOCATION TO STAGING AREA "A"
- LOWER SPAN ONTO GROUND SURFACE
- RETURN SERVICE TO THE IH BACK LEAD, IH05 AND IH04 TRACKS ONCE SPMT CLEARS IH BACK LEAD RIGHT OF WAY
- RETURN SERVICE TO THE L-LEAD TRACKS ONCE SPMT CLEARS L LEAD RIGHT OF WAY

DURING WORKBLOCK DEMOLITION WORKS:

- DISMANTLE/DEMOLISH TRUSS COMPONENTS ON GROUND, WHILE SIMULTANEOUSLY HAULING THEM OFF SITE
- HAULING EQUIPMENT TO REMOVE TRUSS COMPONENTS FROM STAGING AREAS "A" VIA LOGAN AVE

POST-WORKBLOCK DEMOLITION WORKS:

- CLEAN AND RESTORE GROUND SURFACE TO PRE-DEMOLITION STATE AS REQUIRED

LEGEND:

- EXISTING PIER LOCATIONS
- ACCESS ROUTE FOR SPAN 16
- EXISTING AT GRADE CROSSING
- CONSTRUCTION STAGING AREA
- OVERALL WORK AREA
- SPMT TRAVEL PATH

1 SPAN 16 ACCESS PLAN
1 : 750

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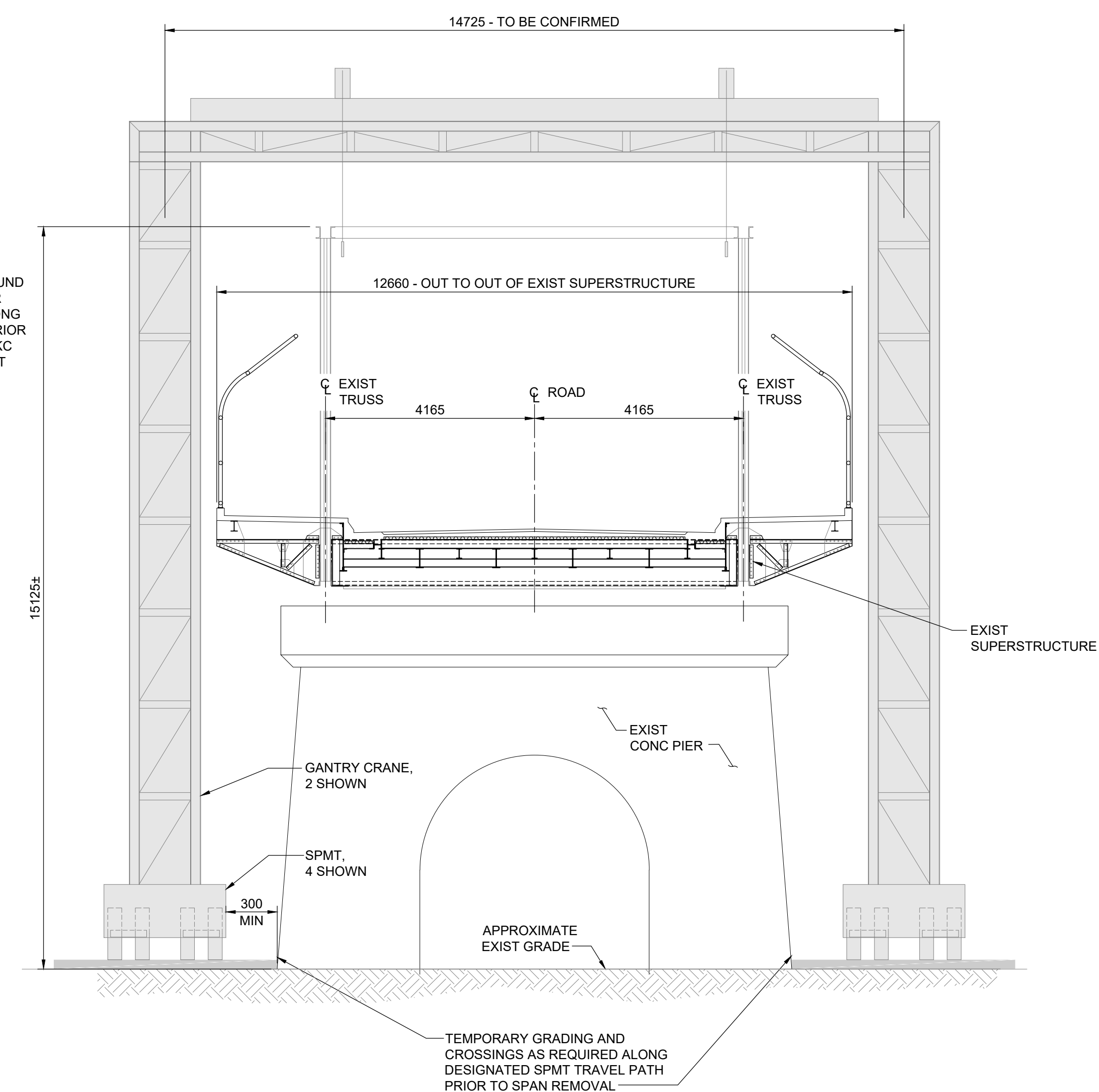
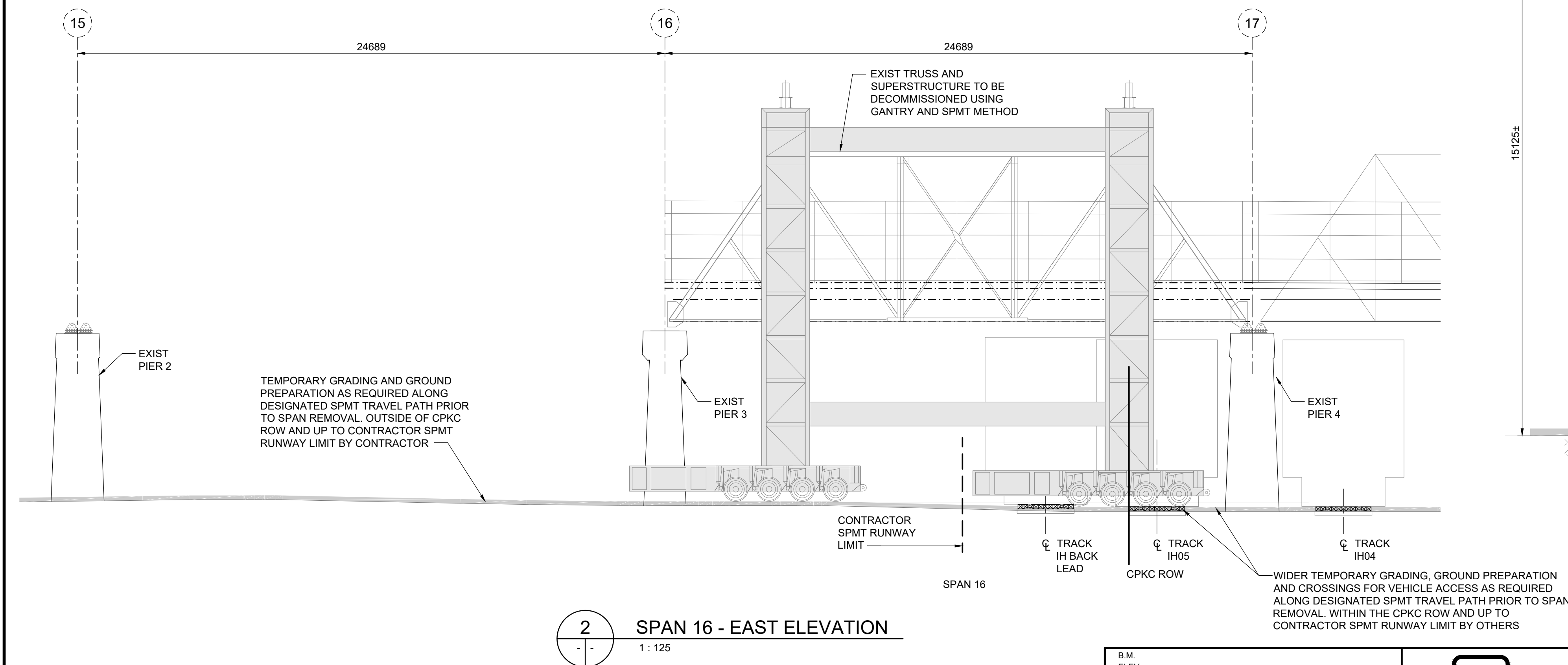
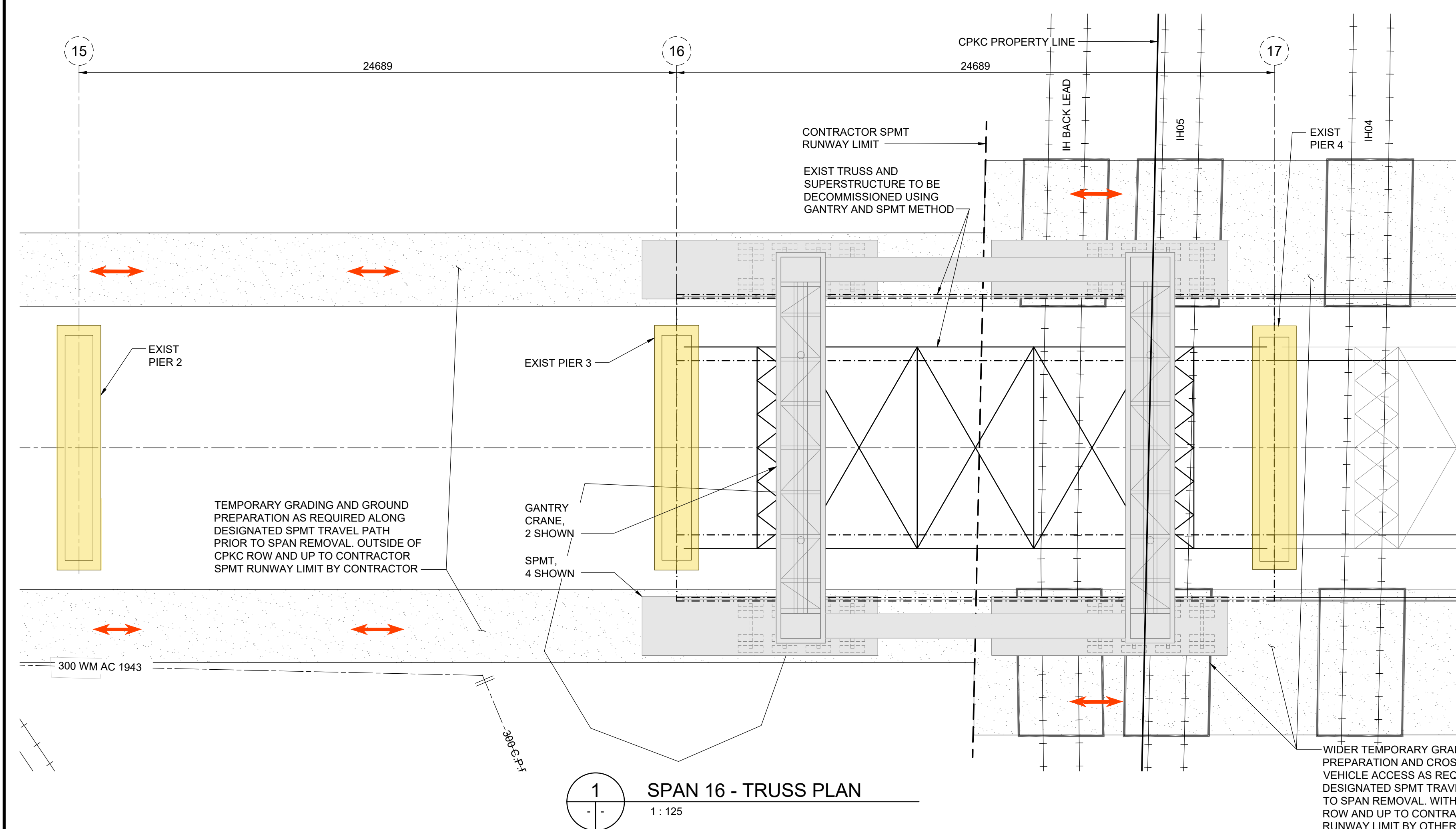
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THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT		
ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	CITY DRAWING NUMBER	
SPAN 16 ACCESS PLAN SPMT AND GANTRY REMOVAL OPTION	SHEET 14 OF 42	
	14	



PRATT TRUSS ESTIMATED
MASS: 200,000 kg

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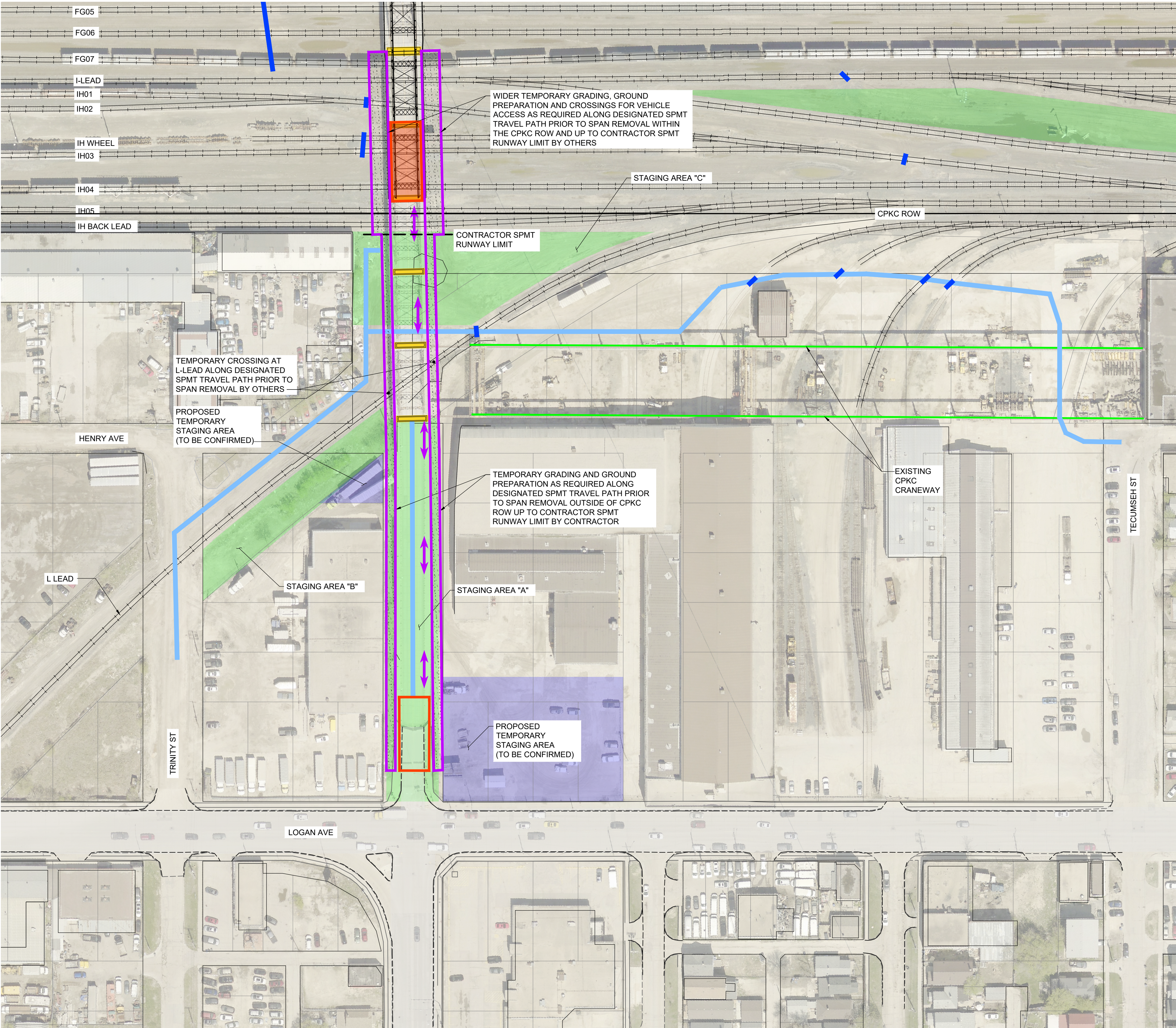
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THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT		CITY DRAWING NUMBER
ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING		SHEET 15 OF 42
SPAN 16 SPMT AND GANTRY REMOVAL OPTION		15



SEQUENCE OF WORK

PRE-WORKBLOCK DEMOLITION WORKS:

- PERFORM LOCALIZED TRUSS MODIFICATIONS/REMOVALS TO FACILITATE TRUSS REMOVAL
- ERECT AND SETUP SELF-PROPELLED MODULAR TRANSPORTS (SPMT) AND GANTRY CRANES
- PRIOR TO SPAN REMOVAL WORKBLOCK, ALL TRAINS SHALL BE REMOVED WITHIN SPMT TRAVEL PATH

TRUSS REMOVAL WORKS:

- ENSURE L-LEAD, IH BACK LEAD, IH05, IH04, IH03 AND IH WHEEL TRACKS ARE REMOVED FROM SERVICE
- POSITION SPMT TO SPAN 17
- LIFT AND MOVE SPAN FROM BRIDGE LOCATION TO STAGING AREA "A"
- LOWER SPAN ONTO GROUND SURFACE
- RETURN SERVICE TO THE IH BACK LEAD, IH05, IH04, IH03 AND IH WHEEL TRACK ONCE SPMT CLEARS IH BACK LEAD RIGHT OF WAY
- RETURN SERVICE TO THE L-LEAD TRACK ONCE SPMT CLEARS L LEAD RIGHT OF WAY

DURING WORKBLOCK DEMOLITION WORKS:

- DISMANTLE/DEMOLISH TRUSS COMPONENTS ON GROUND, WHILE SIMULTANEOUSLY HAULING THEM OFF SITE
- HAULING EQUIPMENT TO REMOVE TRUSS COMPONENTS FROM STAGING AREAS "A" VIA LOGAN AVE

POST-WORKBLOCK DEMOLITION WORKS:

- REMOVE TEMPORARY GRADING AND CROSSINGS FOR SPMT RUNWAY. CLEAN AND RESTORE GROUND SURFACE TO PRE-DEMOLITION STATE AS REQUIRED

LEGEND:

- EXISTING PIER LOCATIONS
- ACCESS ROUTE FOR SPAN 17
- EXISTING AT GRADE CROSSING
- CONSTRUCTION STAGING AREA
- OVERALL WORK AREA
- SPMT TRAVEL PATH

1 SPAN 17 ACCESS PLAN
1:750

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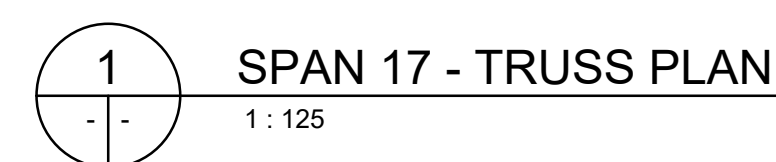
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THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT		
ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	CITY DRAWING NUMBER	
SPAN 17 ACCESS PLAN SPMT AND GANTRY REMOVAL OPTION	SHEET 16 OF 42	
		16



16

24689

17

24689

18

EXIST PIER 3

CONTRACTOR SPMT RUNWAY LIMIT

EXIST PIER 4

EXIST TRUSS AND SUPERSTRUCTURE TO BE DECOMMISSIONED USING GANTRY AND SPMT METHOD

EXIST PIER 5

TEMPORARY GRADING AND GROUND PREPARATION AS REQUIRED ALONG DESIGNATED SPMT TRAVEL PATH PRIOR TO SPAN REMOVAL, OUTSIDE OF CPKC ROW AND UP TO CONTRACTOR SPMT RUNWAY LIMIT BY CONTRACTOR

CPKC PROPERTY LINE

TRACK IH BACK LEAD

TRACK IH05

TRACK IH04

TRACK IH03

TRACK IH WHEEL

TRACK IH01

SPAN 17

WIDER TEMPORARY GRADING, GROUND PREPARATION AND CROSSINGS FOR VEH


2 SPAN 17 - EAST ELEVATION
- - 1 : 125

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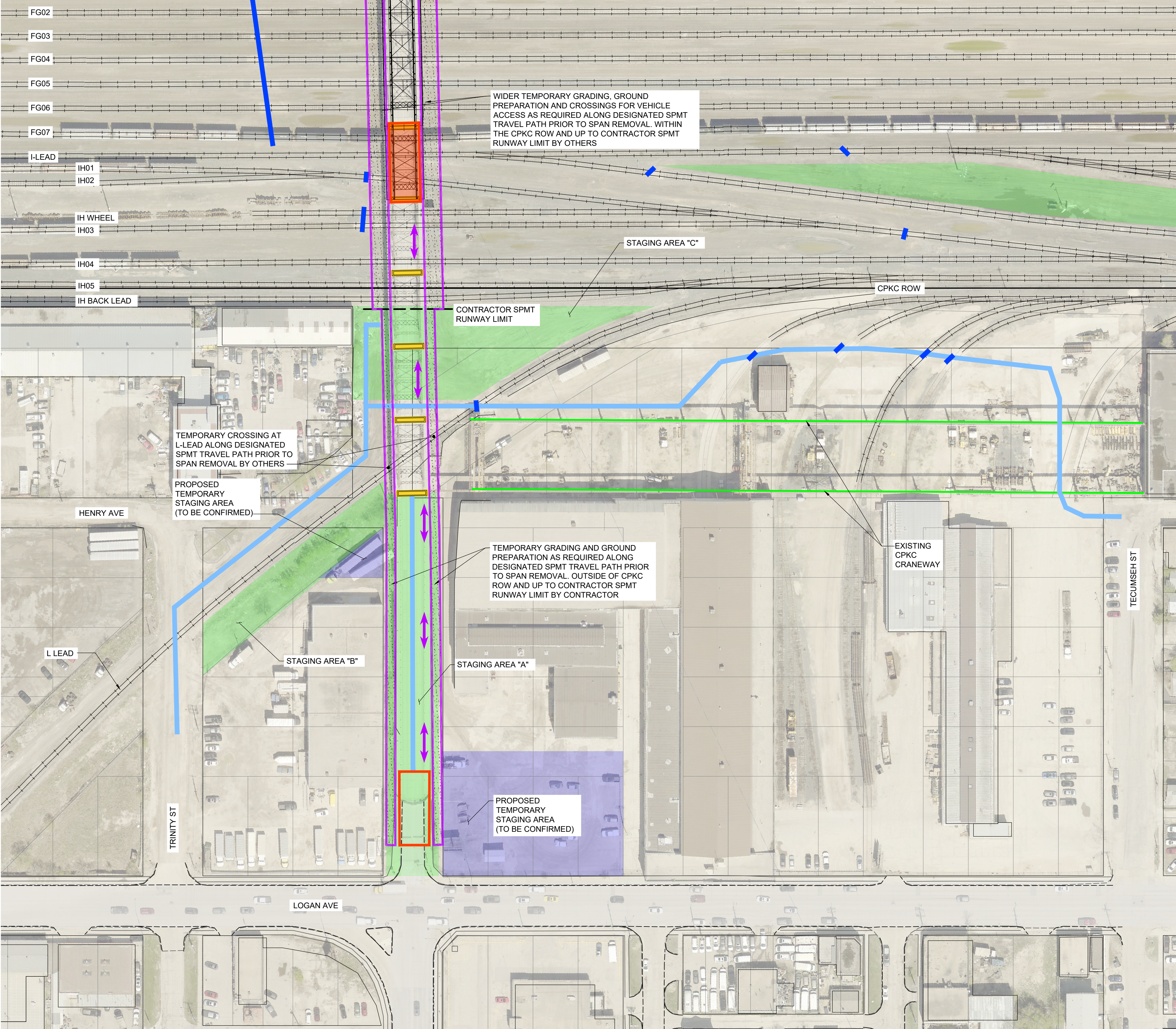
ARLINGTON BRIDGE
PHASE 4 - DECOMMISSIONING

SPAN 17
SPMT AND GANTRY REMOVAL OPTION

CITY DRAWING NUMBER

SHEET 17 OF 42

7



SEQUENCE OF WORK

PRE-WORKBLOCK DEMOLITION WORKS:

- PERFORM LOCALIZED TRUSS MODIFICATIONS/REMOVALS TO FACILITATE TRUSS REMOVAL
- ERECT AND SETUP SELF-PROPELLED MODULAR TRANSPORTS (SPMT) AND GANTRY CRANES
- PRIOR TO SPAN REMOVAL WORKBLOCK, ALL TRAINS SHALL BE REMOVED WITHIN SPMT TRAVEL PATH
- EXTEND TEMPORARY GRADING AND CROSSINGS FROM IH WHEEL TO FG06 TRACK ALONG DESIGNATED SPMT TRAVEL PATH (BY OTHERS)

TRUSS REMOVAL WORKS:

- ENSURE L-LEAD, IH BACK LEAD, IH05, IH04, IH03 IH WHEEL, IH01, I-LEAD AND FG07 TRACKS ARE REMOVED FROM SERVICE
- POSITION SPMT TO SPAN 18
- LIFT AND MOVE SPAN FROM BRIDGE LOCATION TO STAGING AREA "A"
- LOWER SPAN ONTO GROUND SURFACE
- RETURN SERVICE TO THE IH BACK LEAD, IH05, IH04, IH03, IH WHEEL, IH01, I-LEAD AND FG07 TRACK ONCE SPMT CLEARS IH BACK LEAD RIGHT OF WAY
- RETURN SERVICE TO THE L-LEAD TRACK ONCE SPMT CLEARS L LEAD RIGHT OF WAY

DURING WORKBLOCK DEMOLITION WORKS:

- DISMANTLE/DEMOLISH TRUSS COMPONENTS ON GROUND, WHILE SIMULTANEOUSLY HAULING THEM OFF SITE
- HAULING EQUIPMENT TO REMOVE TRUSS COMPONENTS FROM STAGING AREAS "A" VIA LOGAN AVE
- TRACK BLOCKS AND SERVICE REINSTATEMENTS CAN BE STAGED TO REDUCE OVERALL OUT OF SERVICE IMPACTS
- ONCE SPMT GETS TO SPAN 18, IH BACK LEAD, IH04 AND IH05 CAN RETURN TO SERVICE (UPON CPKC DIRECTION) UNTIL SPMT IS READY TO MOVE
- ONCE SPMT STARTS MOVING TRACKS THAT HAVE BEEN CLEARED CAN BE PUT BACK IN SERVICE IN A PHASED APPROACH

POST-WORKBLOCK DEMOLITION WORKS:

- REMOVE TEMPORARY GRADING AND CROSSINGS FOR SPMT RUNWAY. CLEAN AND RESTORE GROUND SURFACE TO PRE-DEMOLITION STATE AS REQUIRED

LEGEND:

- EXISTING PIER LOCATIONS
- ACCESS ROUTE FOR SPAN 18
- EXISTING AT GRADE CROSSING
- CONSTRUCTION STAGING AREA
- OVERALL WORK AREA
- SPMT TRAVEL PATH

1 SPAN 18 ACCESS PLAN
1 : 750

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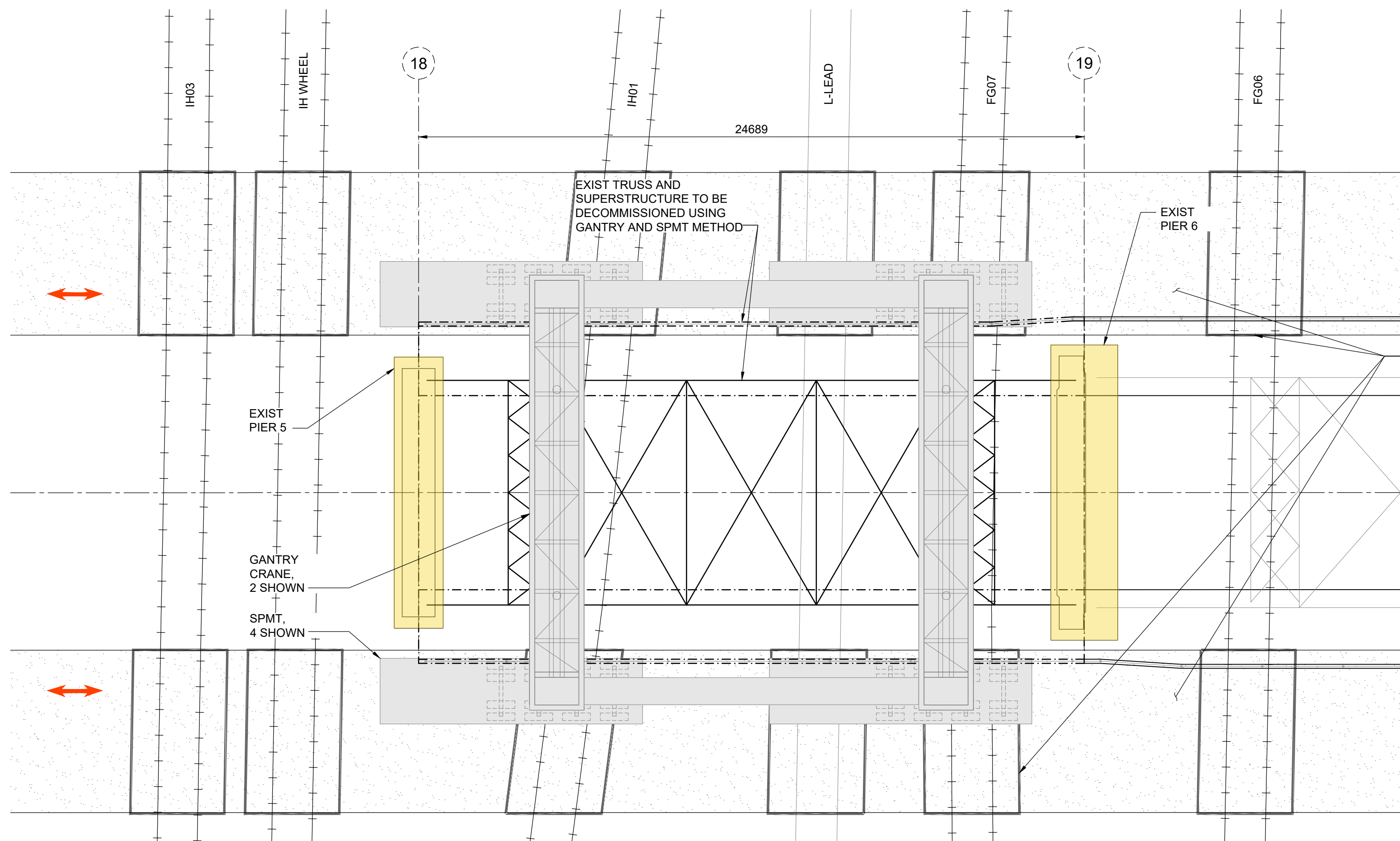
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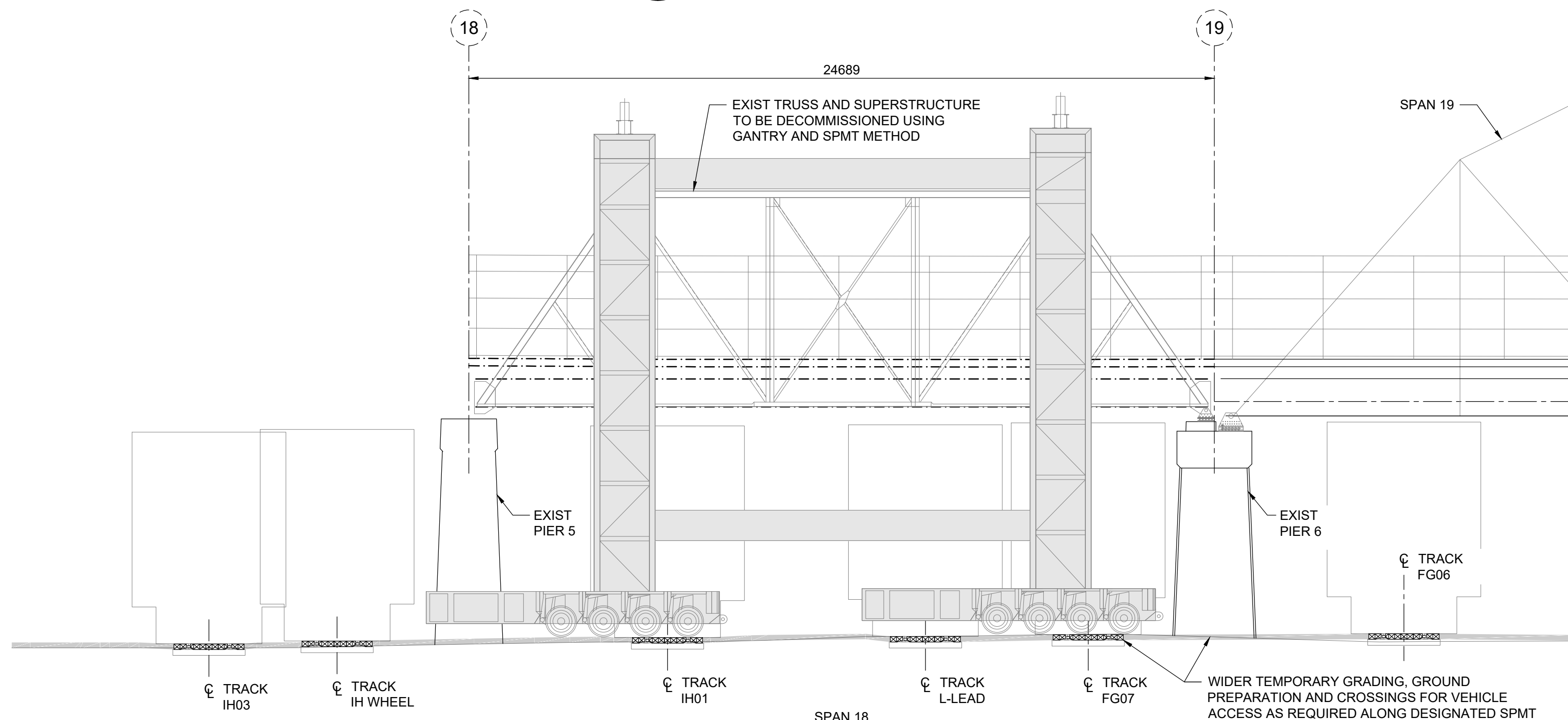
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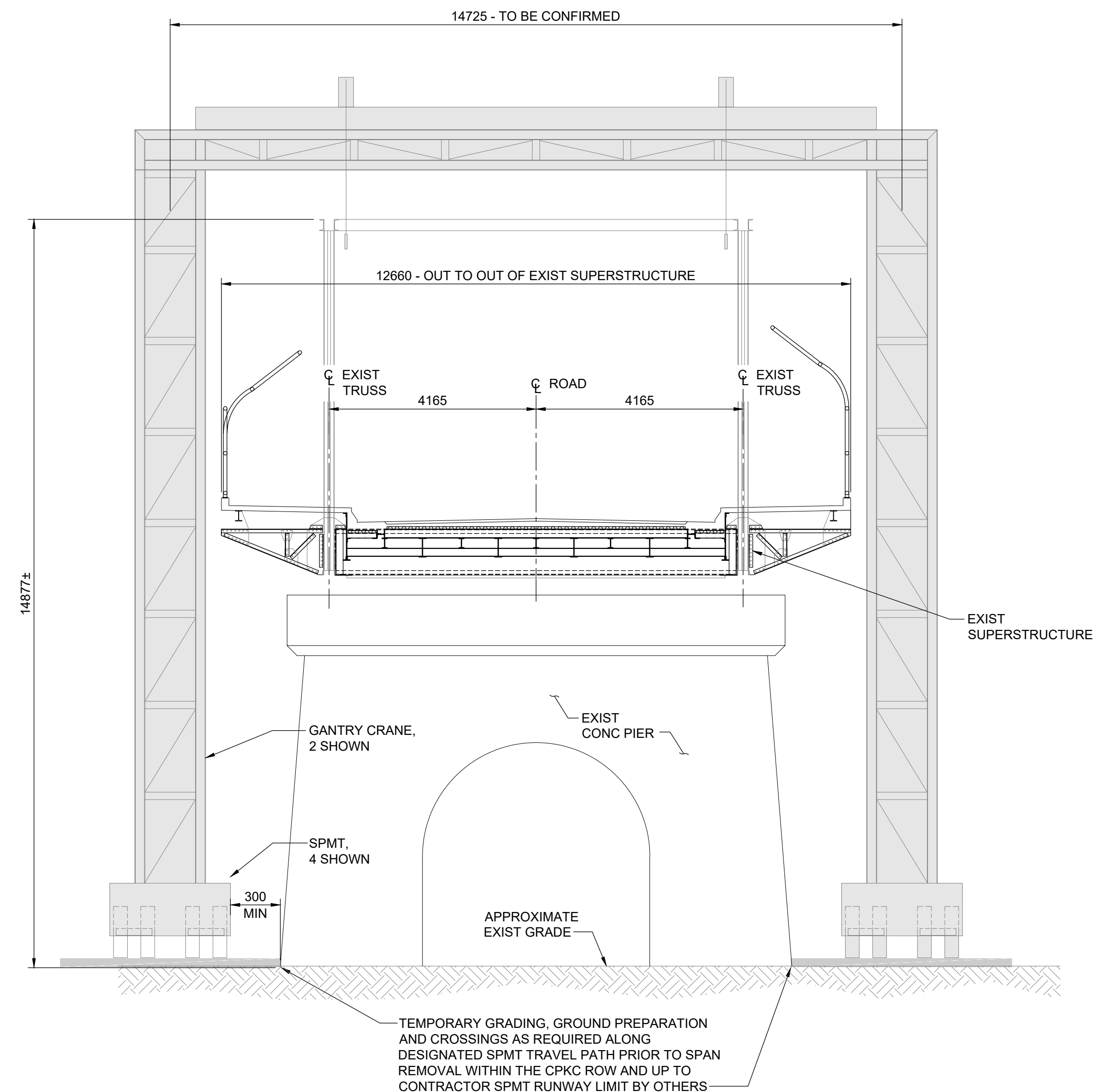
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ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	CITY DRAWING NUMBER	
SPAN 18 ACCESS PLAN SPMT AND GANTRY REMOVAL OPTION	SHEET 18 OF 42	
	18	



1
- -
SPAN 18 - TRUSS PLAN
1 : 125



2
- -
SPAN 18 - EAST ELEVATION
1 : 125



3
- -
SPAN 18 - CROSS SECTION
1 : 75

PRATT TRUSS ESTIMATED
MASS: 200,000 kg

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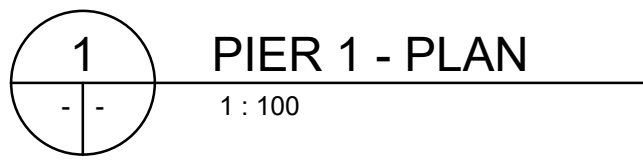
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ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	CITY DRAWING NUMBER
SPAN 18 SPMT AND GANTRY REMOVAL OPTION	SHEET 19 OF 42
	19





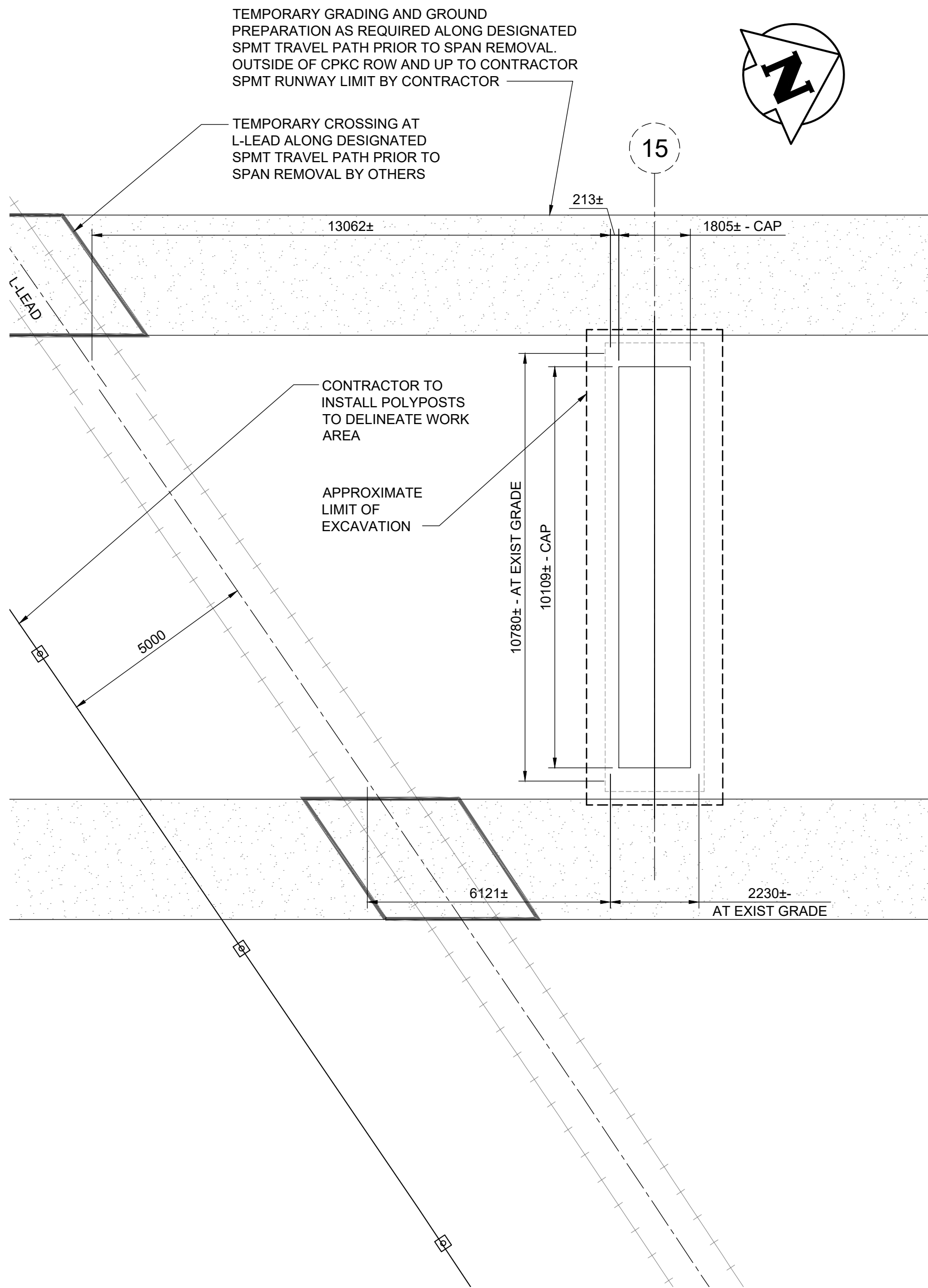
- NOTES:
1. ENSURE L-LEAD TRACK IS REMOVED FROM SERVICE PRIOR TO DEMOLITION.
 2. ACCESS TO PIER 1 FROM TRINITY STREET.
 3. CONCRETE RUBBLE FROM PIER DEMOLITION TO BE REMOVED FROM SITE VIA LOGAN AVE
 4. RETURN TO SERVICE L-LEAD TRACK ONCE SITE HAS BE CLEARED OF DEBRIS.

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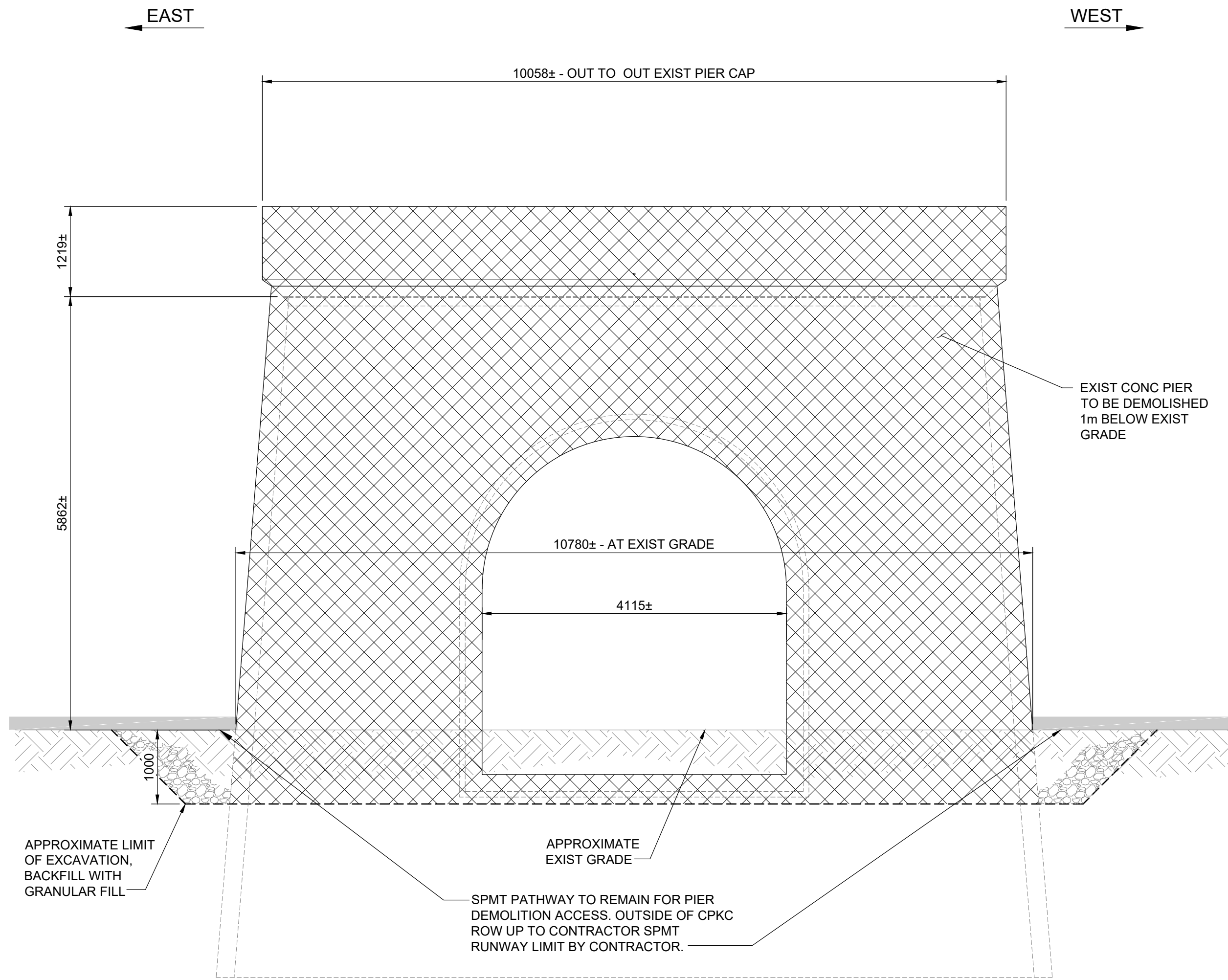


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								SHEET 20 OF 42	
						PIER 1 DEMOLITION		20	

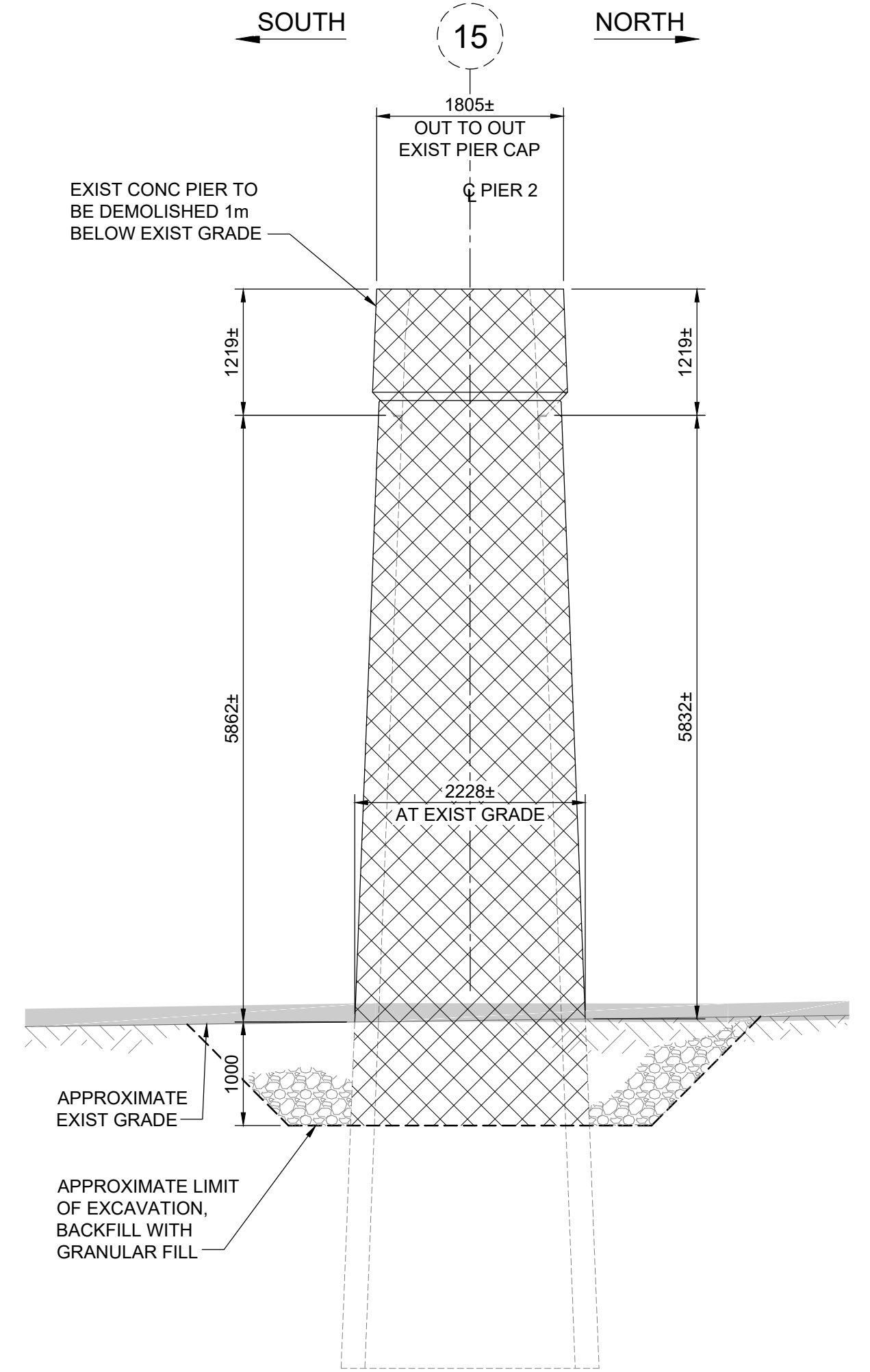


1
- -
PIER 2 - PLAN
1 : 100

- NOTES:
1. ENSURE L-LEAD TRACK IS REMOVED FROM SERVICE PRIOR TO DEMOLITION.
 2. ACCESS TO PIER 2 FROM TRINITY STREET.
 3. CONCRETE RUBBLE FROM PIER DEMOLITION TO BE REMOVED FROM SITE VIA SPMT PATHWAY AND LOGAN AVE.
 4. RETURN TO SERVICE L-LEAD TRACK ONCE SITE HAS BE CLEARED OF DEBRIS.



2
- -
PIER 2 - NORTH ELEVATION
1 : 50



3
- -
PIER 2 - END EAST ELEVATION
1 : 50

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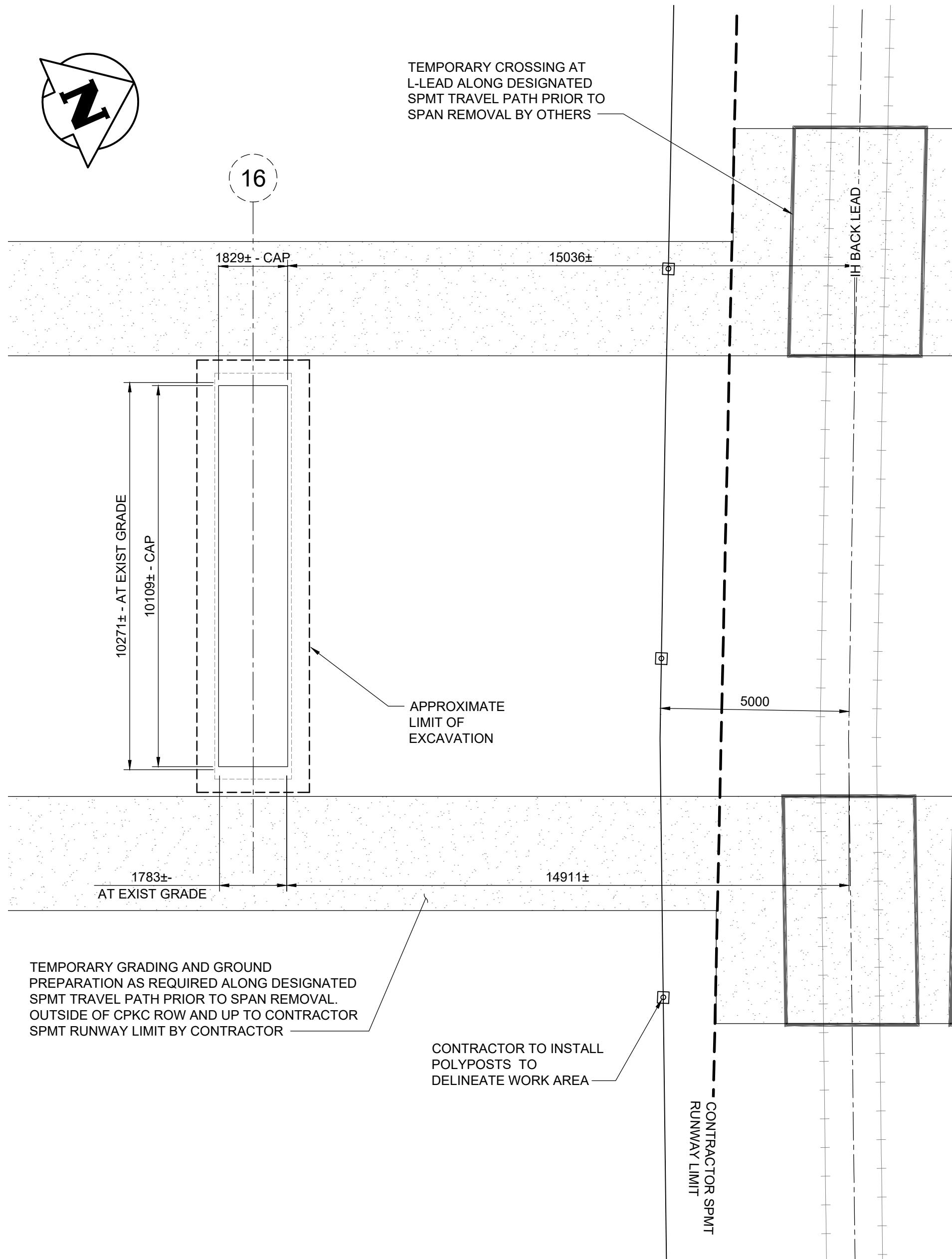
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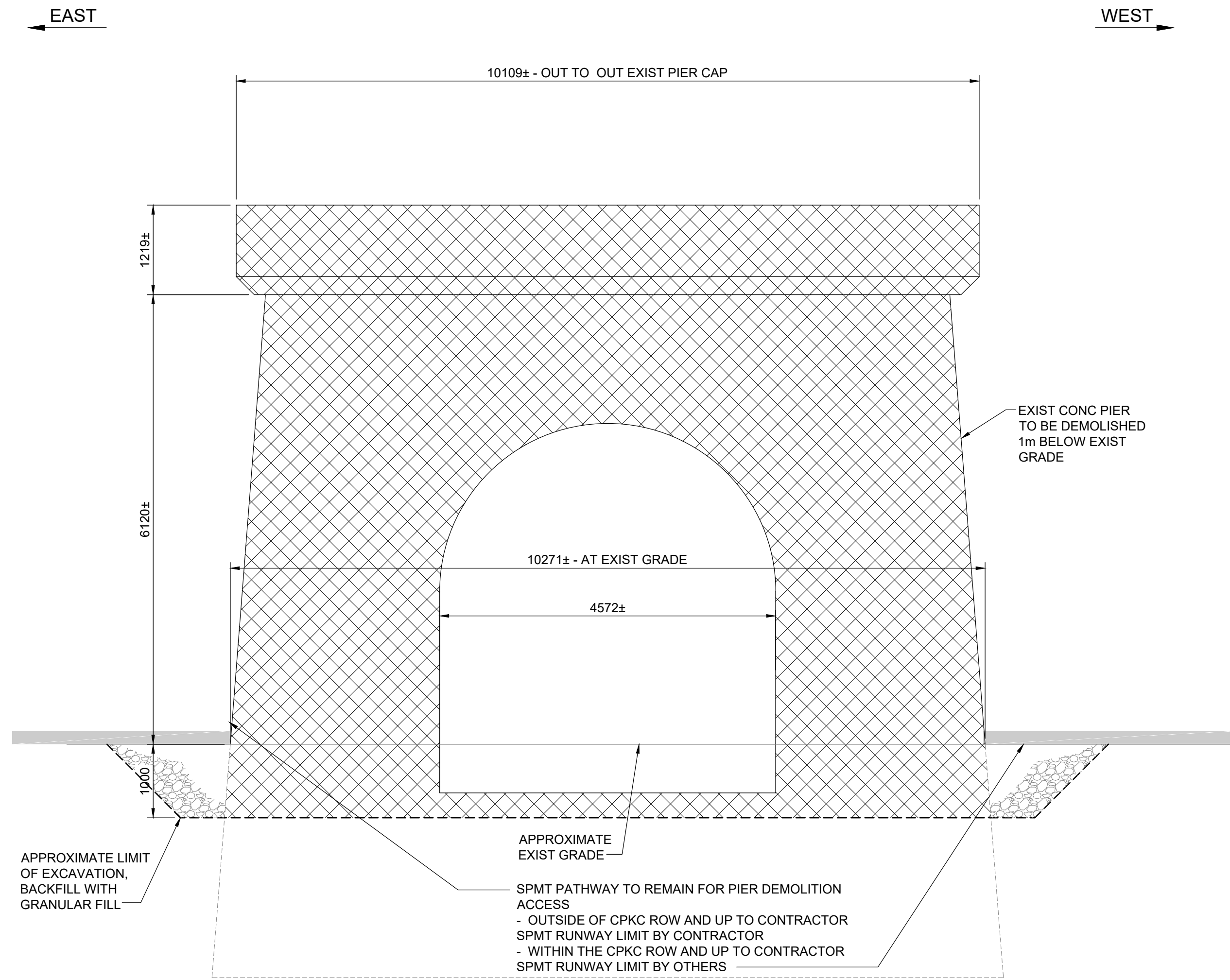
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ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	CITY DRAWING NUMBER
PIER 2 DEMOLITION	SHEET 21 OF 42
	21

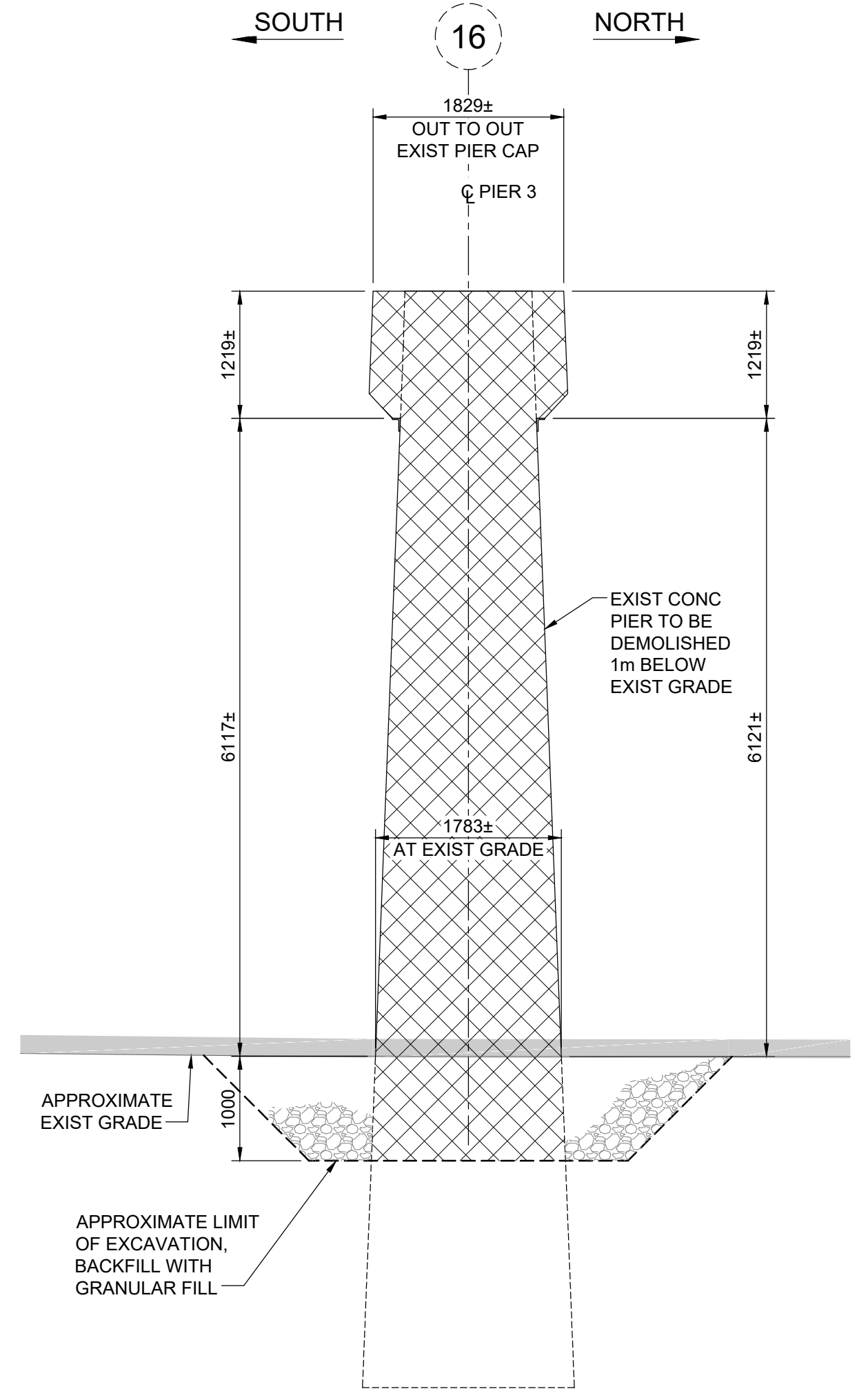


1
PIER 3 - PLAN
1 : 100

- NOTES:
1. ENSURE L-LEAD TRACK IS REMOVED FROM SERVICE PRIOR TO DEMOLITION.
 2. ACCESS TO PIER 3 FROM TRINITY STREET.
 3. CONCRETE RUBBLE FROM PIER DEMOLITION TO BE REMOVED FROM SITE SPMT PATHWAY AND VIA LOGAN AVE.
 4. RETURN TO SERVICE L-LEAD TRACK ONCE SITE HAS BE CLEARED OF DEBRIS.



2
PIER 3 - NORTH ELEVATION
1 : 50



3
PIER 3 - END EAST ELEVATION
1 : 50

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THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT	CITY DRAWING NUMBER
ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	SHEET 22 OF 42
PIER 3 DEMOLITION	22





- NOTES:
1. ENSURE L-LEAD, IH BACK LEAD, IH05, IH04 TRACKS ARE REMOVED FROM SERVICE PRIOR TO DEMOLITION.
 2. ACCESS TO PIER 4 FROM TRINITY STREET AND SPMT PATHWAY.
 3. CONCRETE RUBBLE FROM PIER DEMOLITION TO BE REMOVED FROM SITE VIA SPMT PATHWAY AND LOGAN AVE.
 4. RETURN TO SERVICE L-LEAD, IH BACK LEAD, IH05, IH04 TRACKS ONCE SITE HAS BE CLEARED OF DEBRIS.

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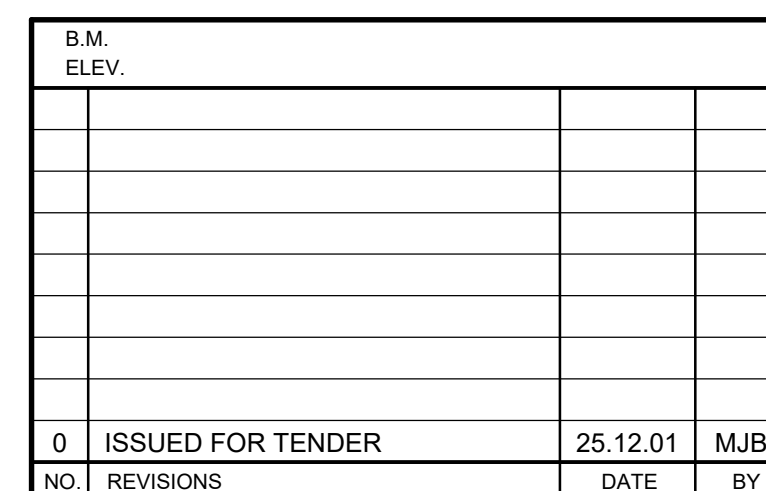
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							ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING		CITY DRAWING NUMBER
									SHEET 23 OF 42
							PIER 4 DEMOLITION		23



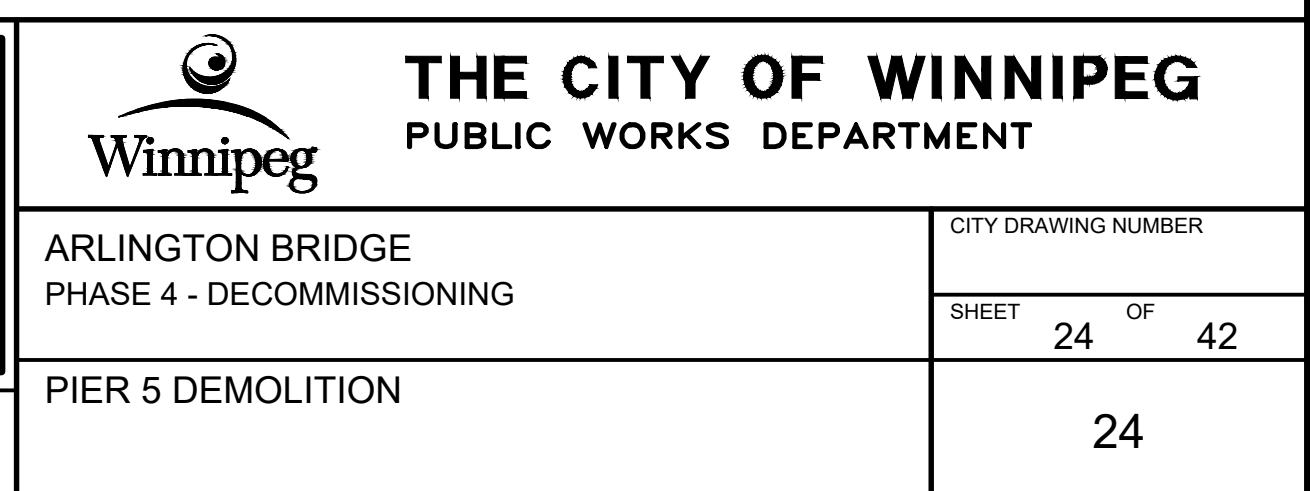
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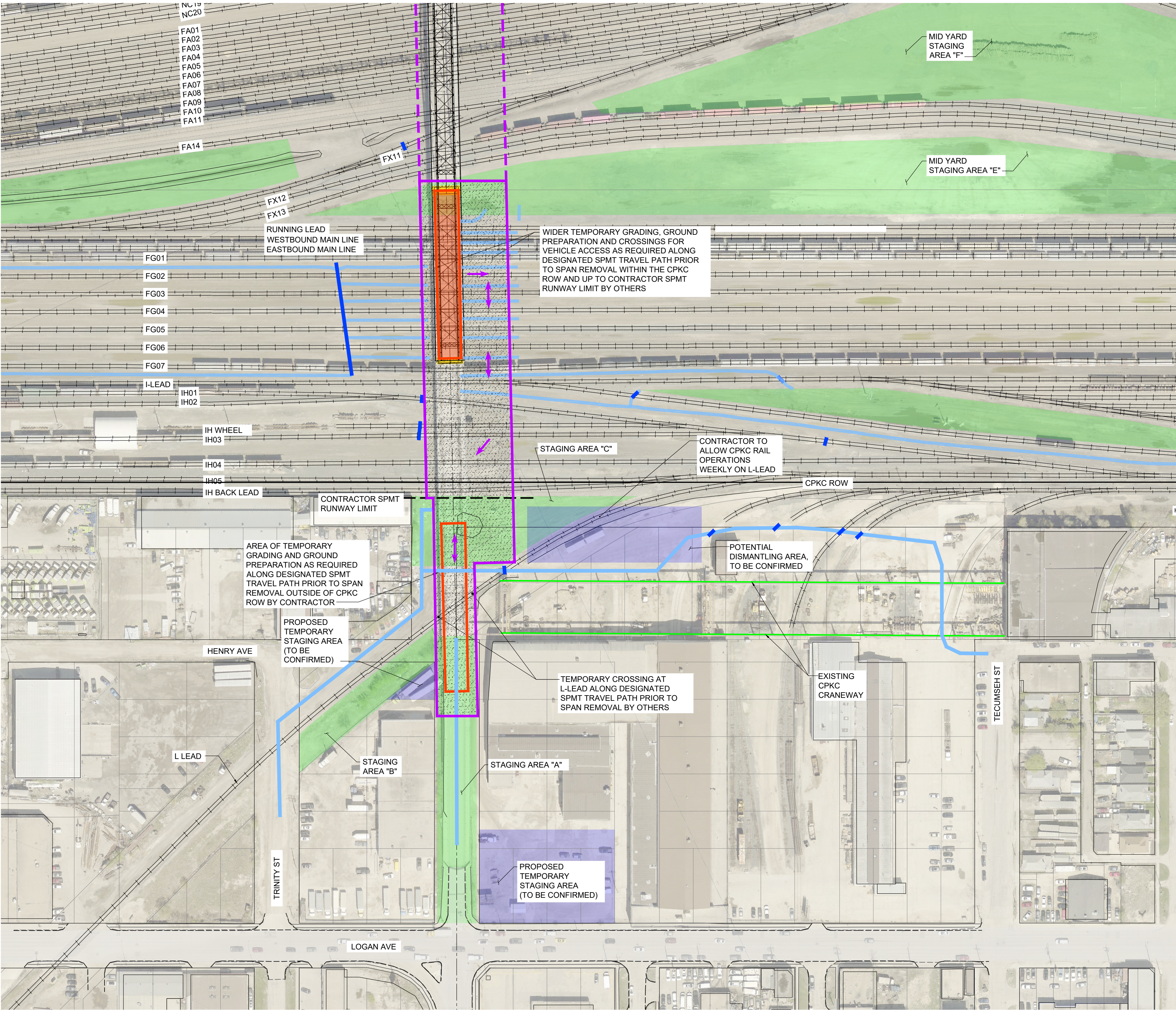


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SEQUENCE OF WORK

PRE-WORKBLOCK DEMOLITION WORKS:

- PERFORM LOCALIZED TRUSS MODIFICATIONS/REMOVALS TO FACILITATE TRUSS REMOVAL
- ERECT AND SETUP SELF-PROPELLED MODULAR TRANSPORTS (SPMT)
- PRIOR TO SPAN REMOVAL WORKBLOCK, ALL TRAINS SHALL BE REMOVED WITHIN SPMT TRAVEL PATH
- EXTEND TEMPORARY GRADING, GROUND PREPARATION AND CROSSINGS FROM ALONG DESIGNATED SPMT TRAVEL PATH

TRUSS REMOVAL WORKS:

- ENSURE RUNNING LEAD, WESTBOUND MAINLINE, EASTBOUND MAINLINE, FG01, FG02, FG03, FG04, FG05, FG06 AND FG07 TRACKS ARE REMOVED FROM SERVICE
- FG07, I-LEAD, IH01, IH WHEEL, IH03, IH04, IH05, IH BACKLEAD AND L-LEAD TRACKS TO BE UNDER WORKBLOCKS AS REQUIRED DURING SPMT MOVEMENT TO FINAL SPOT INFRONT STAGING AREA "C"
- EXISTING PIER 1, 2, 3, 4 AND 5 TO BE DEMOLISHED PRIOR TO SPAN 19 REMOVAL
- POSITION SPMT TO SPAN 19
- LIFT AND MOVE SPAN FROM BRIDGE LOCATION TO INFRONT OF STAGING AREA "C"
- LOWER SPAN ONTO GROUND SURFACE
- RETURN SERVICE TO THE RUNNING LEAD, WESTBOUND MAINLINE, EASTBOUND MAINLINE, FG01, FG02, FG03, FG04, FG05, FG06, FG07 AND I-LEAD TRACKS ONCE SPMT CLEARS I-LEAD LEAD RIGHT OF WAY
- RETURN SERVICE TO THE IH01, IH03, IH WHEEL, IH04, IH05, IH BACKLEAD AND L-LEAD TRACKS ONCE SPAN 19 DEMOLITION HAS CLEARS THAT TRACKS RIGHT OF WAYS

DURING WORKBLOCK DEMOLITION WORKS:

- DISMANTLE/DEMOLISH TRUSS COMPONENTS ON GROUND, WHILE SIMULTANEOUSLY HAULING THEM OFF SITE
- HAULING EQUIPMENT TO REMOVE TRUSS COMPONENTS FROM STAGING AREAS "C" VIA LOGAN AVE
- TRACK BLOCKS AND SERVICE REINSTATEMENTS CAN BE STAGED TO REDUCE OVERALL OUT OR SERVICE IMPACTS
- ONCE SPMT GETS TO SPAN 19, IH BACK LEAD TO IH01 CAN BE RETURNED TO SERVICE (UPON CPKC DIRECTION) UNTIL SPMT IS READY TO MOVE
- ONCE SPMT STARTS MOVING, TRACKS THAT HAVE BEEN CLEARED CAN BE PUT BACK IN SERVICE IN A PHASED APPROACH

POST-WORKBLOCK DEMOLITION WORKS:

- REMOVE TEMPORARY GRADING AND CROSSINGS FOR SPMT RUNWAY. CLEAN AND RESTORE GROUND SURFACE TO PRE-DEMOLITION STATE AS REQUIRED

LEGEND:

- EXISTING PIER LOCATIONS
- ACCESS ROUTE FOR SPAN 19
- EXISTING AT GRADE CROSSING
- CONSTRUCTION STAGING AREA
- OVERALL WORK AREA
- SPMT TRAVEL PATH AREA
- SPMT TRAVEL PATH AREA NEXT SPAN

1 SPAN 19 ACCESS PLAN
1 : 1000

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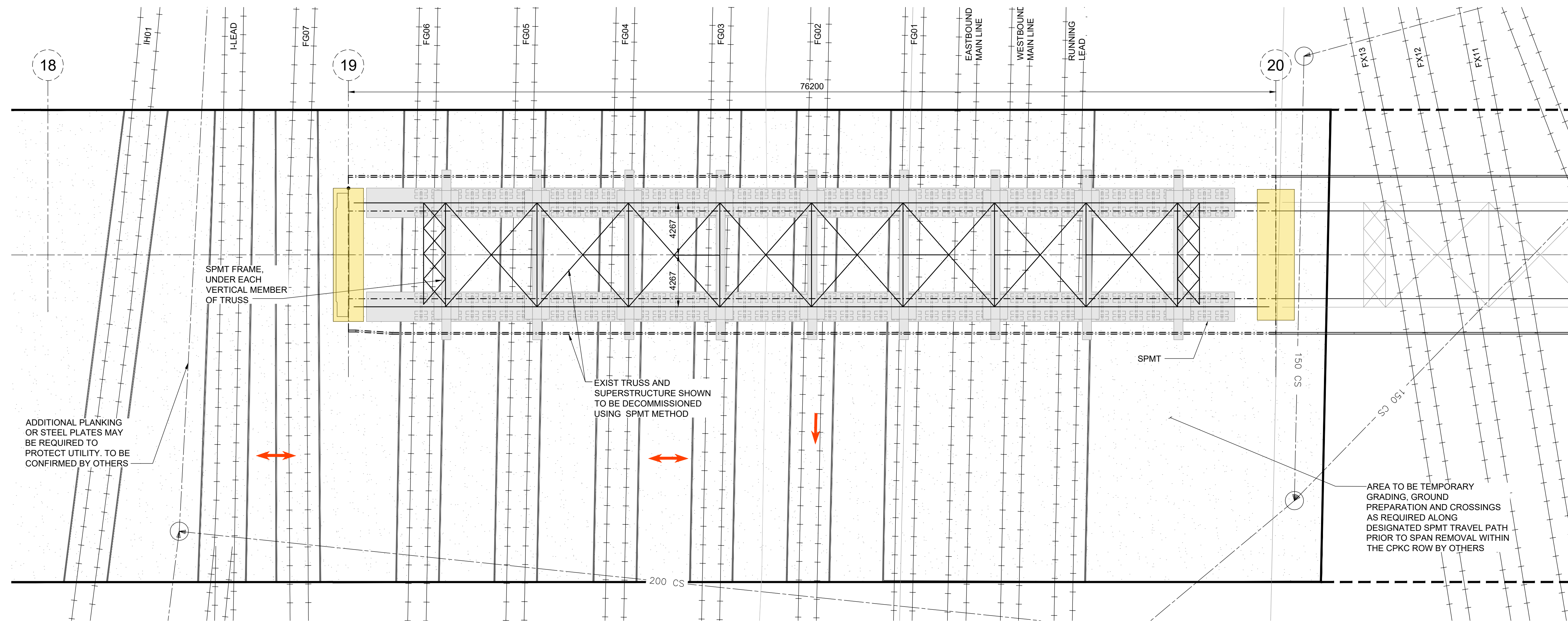
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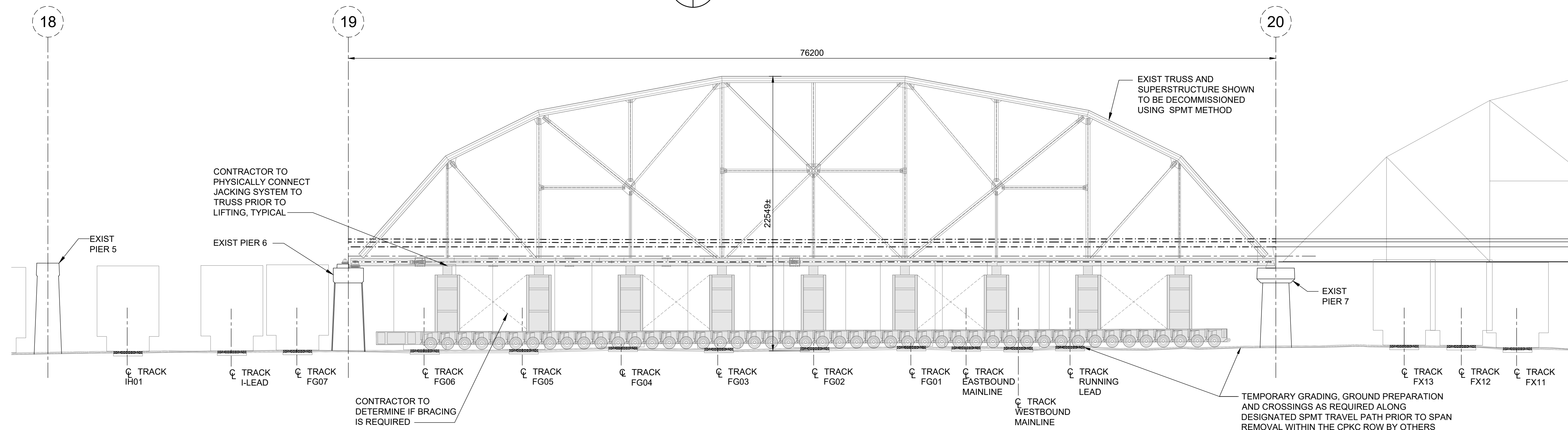
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ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	CITY DRAWING NUMBER
SPAN 19 ACCESS PLAN SPMT REMOVAL OPTION	SHEET 25 OF 42
	25



1 SPAN 19 - TRUSS PLAN
1 : 200



2 SPAN 19 - EAST ELEVATION
1 : 200

LEGEND:

EXISTING PIER LOCATIONS

SPMT TRAVEL PATH AREA

SPMT TRAVEL PATH AREA NEXT SPAN

NOTE:
THE STRUCTURAL CAPACITY OF SPAN 19 HAS NOT BEEN DEEMED ADEQUATE FOR REMOVAL VIA SPMT METHOD AS SHOWN

SPAN 19 TRUSS ESTIMATED MASS: 800,000 kg

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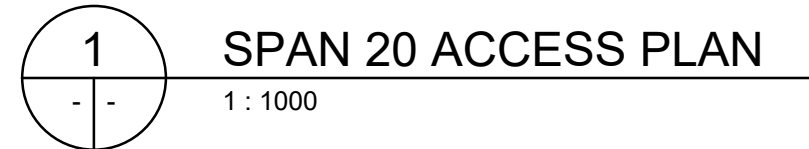
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ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	CITY DRAWING NUMBER
SPAN 19 SPMT REMOVAL OPTION	SHEET 26 OF 42
	26



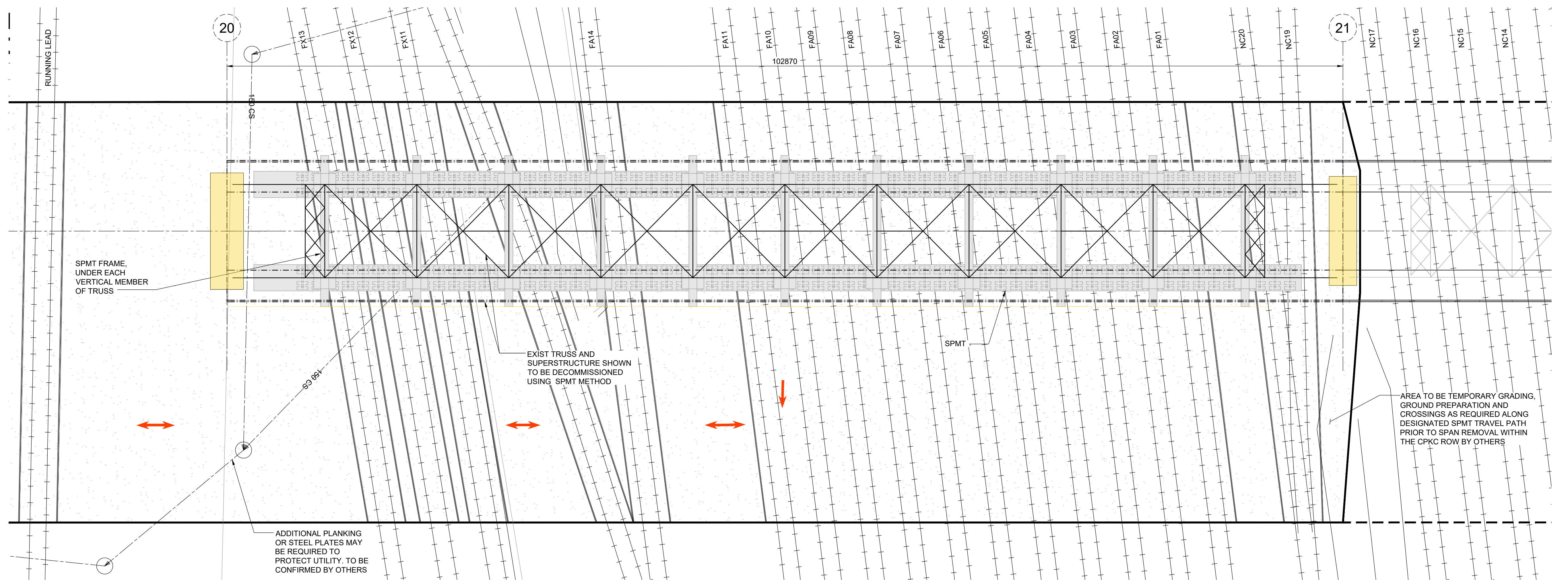
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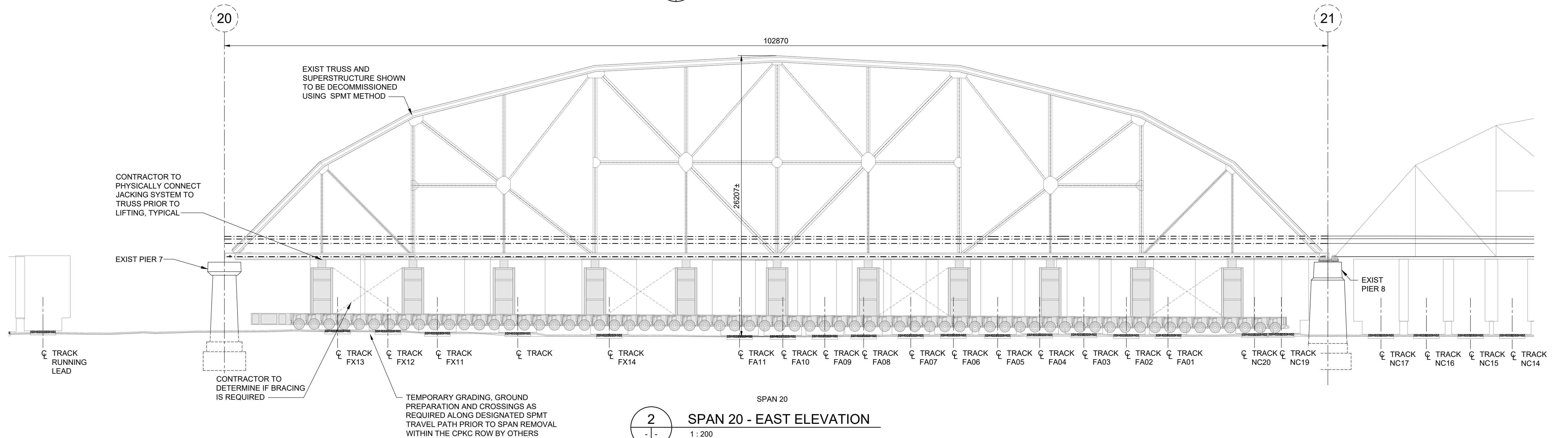
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27



1
- -
SPAN 20 - TRUSS PLAN
1 : 200



2
- -
SPAN 20 - EAST ELEVATION
1 : 200

- LEGEND:
- EXISTING PIER LOCATIONS
 - SPMT TRAVEL PATH AREA
 - SPMT TRAVEL PATH AREA NEXT SPAN

NOTE:
THE STRUCTURAL CAPACITY OF SPAN 20 HAS NOT BEEN DEEMED ADEQUATE FOR REMOVAL VIA SPMT METHOD AS SHOWN

SPAN 20 TRUSS ESTIMATED MASS: 1,400,000 kg

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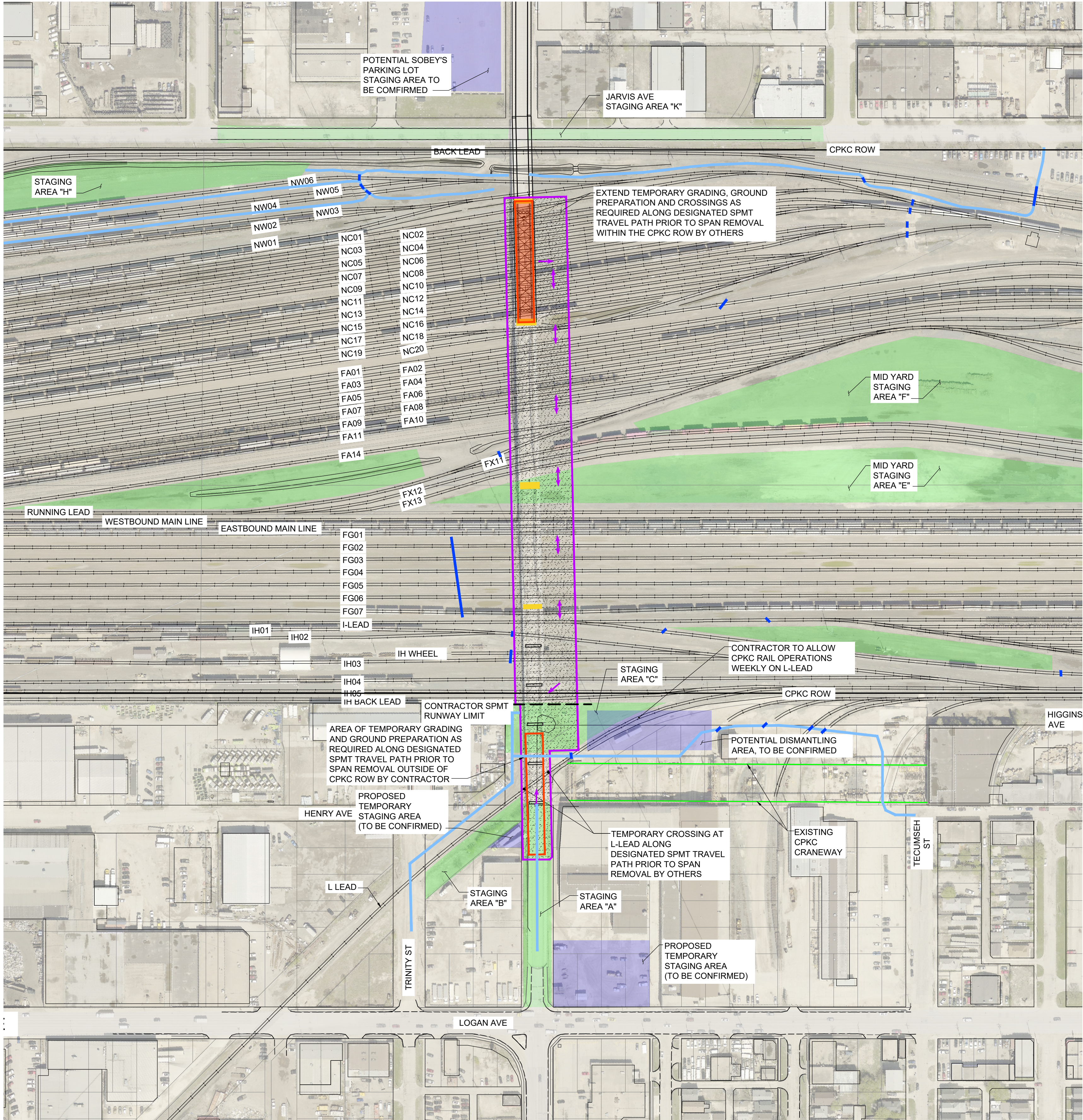
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SPAN 20 SPMT REMOVAL OPTION	SHEET 28 OF 42
	28



SEQUENCE OF WORK

PRE-WORKBLOCK DEMOLITION WORKS:

- PERFORM LOCALIZED TRUSS MODIFICATIONS/REMOVALS TO FACILITATE TRUSS REMOVAL
- ERECT AND SETUP SELF-PROPELLED MODULAR TRANSPORTS (SPMT)
- PRIOR TO SPAN REMOVAL WORKBLOCK, ALL TRAINS SHALL BE REMOVED WITHIN SPMT TRAVEL PATH
- EXTEND TEMPORARY GRADING AND CROSSINGS ALONG DESIGNATED SPMT TRAVEL PATH

TRUSS REMOVAL WORKS:

- ENSURE NC01, NC02, NC03, NC04, NC05, NC06, NC07, NC08, NC09, NC10, NC11, NC12, NC13, NC14, NC15, NC16, NC17, NC19, NC 20 AND NW01 TRACKS ARE REMOVED FROM SERVICE
- FA01, FA02, FA03, FA04, FA05, FA06, FA07, FA08, FA09, FA10, FA11, FA14, FX11, FX12, FX13, RUNNING LEAD, WESTBOUND MAINLINE, EASTBOUND MAINLINE, FG01, FG02, FG03, FG04, FG05, FG06, FG07, I-LEAD, IH01, IH02, IH WHEEL, IH03, IH04, IH05, IH BACKLEAD AND L-LEAD TRACKS TO BE UNDER WORKBLOCKS AS REQUIRED DURING SPMT MOVEMENT TO FINAL SPOT INFRONT STAGING AREA "C"
- POSITION SPMT TO SPAN 21
- LIFT AND MOVE SPAN FROM BRIDGE LOCATION TO INFRONT OF STAGING AREA "C"
- LOWER SPAN ONTO GROUND SURFACE
- RETURN SERVICE TO THE NW01, NC01, NC02, NC03, NC04, NC05, NC06, NC07, NC08, NC09, NC10, NC11, NC12, NC13, NC14, NC15, NC16, NC17, NC19, NC 20, FA01, FA02, FA03, FA04, FA05, FA06, FA07, FA08, FA09, FA10, FA11, FA14, FX11, FX12, FX13, RUNNING LEAD, WESTBOUND MAINLINE, EASTBOUND MAINLINE, FG01, FG02, FG03, FG04, FG05, FG06, FG07, I-LEAD, IH01, IH02, IH WHEEL, IH03, IH04, IH05, AND IH BACKLEAD TRACKS ONCE SPMT CLEARS IH BACKLEAD RIGHT OF WAY

DURING WORKBLOCK DEMOLITION WORKS:

- DISMANTLE/DEMOLISH TRUSS COMPONENTS ON GROUND, WHILE SIMULTANEOUSLY HAULING THEM OFF SITE
- HAULING EQUIPMENT TO REMOVE TRUSS COMPONENTS FROM STAGING AREAS "C" VIA LOGAN AVE
- TRACK BLOCKS AND SERVICE REINSTATEMENTS CAN BE STAGED TO REDUCE OVERALL OUT OR SERVICE IMPACTS
- ONCE SPMT GETS TO SPAN 21, IH BACK LEAD TO NC20 CAN BE RETURNED TO SERVICE (UPON CPKC DIRECTION) UNTIL SPMT IS READY TO MOVE
- ONCE SPMT STARTS MOVING, TRACKS THAT HAVE BEEN CLEARED CAN BE PUT BACK IN SERVICE IN A PHASED APPROACH

POST-WORKBLOCK DEMOLITION WORKS:

- REMOVE TEMPORARY GRADING AND CROSSINGS FOR SPMT RUNWAY. CLEAN AND RESTORE GROUND SURFACE TO PRE-DEMOLITION STATE AS REQUIRED

LEGEND:

- EXISTING PIER LOCATIONS
- EXISTING PIERS TO BE DEMOLISHED
- ACCESS ROUTE FOR SPAN 21
- EXISTING AT GRADE CROSSING
- CONSTRUCTION STAGING AREA
- OVERALL WORK AREA
- SPMT TRAVEL PATH

1 SPAN 21 ACCESS PLAN
1 : 1250

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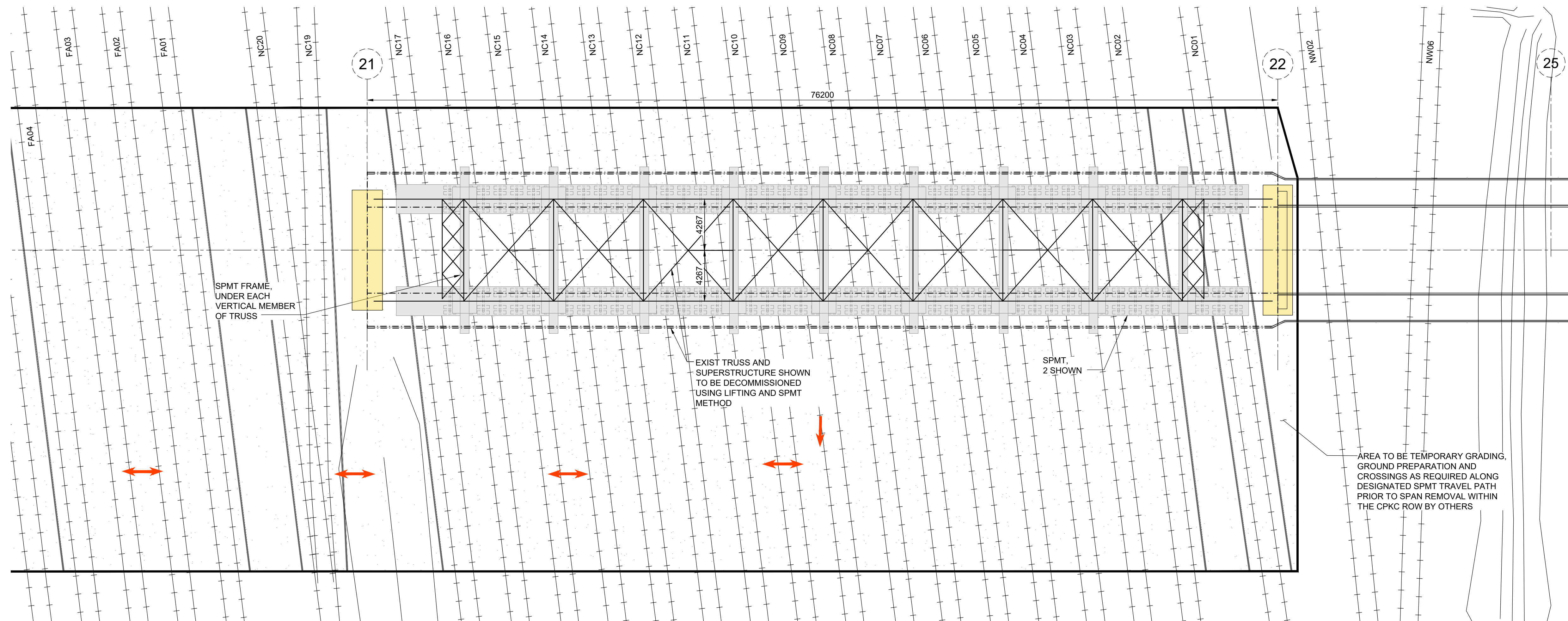
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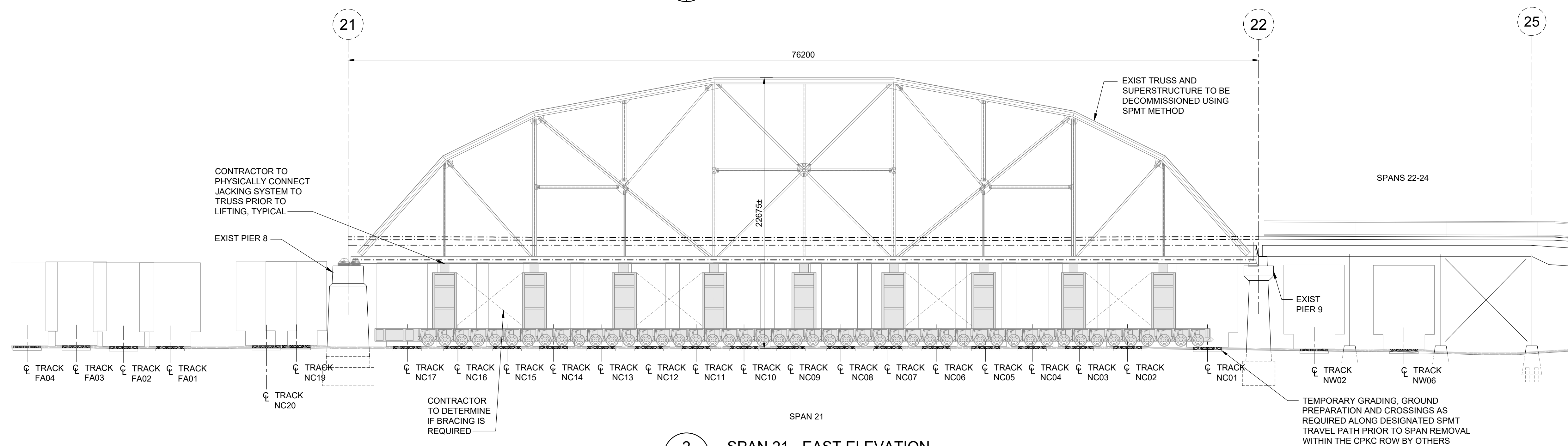
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DRAWN BY	EV	APPROVED BY	MJB
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ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	CITY DRAWING NUMBER	
SPAN 21 ACCESS PLAN SPMT REMOVAL OPTION	SHEET 29 OF 42	
	29	



1
- -
SPAN 21 - TRUSS PLAN
1 : 200



2
- -
SPAN 21 - EAST ELEVATION
1 : 200

LEGEND:

EXISTING PIER LOCATIONS

SPMT TRAVEL PATH AREA

NOTE:
THE STRUCTURAL CAPACITY OF SPAN 21 HAS NOT BEEN DEEMED ADEQUATE FOR REMOVAL VIA SPMT METHOD AS SHOWN

SPAN 21 TRUSS ESTIMATED MASS: 800,000 kg

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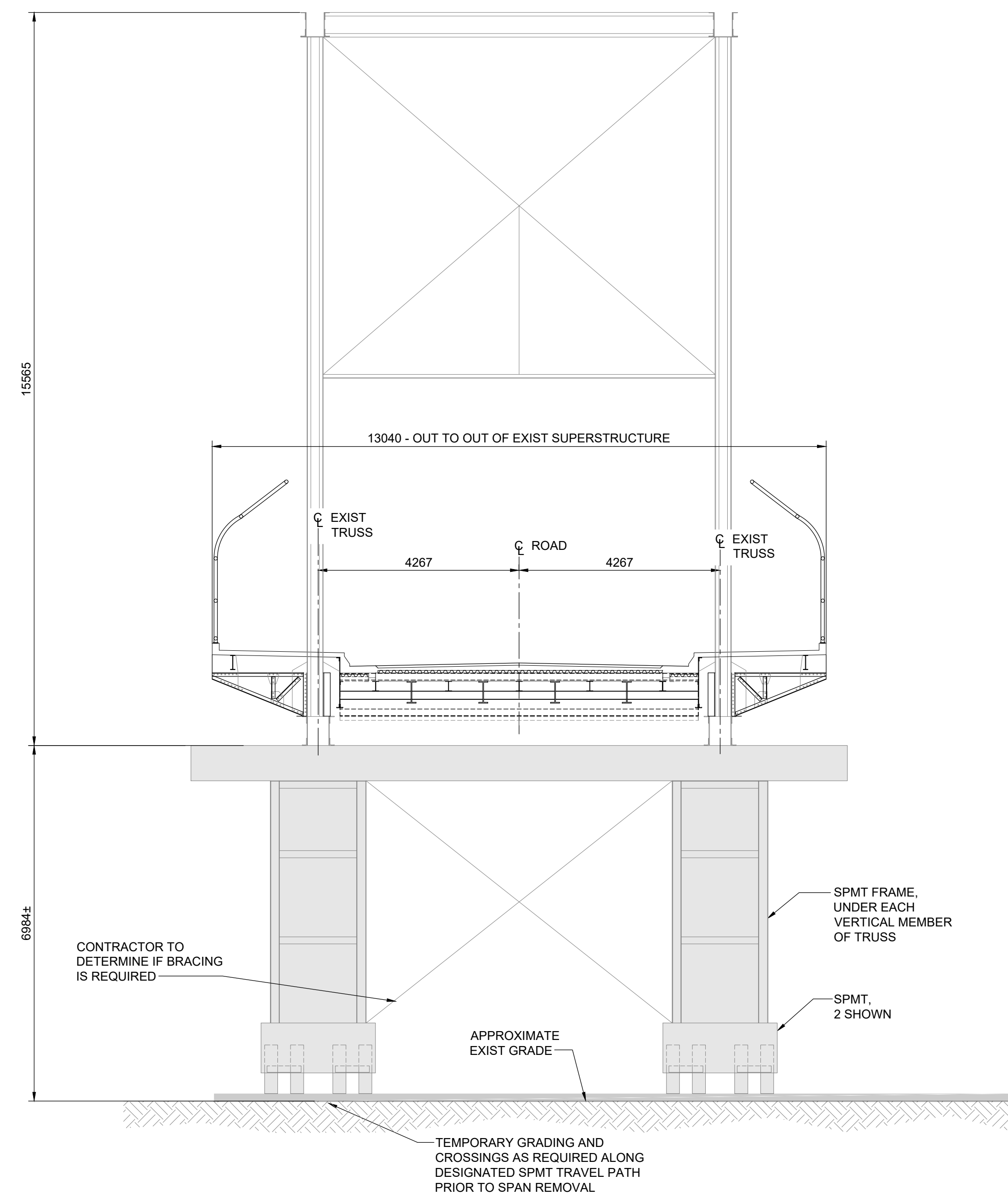
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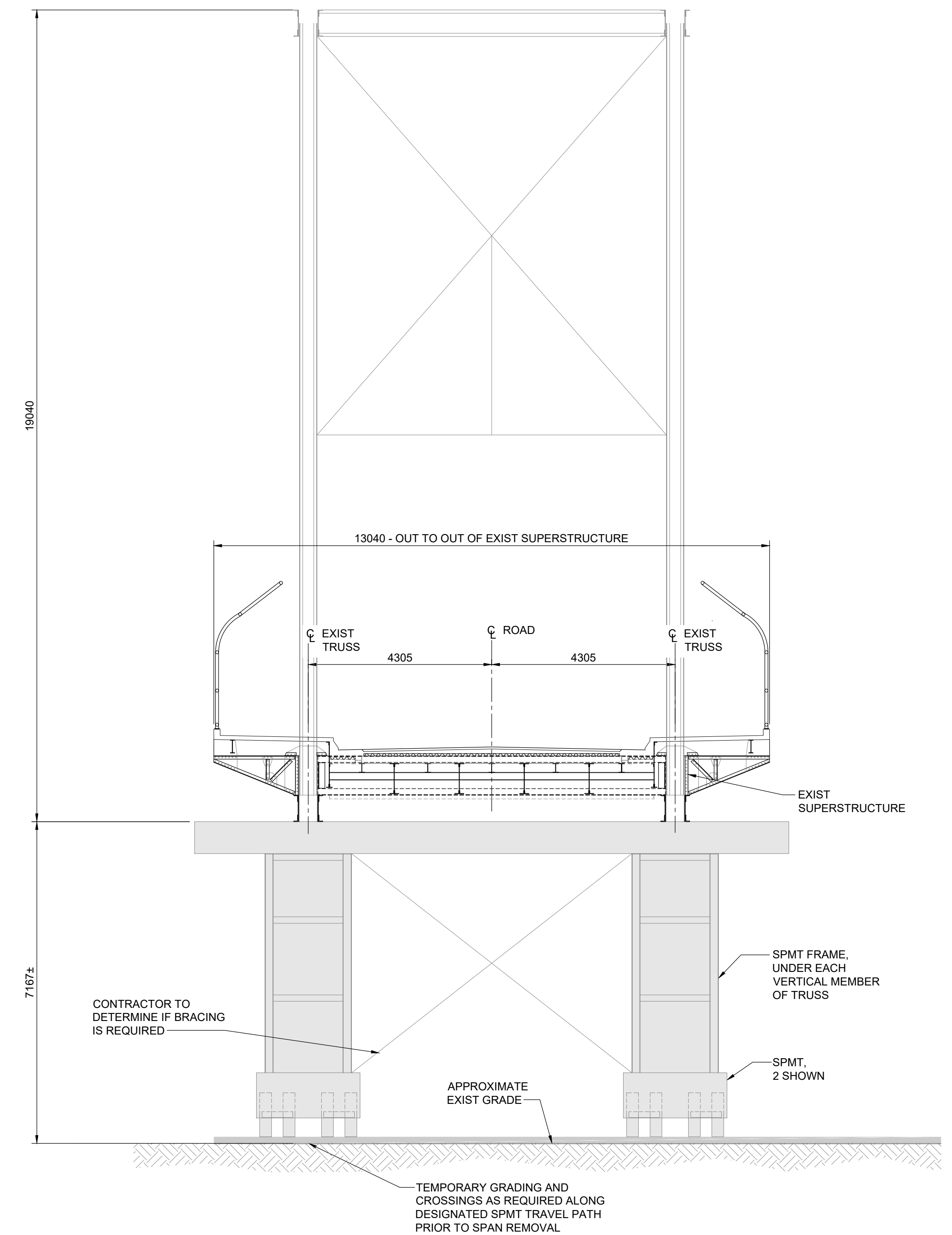
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ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	CITY DRAWING NUMBER
SPAN 21 SPMT REMOVAL OPTION	SHEET 30 OF 42
	30



1 SPAN 19 & 21 - CROSS SECTION
1:75



2 SPAN 20 - CROSS SECTION

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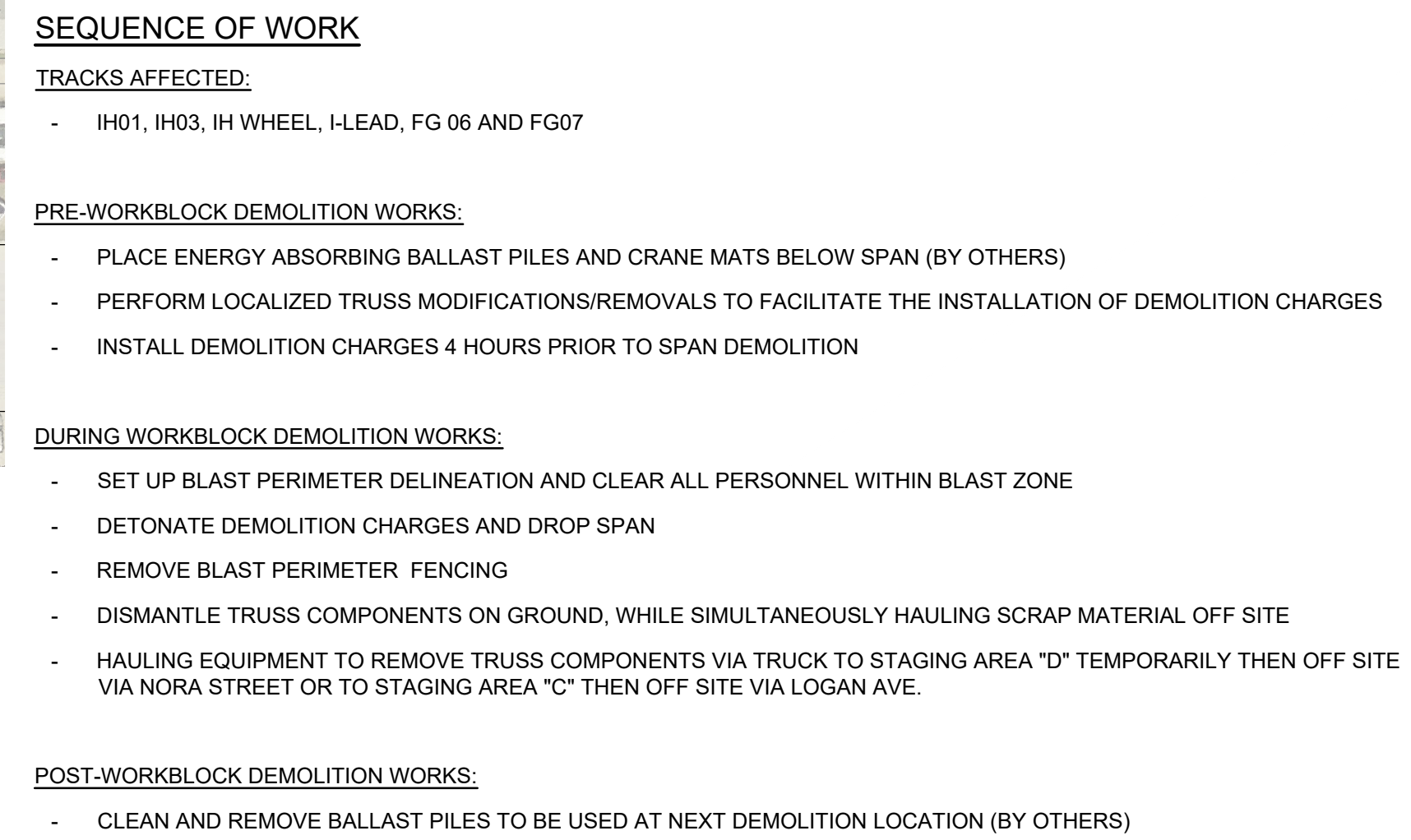
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ARLINGTON BRIDGE
PHASE 4 - DECOMMISSIONING

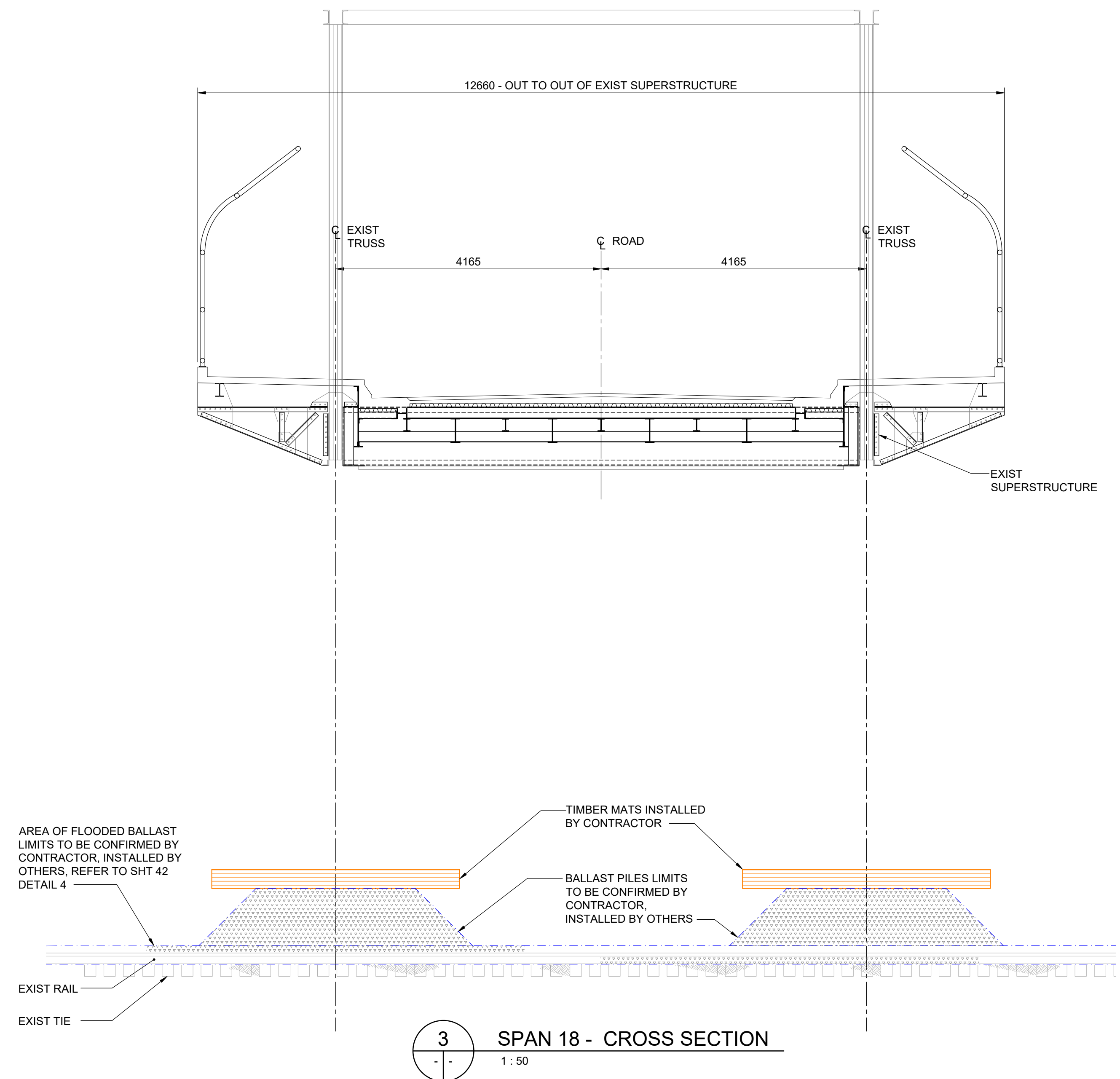
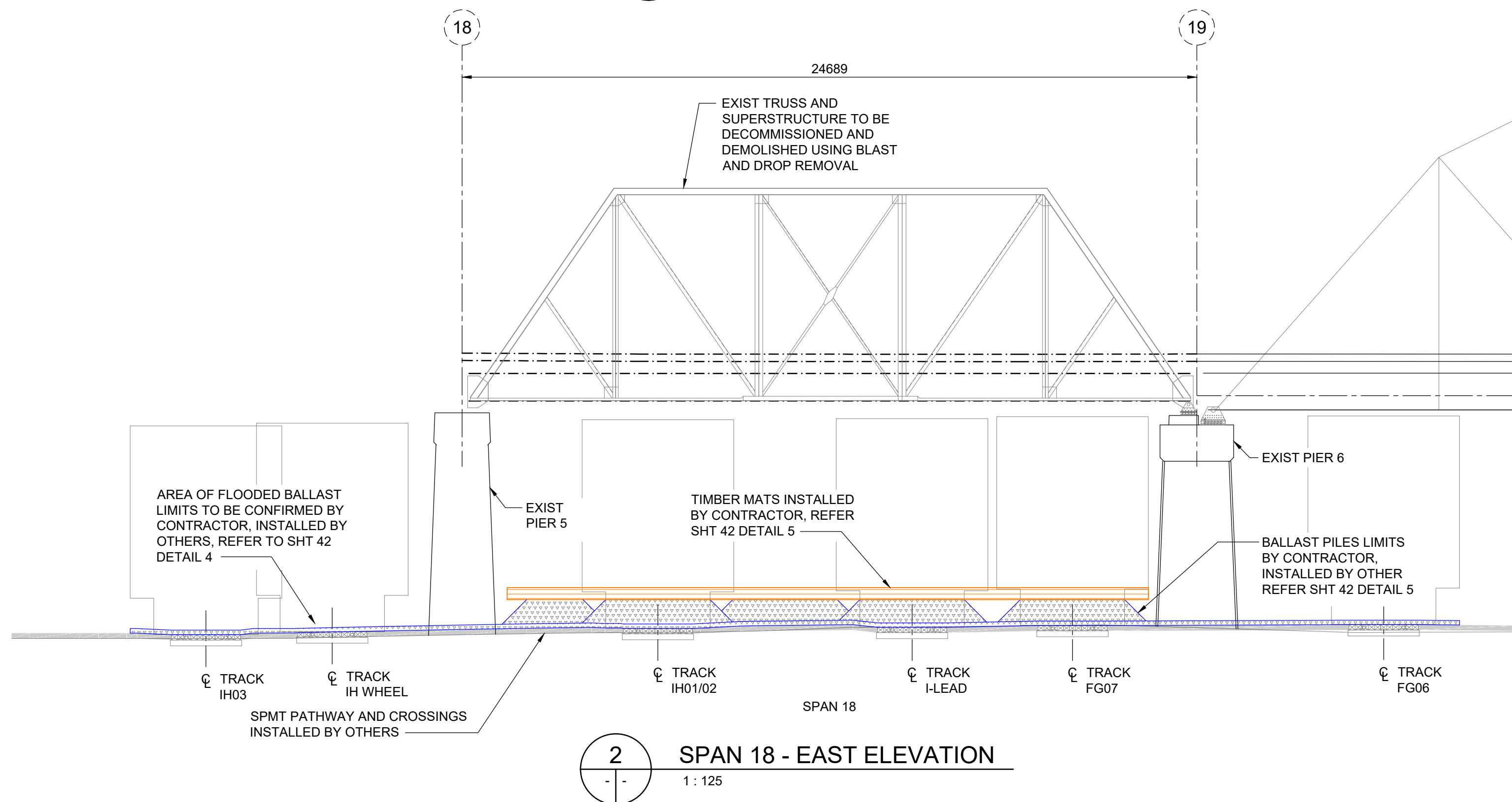
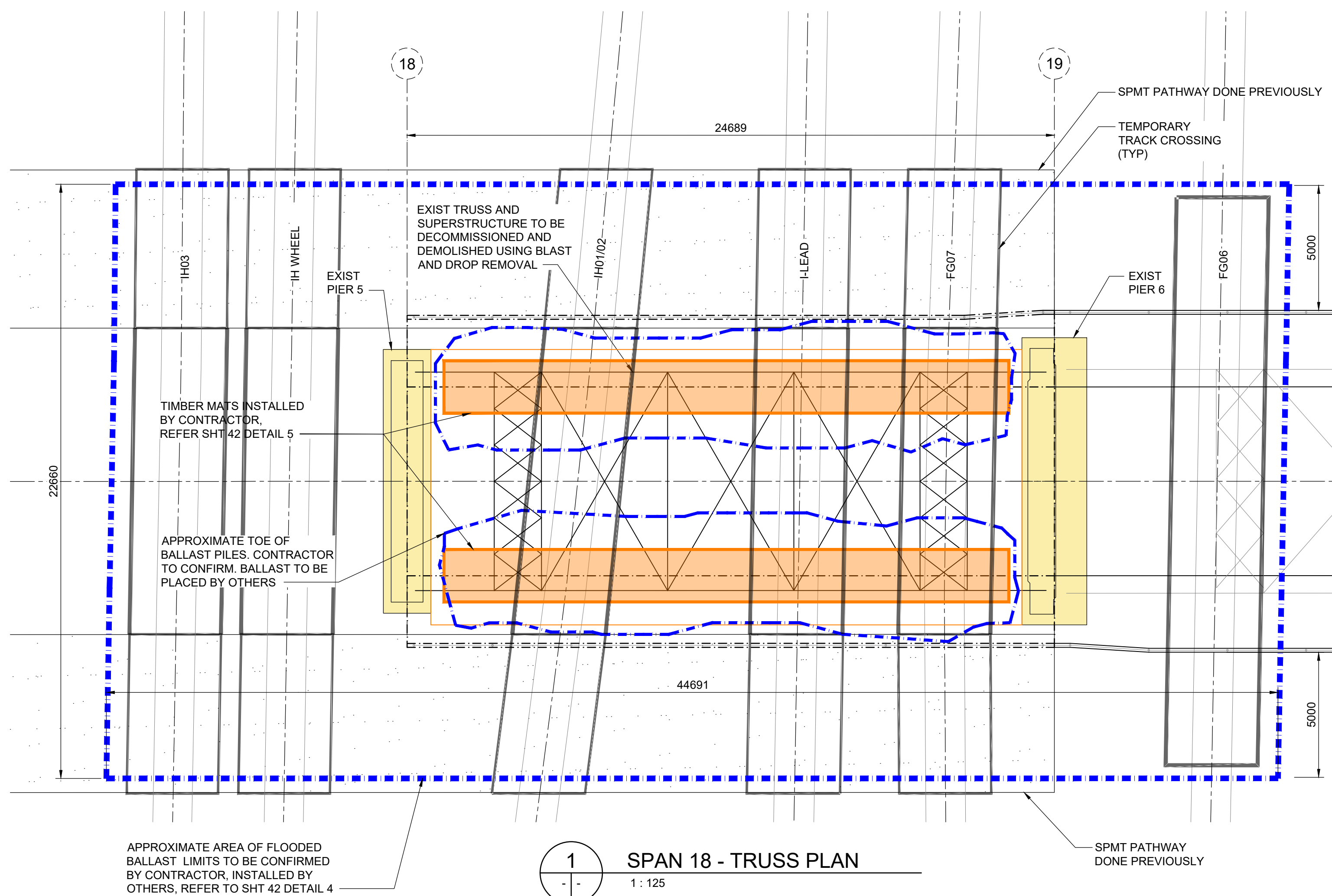
SPAN 19, 20 AND 21 CROSS SECTIONS
SPMT REMOVAL OPTION

SHEET 31 OF 42

1



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								ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	
				DESIGNED BY ML	REVIEWED BY MJB			SHEET 32 OF 42	
				DRAWN BY EV	APPROVED BY MJB				
				SCALE: AS NOTED	ACCEPTED BY DATE				
0	ISSUED FOR TENDER		25.12.01	MJB			SPAN 18 ACCESS PLAN BLAST AND DROP REMOVAL OPTION		
NO.	REVISIONS	DATE	BY	DATE	2025.12.01			32	



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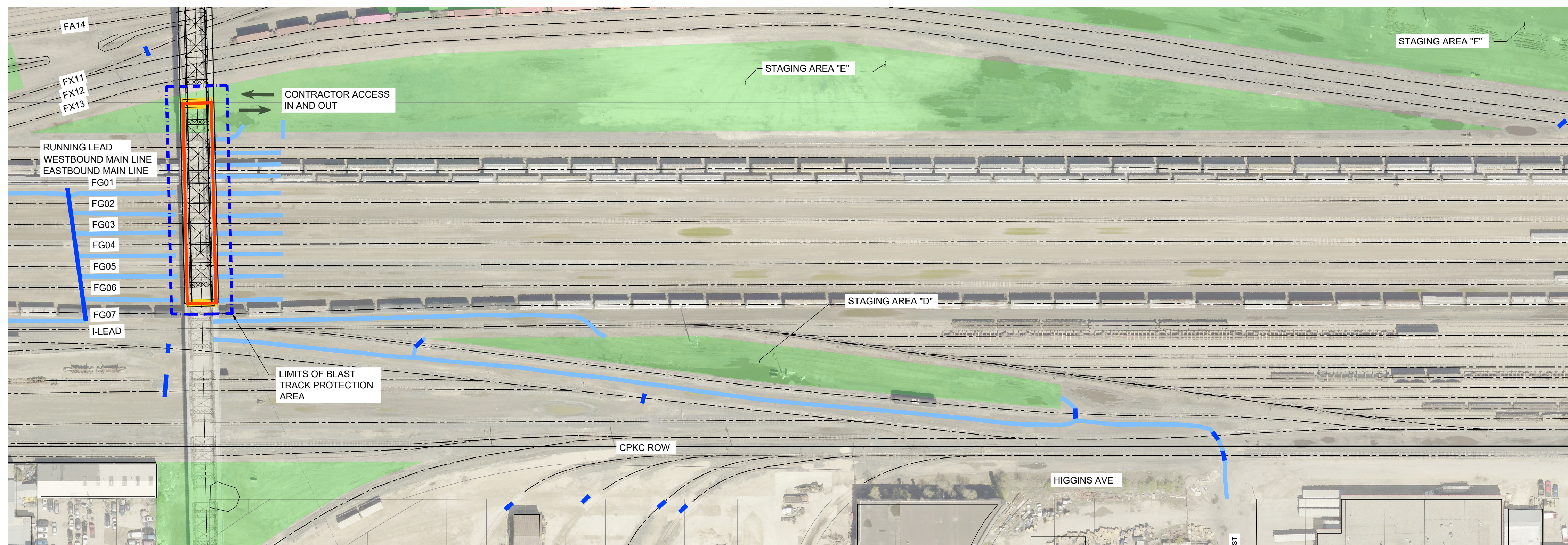
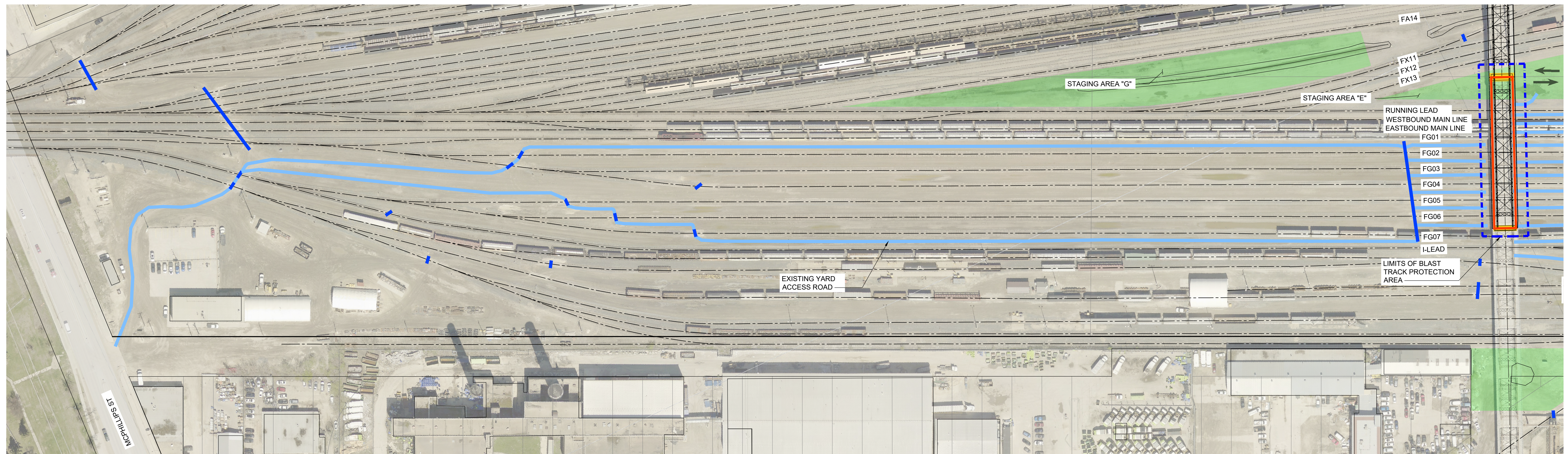
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THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT	
ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	CITY DRAWING NUMBER
SPAN 18 BLAST AND DROP REMOVAL OPTION	SHEET 33 OF 42
	33



SEQUENCE OF WORK

TRACKS AFFECTED:

- FG07, FG06, FG05, FG04, FG03, FG02, FG01, EB MAINLINE, WB MAINLINE, RUNNING LEAD

PRE-WORKBLOCK DEMOLITION WORKS:

- PERFORM LOCALIZED TRUSS MODIFICATIONS/REMOVALS TO FACILITATE THE INSTALLATION OF DEMOLITION CHARGES
- INSTALL DEMOLITION CHARGES 4 HRS PRIOR TO SPAN DEMOLITION

DURING WORKBLOCK DEMOLITION WORKS:

- CPKC TO SETUP TRACK PROTECTION TO TAKE AFFECTED TRACKS OUT OF SERVICE
- PLACE ENERGY ABSORBING BALLAST PILES AND CRANE MATS BELOW SPAN (BY OTHERS)
- SETUP BLAST PERIMETER FENCING AND CLEAR ALL PERSONNEL WITHIN BLAST ZONE
- DETONATE DEMOLITION CHARGES AND DROP SPAN
- REMOVE BLAST PERIMETER FENCING
- DISMANTLE TRUSS COMPONENTS ON GROUND WHILE SIMULTANEOUSLY HAULING DEMOLITION DEBRIS TO MID YARD STAGING AREA "E".
- CLEAN AND REMOVE BALLAST PILES TO BE USED AT NEXT DEMOLITION LOCATION (BY OTHERS)
- CPKC REMOVES TRACK PROTECTION. ALL AFFECTED TRACK CLEAR






POST-WORKBLOCK DEMOLITION WORKS:

- DEMOLITION DEBRIS TO BE REMOVED FROM MID YARD STAGING AREA "D" AND "E" VIA CARS UTILIZING TRACK FX13 OR VIA TRUCK TO NORA ST.

SPAN 19 - ACCESS PLAN

1 : 1000

LEGEND:

-  EXISTING PIER LOCATIONS
-  ACCESS ROUTE FOR SPAN 19
-  EXISTING AT GRADE CROSSING
-  CONSTRUCTION STAGING AREA
-  OVERALL WORK AREA
-  BLAST TRACK PROTECTION AREA

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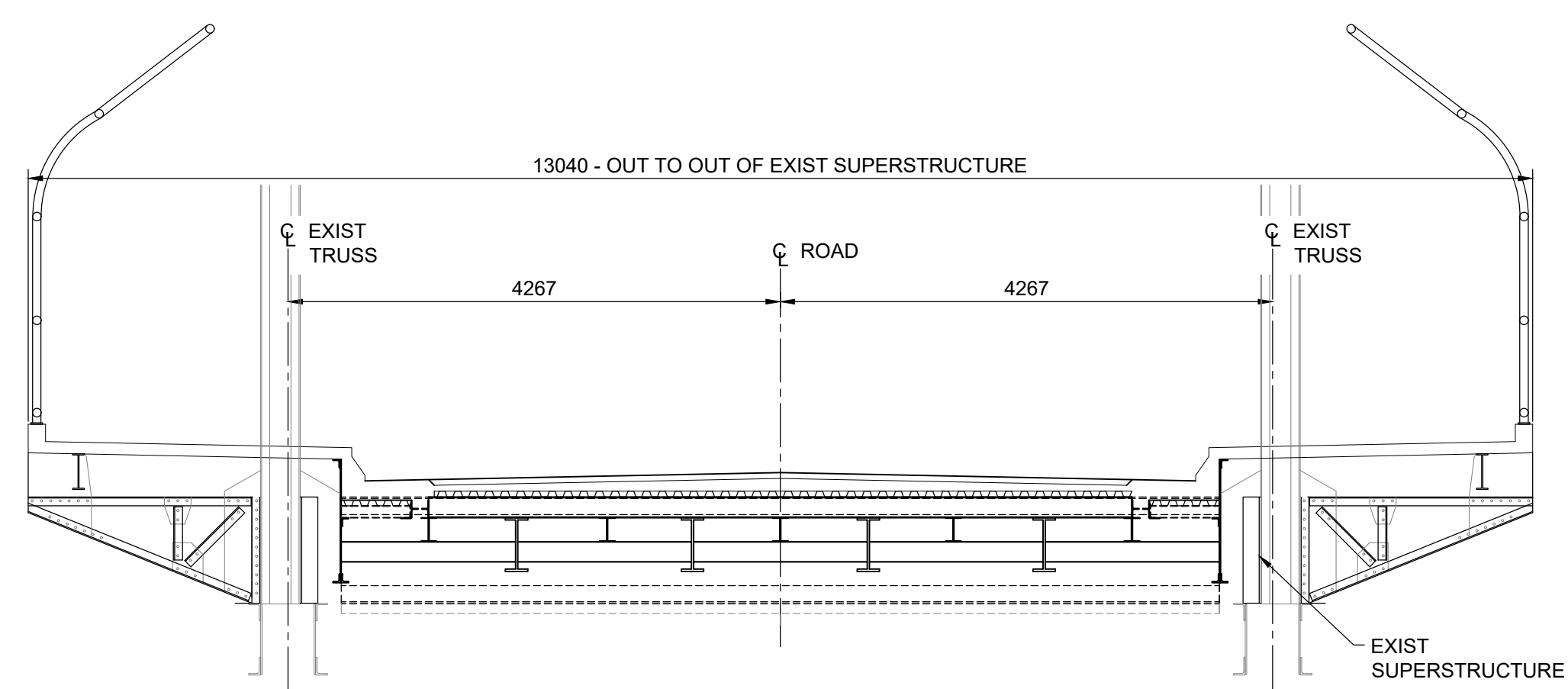
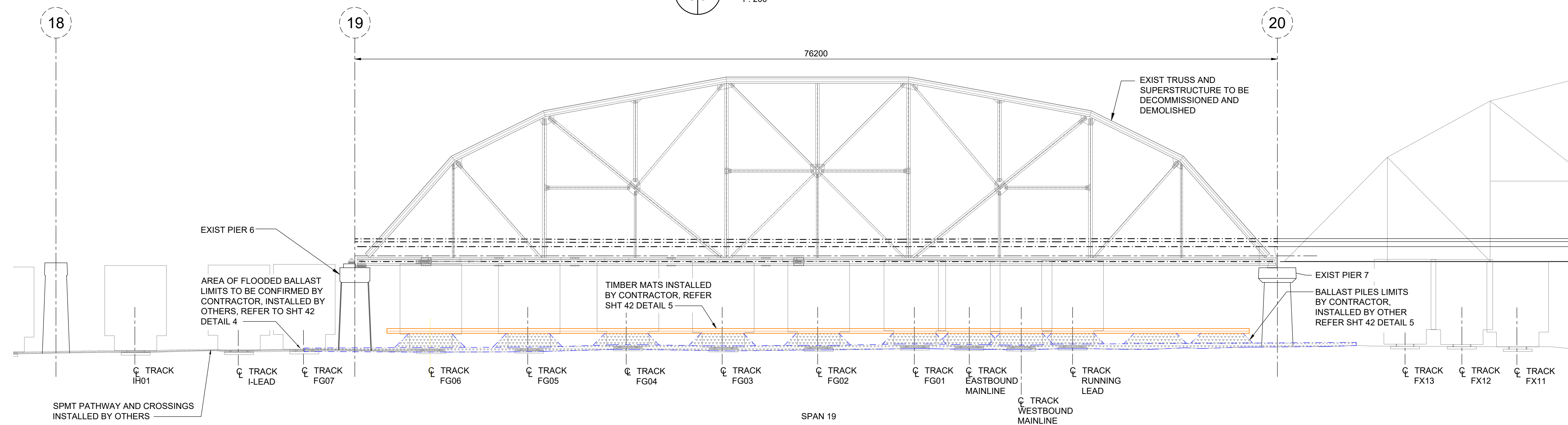
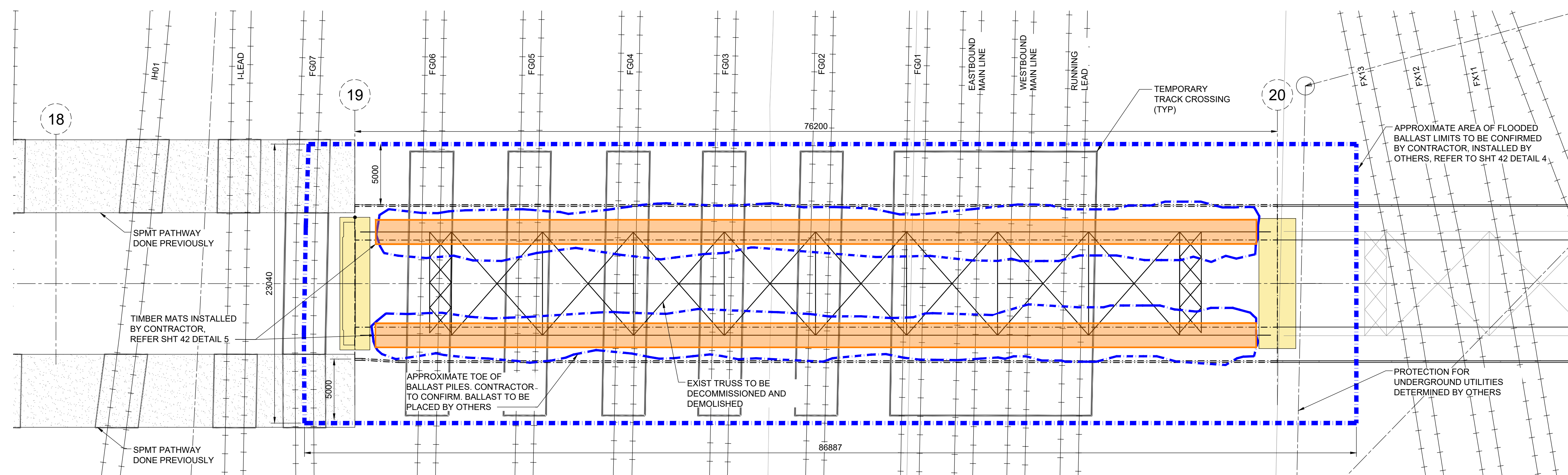
ARLINGTON BRIDGE
PHASE 4 - DECOMMISSIONING

SPAN 19 ACCESS PLAN
BLAST AND DROP REMOVAL OPTION

CITY DRAWING NUMBER

SHEET 34 OF 42

34





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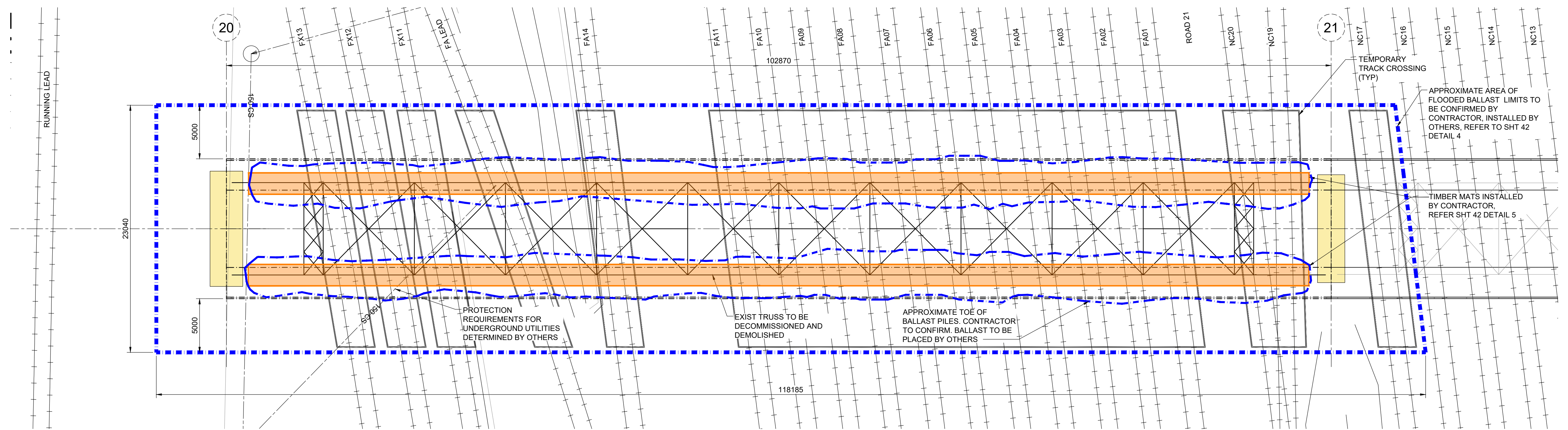
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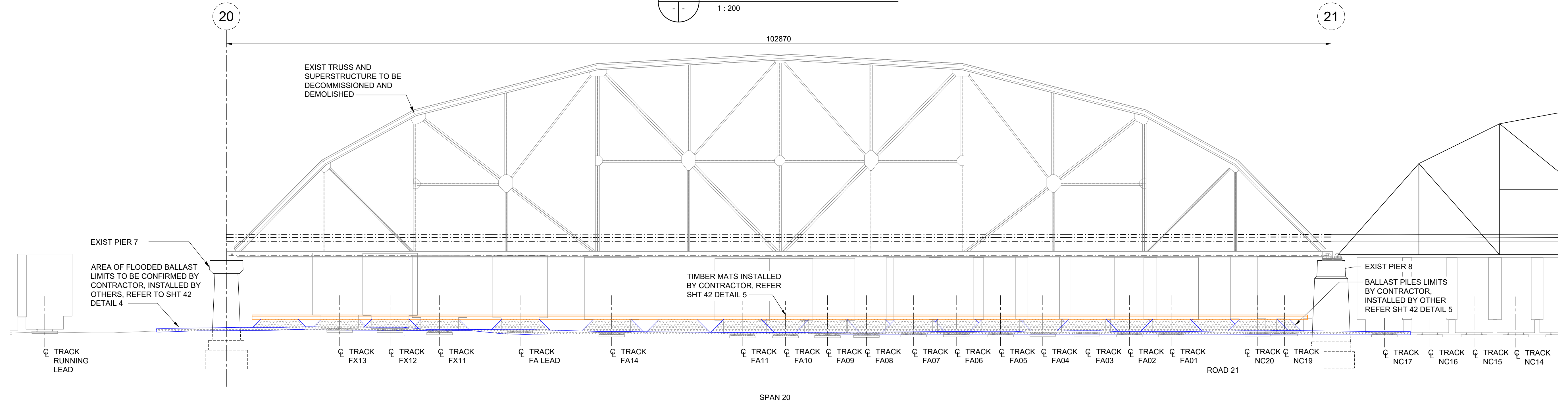
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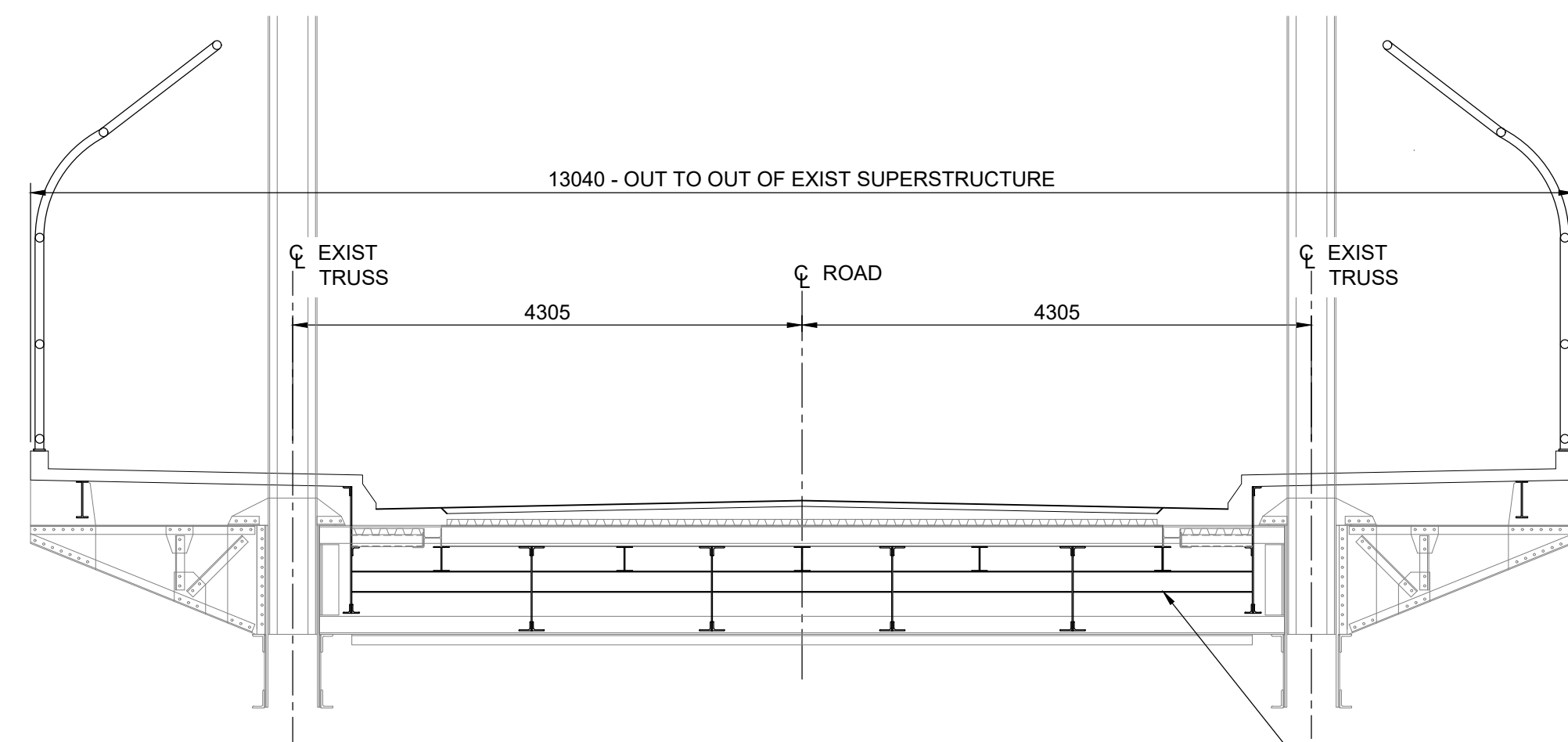
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								ARLINGTON BRIDGE		CITY DRAWING NUMBER	
								PHASE 4 - DECOMMISSIONING		SHEET 35 OF 42	
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1
- -
SPAN 20 - TRUSS PLAN
1 : 200



2
- -
SPAN 20 - EAST ELEVATION
1 : 200



3
- -
SPAN 20 - CROSS SECTION
1 : 50



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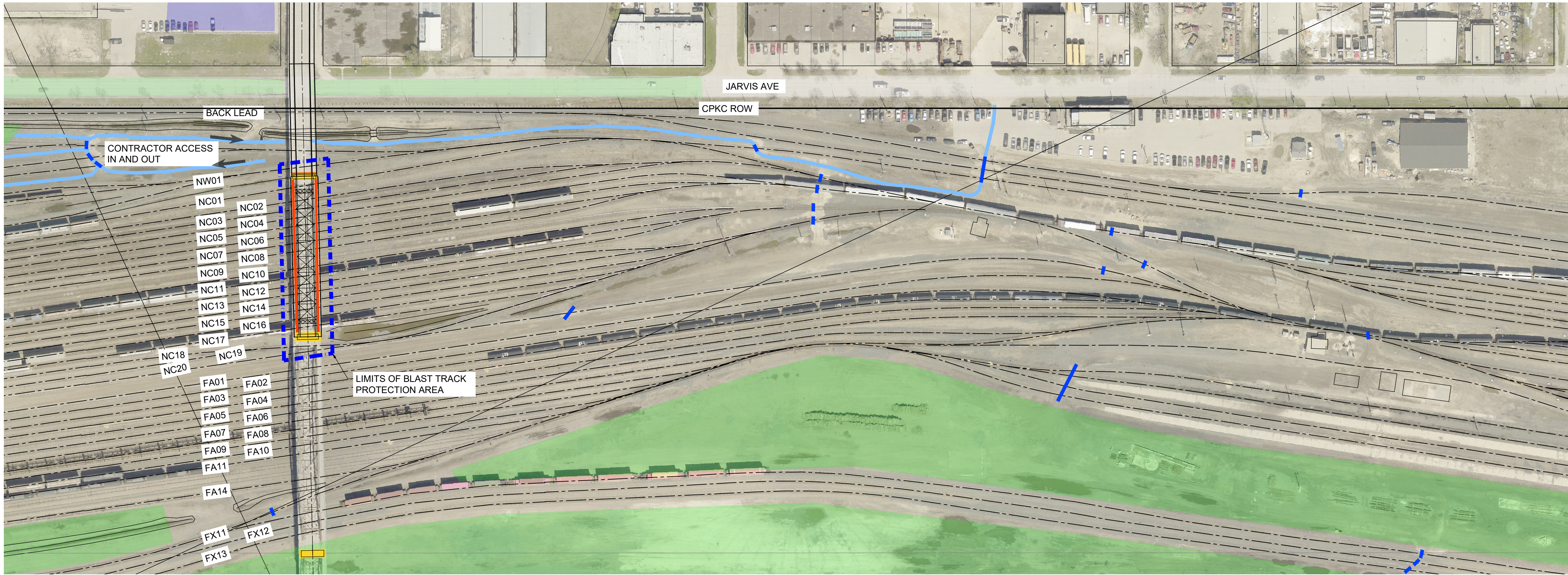
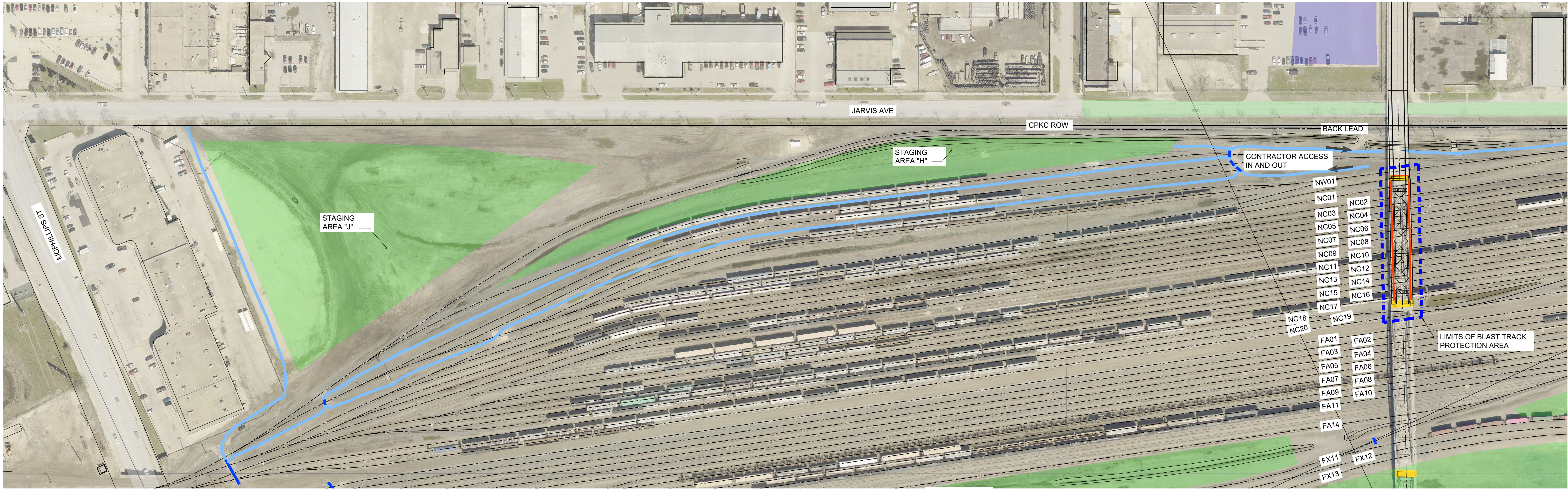
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THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT	
ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	CITY DRAWING NUMBER
SPAN 20 BLAST AND DROP REMOVAL OPTION	SHEET 37 OF 42
	37

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SEQUENCE OF WORK

TRACKS AFFECTED:

- NC01, NC02, NC03, NC04, NC05, NC06, NC07, NC08, NC09, NC10, NC11, NC12, NC13, NC14, NC15, NC16, NC17, NC19, NC 20, NW01, NW02, NW03, NW04, NW05, NW06 AND BACK LEAD

PRE-WORKBLOCK DEMOLITION WORKS:

- REMOVE ALL ANCILLARY BRIDGE COMPONENTS.
- PERFORM LOCALIZED TRUSS MODIFICATIONS/REMOVALS TO FACILITATE THE INSTALLATION OF DEMOLITION CHARGES
- INSTALL DEMOLITION CHARGES 24 HRS PRIOR TO SPAN DEMOLITION

DURING WORKBLOCK DEMOLITION WORKS:

- CPKC TO SETUP TRACK PROTECTION TO TAKE AFFECTED TRACKS OUT OF SERVICE
- PLACE ENERGY ABSORBING BALLAST PILES AND CRANE MATS BELOW SPAN (BY OTHERS)
- SETUP BLAST PERIMETER FENCING AND CLEAR ALL PERSONNEL WITHIN BLAST ZONE
- DETONATE DEMOLITION CHARGES AND DROP SPAN
- REMOVE BLAST PERIMETER FENCING
- DISMANTLE TRUSS COMPONENTS ON GROUND WHILE SIMULTANEOUSLY HAULING DEMOLITION DEBRIS TO MID YARD STAGING AREA "F".
- CLEAN AND REMOVE BALLAST PILES TO BE USED AT NEXT DEMOLITION LOCATION (BY OTHERS)
- CPKC REMOVES TRACK PROTECTION. ALL AFFECTED TRACK CLEAR

POST-WORKBLOCK DEMOLITION WORKS:

- DEMOLITION DEBRIS TO BE REMOVED OFF SITE ACROSS THE BACK LEAD AND ONTO JARVIS AVE.

- LEGEND:**
- EXISTING PIER LOCATIONS
 - ACCESS ROUTE FOR SPAN 21
 - EXISTING AT GRADE CROSSING
 - CONSTRUCTION STAGING AREA
 - OVERALL WORK AREA
 - BLAST TRACK PROTECTION AREA

SPAN 21 - ACCESS PLAN
1 : 1250

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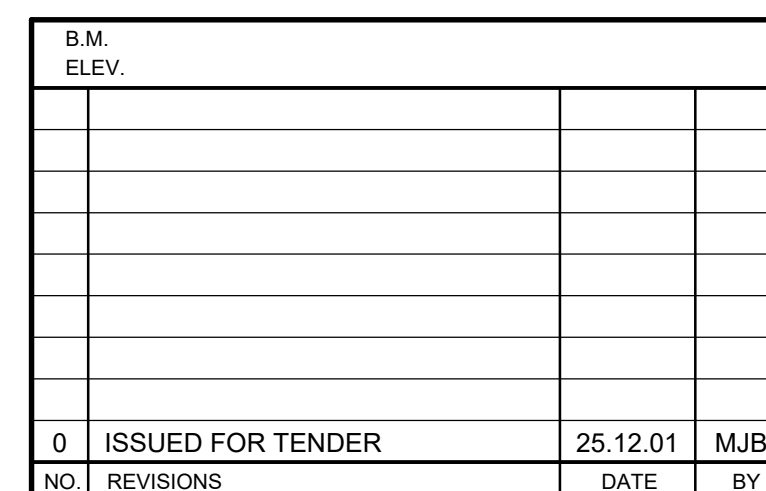
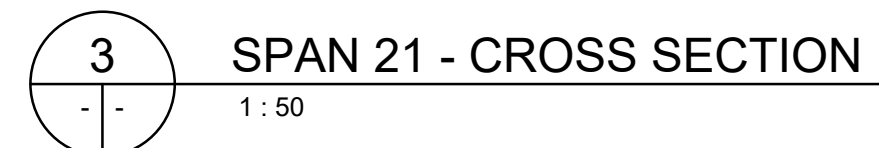
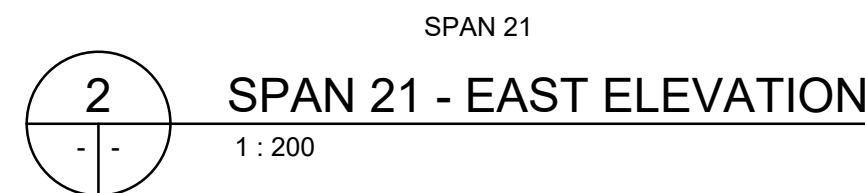
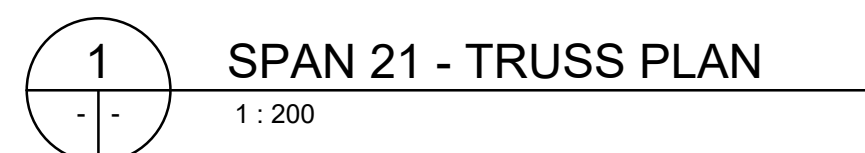
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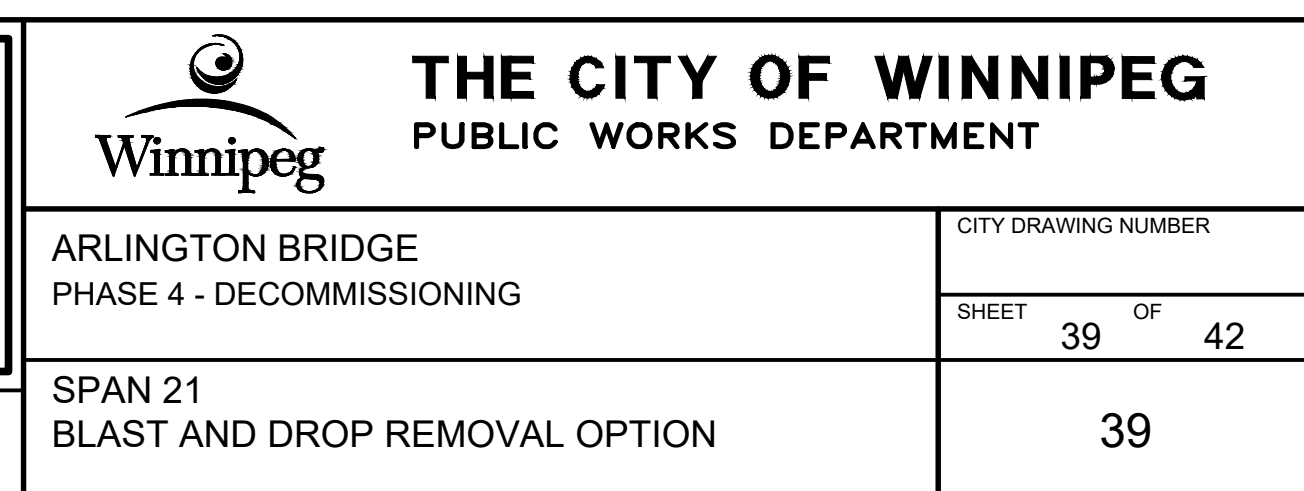
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ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING		SHEET 38 OF 42
SPAN 21 ACCESS PLAN BLAST AND DROP REMOVAL OPTION		38

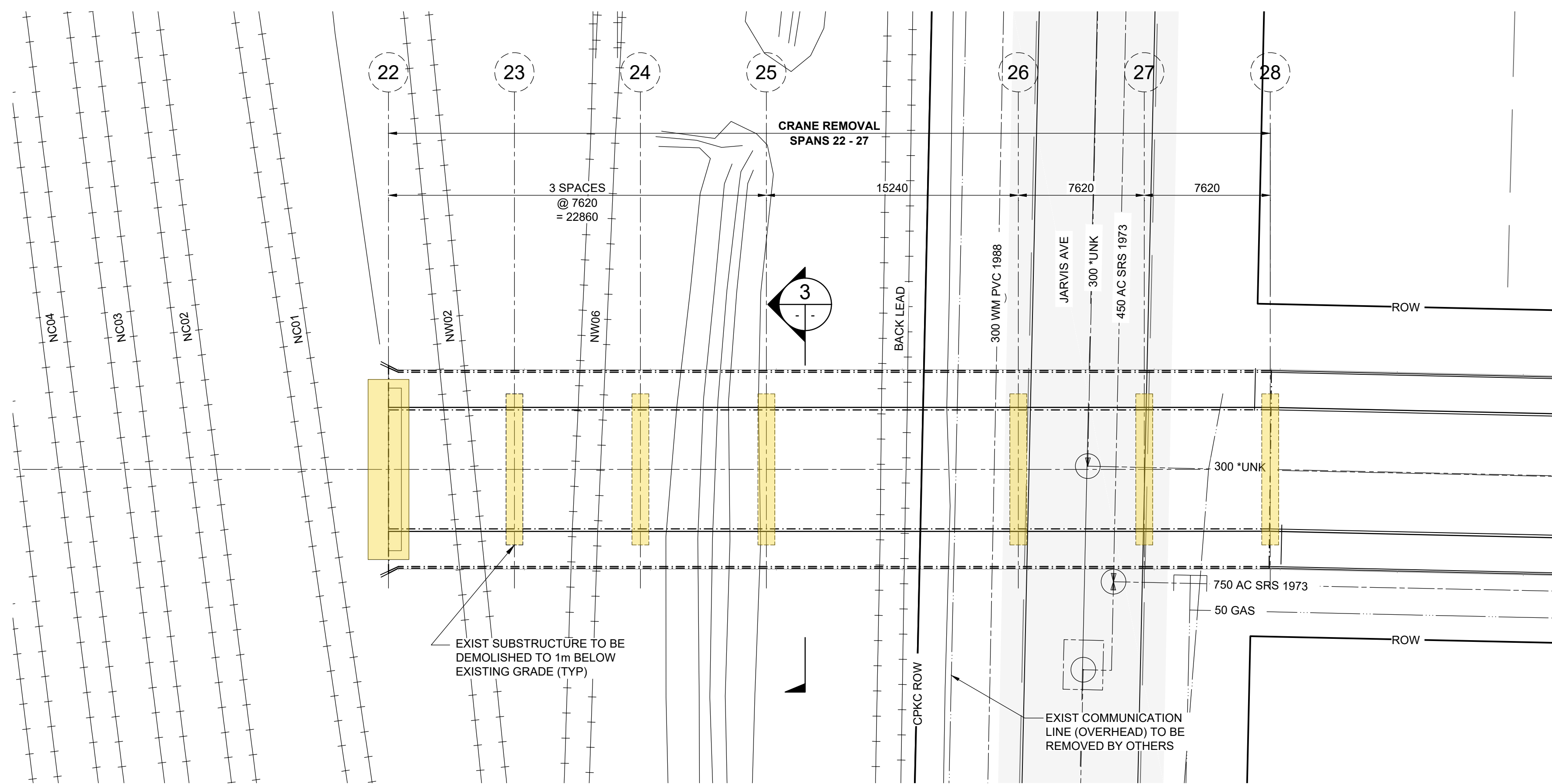


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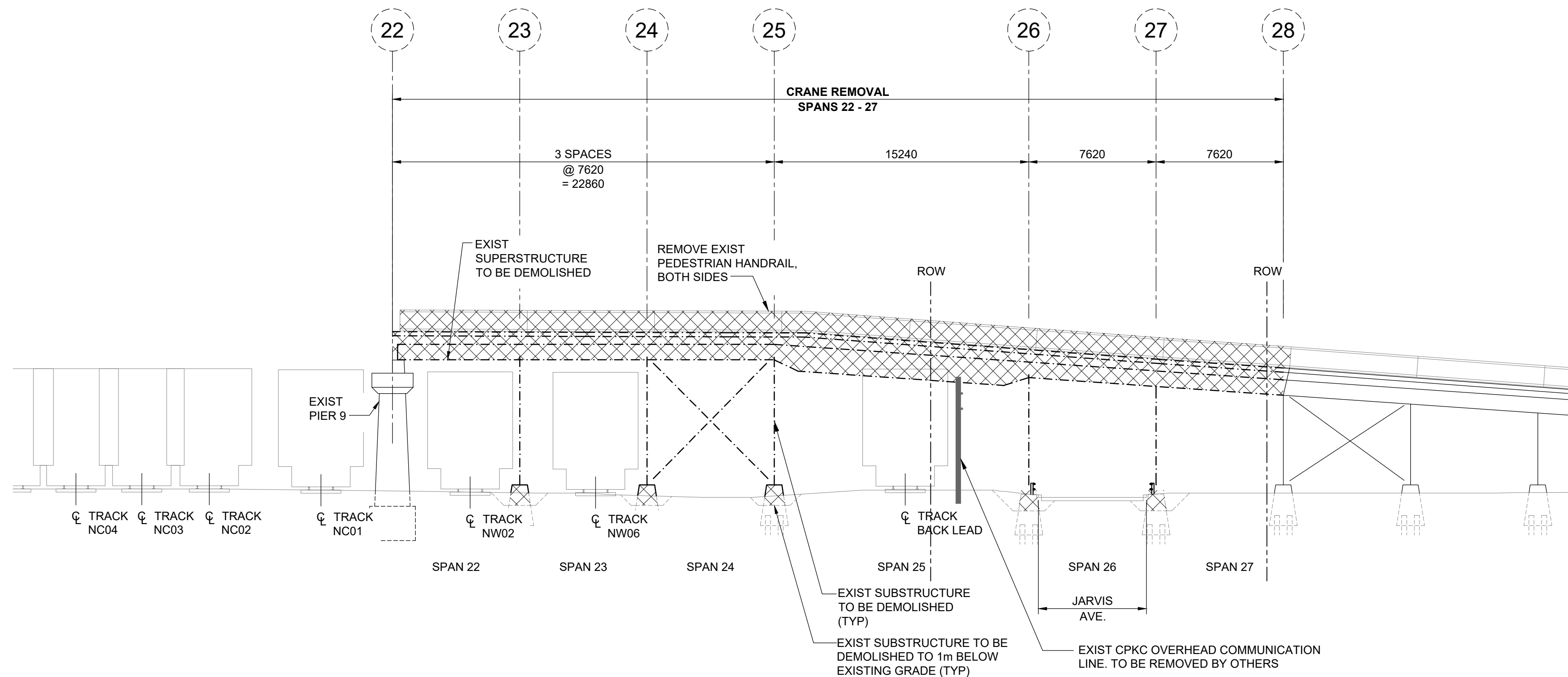
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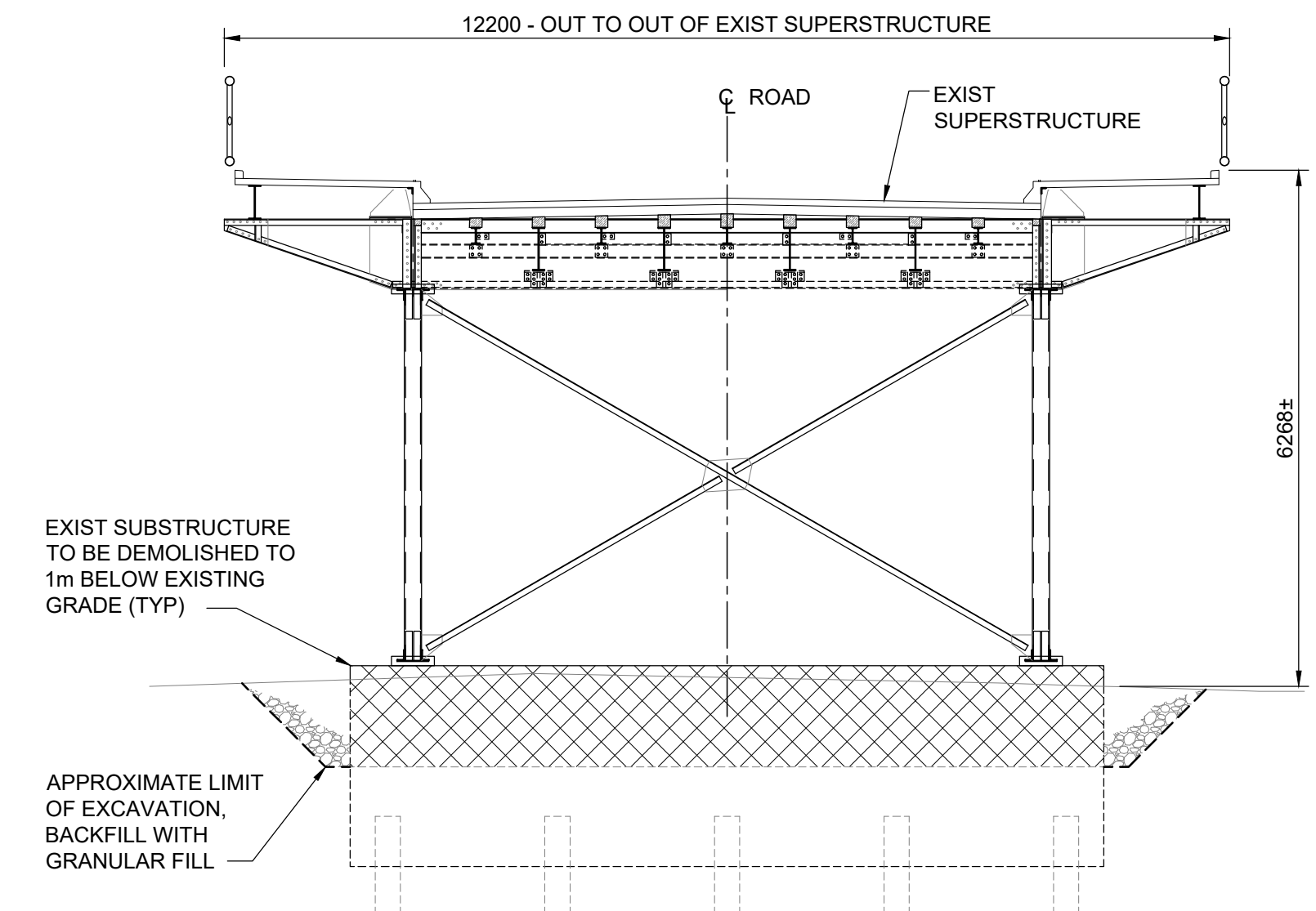




1 SPANS 22-27 - PLAN
1 : 200



2 SPANS 22-27 - EAST ELEVATION
1 : 200



3 CROSS SECTION
1 : 75

ESTIMATED MASS:
SPANS 22-24 - 55,000 kg
SPAN 25 - 105,000 kg
SPANS 26-27 - 55,000 kg

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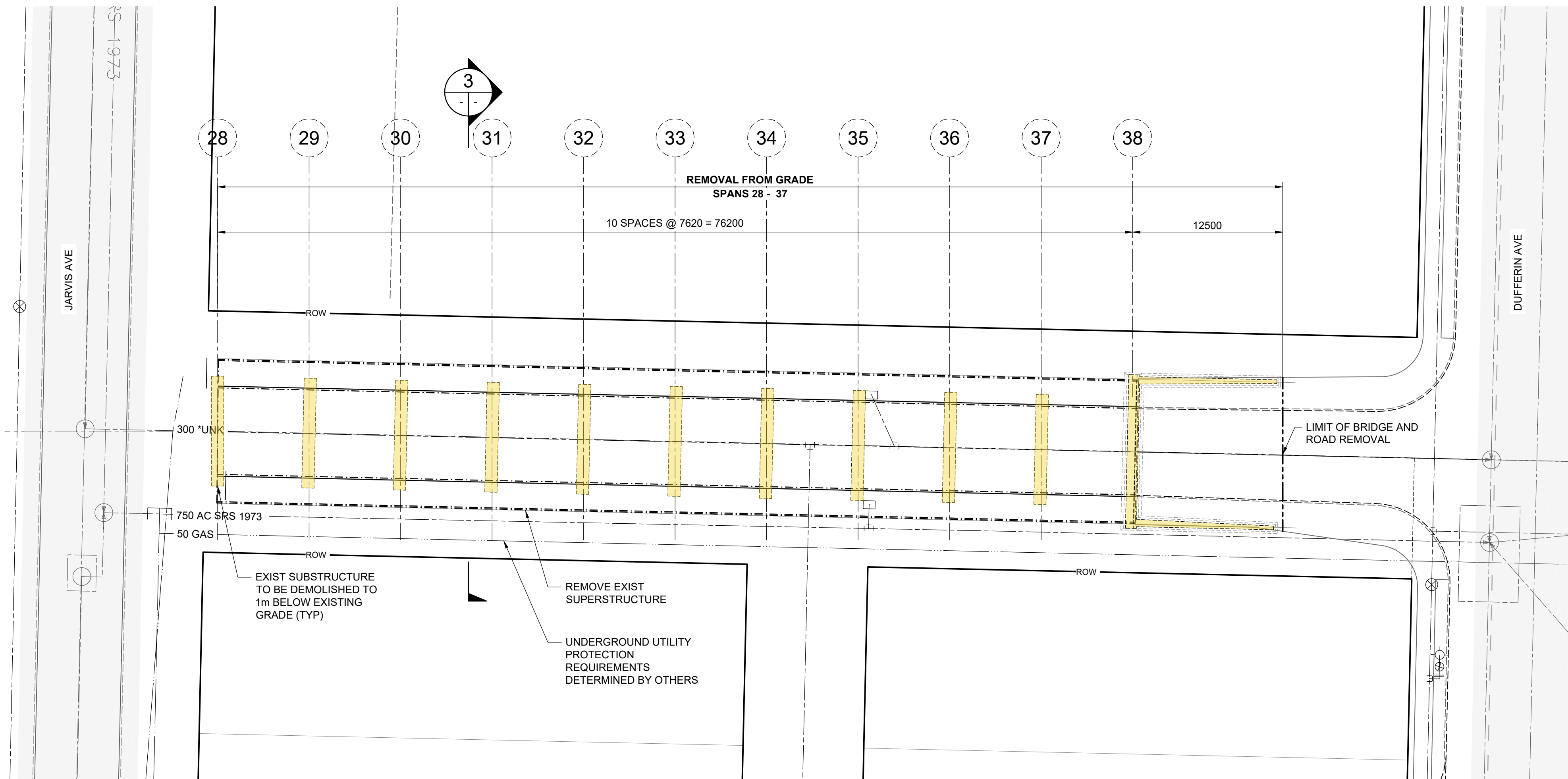
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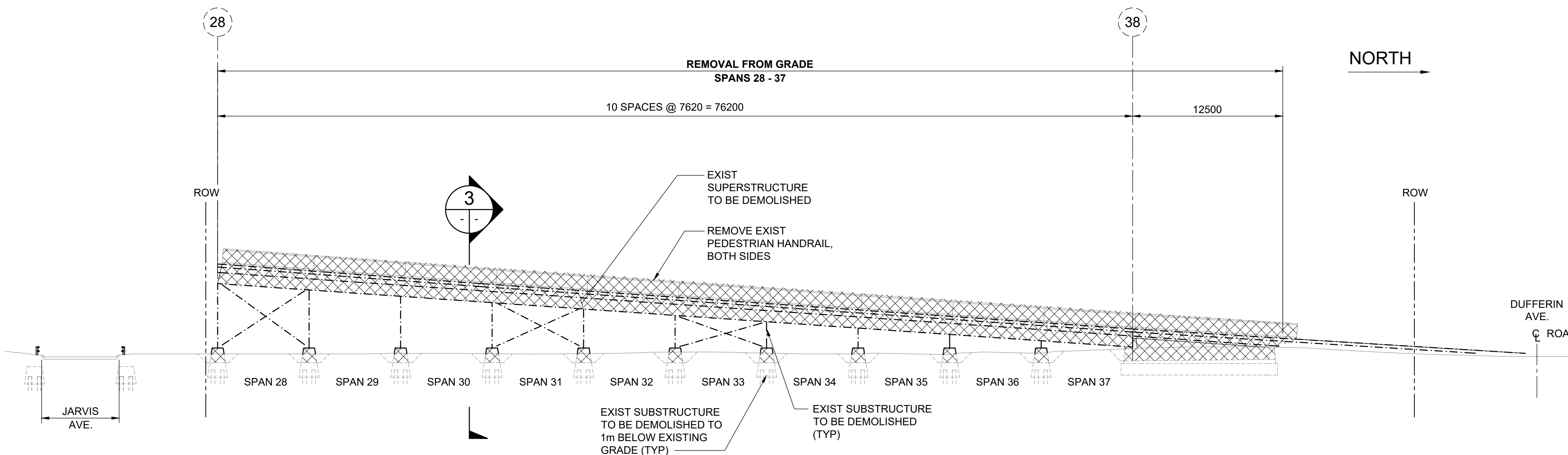
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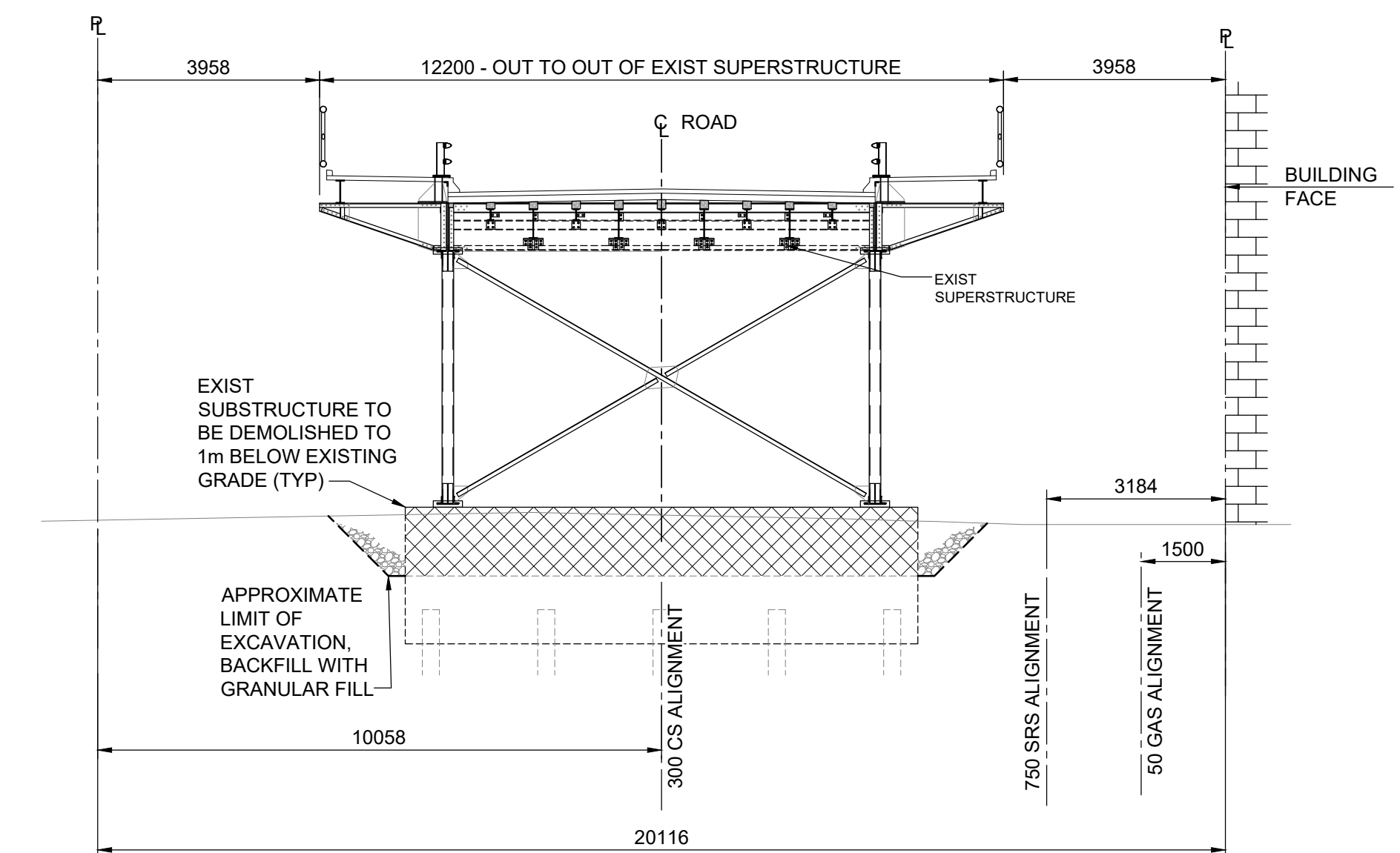
THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT	
ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	CITY DRAWING NUMBER
SPANS 22-27 CRANE REMOVAL OPTION	SHEET 40 OF 42
	40



1 NORTH APPROACH - PLAN
1 : 250



2 NORTH APPROACH - EAST ELEVATION
1 : 250



3 NORTH APPROACH - CROSS SECTION
1 : 100

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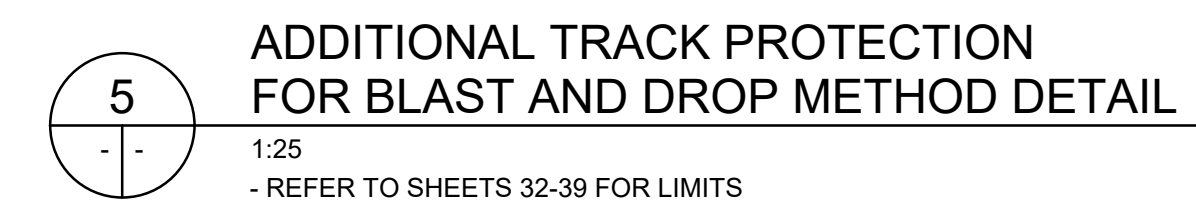
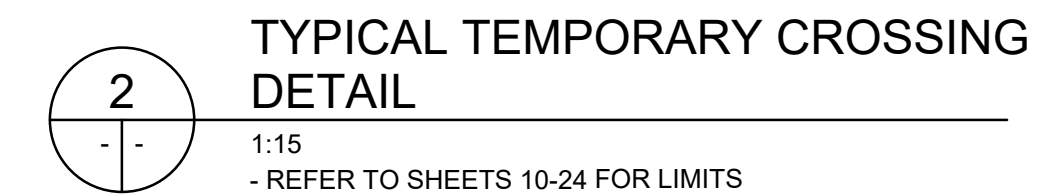
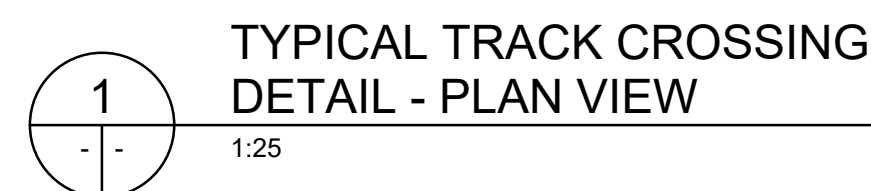
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THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT	
ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING	CITY DRAWING NUMBER
NORTH APPROACH SPANS 28-37 REMOVAL FROM GRADE OPTION	SHEET 41 OF 42
	41

CROSSINGS OTHER THAN 90° TO RAILWAY TRACK



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B.M. ELEV.				 TETRA TECH		<div style="border: 2px solid black; padding: 10px; text-align: center;"> PRELIMINARY DRAWING </div> <p>NOT TO BE USED FOR CONSTRUCTION</p>		 THE CITY OF WINNIPEG PUBLIC WORKS DEPARTMENT	
				DESIGNED BY ML	REVIEWED BY MJB	ARLINGTON BRIDGE PHASE 4 - DECOMMISSIONING		CITY DRAWING NUMBER	
				DRAWN BY EV	APPROVED BY MJB			SHEET 42 OF 42	
				SCALE: AS NOTED	ACCEPTED BY DATE	TYPICAL PROTECTION DETAILS		42	
0	ISSUED FOR TENDER	25.12.01	MJB						
NO.	REVISIONS	DATE	BY	DATE 2025.12.01		CONSULTANT DRAWING NO.			
						704-INF.MBI0304301-S0042			