

# **A Better Bridge for Arlington Public Engagement Report**



May 2019

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### **Acknowledgement**

*Thank you to all Winnipeggers who participated in the Better Bridge for Arlington process. Thank you for stopping to chat with us at our pop-ups, for taking the time to provide input online and in person, and for being open with your thoughts and ideas. Without your willingness to get involved, public engagement would not be possible.*

## 1.0 Introduction

The century old Arlington Bridge is nearing the end of its useable life and must be replaced. The City of Winnipeg (City) is working with technical experts and the public to design a new bridge that will continue to connect our communities while also meeting modern accessibility standards, providing new, and improved active transportation options and better accommodating traffic, including commuters, buses and other large vehicles. Winnipeg's population is growing – forecasted to grow by approximately 200,000 people over the next 25 years – and a better bridge for Arlington will help make it easier for all of us to get around.

This report provides an analysis of the input received from the public engagement process. Winnipeggers' input on their preferences and priorities will be used to shape the recommendations to City Council on the design of a new bridge, and the adjacent lands and corridor.

A full list of key engagement themes and how that information was used is available in section [6.0 Conclusion](#).

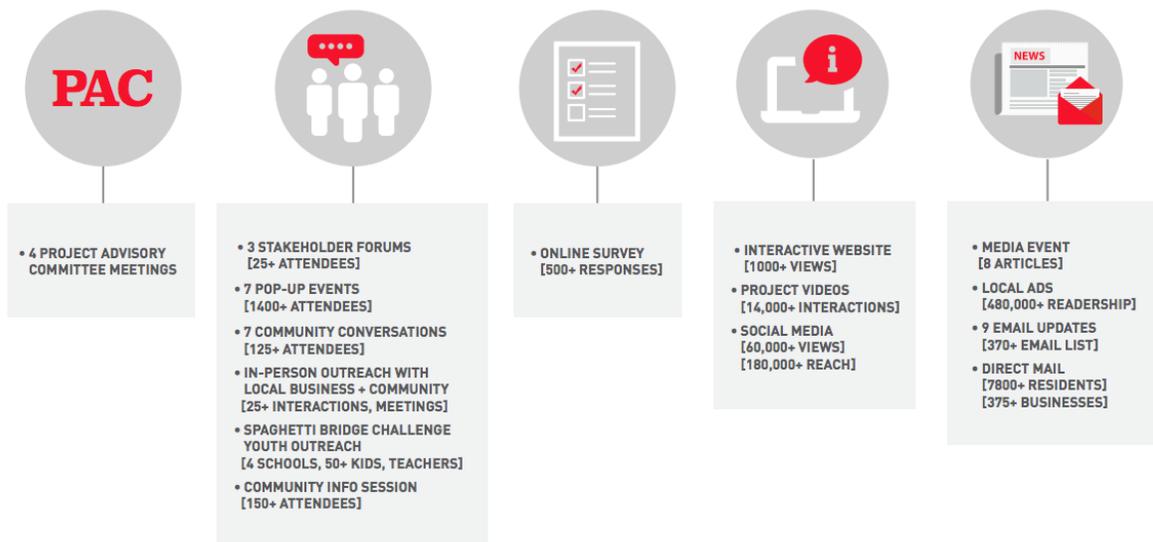
## 2.0 Strategy

The public engagement strategy was developed to ensure that an array of perspectives and viewpoints could be heard to help inform the preliminary design used to shape the recommendation for Council consideration. The following goals guided the public engagement for the Better Bridge for Arlington project:

1. Maintain and build on the public input received and decisions made in earlier studies related to the Arlington Bridge;
2. Inform the public as to why the Arlington Bridge must be replaced;
3. Consult with the public to gather input on key aspects of the project, where public and stakeholder perspectives could meaningfully shape the design and decisions to be made; and,
4. Learn from different community perspectives and address input received in the design of the new Arlington Bridge and related changes to Arlington Street between McDermot Avenue and Selkirk Avenue.

## 3.0 Engagement Activities

A public engagement process for the Better Bridge for Arlington project was designed to enable meaningful engagement with those directly affected or interested, provide greater transparency in the design process, and ensure stakeholder and public input was considered in the design. Stakeholder and public engagement was conducted between April 2017 and February 2018. Public and stakeholder engagement activities and participation are summarized in the following graphic.



It was determined early on that awareness of the project would be essential to the success of the public engagement process. The Better Bridge for Arlington Project Advisory Committee (PAC) provided advice on how to effectively create awareness of opportunities to provide input in the local communities north and south of the bridge. Promotion included events, hand-delivered notices, emails to stakeholders, mailed letters, newsletters, social media posts, print ads, posters, and signs. Details about these events are available in section [3.2 Events](#). [Videos](#) were also produced featuring PAC members, which highlighted different aspects of the project for community feedback, awareness of the opportunities for engagement, and a compilation of engagement results highlighting key themes.

## Methods of Promotion

Method of Promotion	Date	Reach
City of Winnipeg Website	June 29, 2017	8,992 page views
Media events & news releases	August 30, 2017	8 articles generated
	January 25, 2018	5 articles generated
Facebook posts (City of Winnipeg)	August 30 - September 30, 2017	183,299 online impressions
	January 25 - February 12, 2018	26,612 online impressions
Twitter posts (City of Winnipeg)	August 30 - September 30, 2017	61,104 online impressions
	January 29 - February 12, 2018	24,878 online impressions
Community posters, signs & handouts	August 30 - September 30, 2017	1,900 distributed/posted
	January 31 - February 5, 2017	1,100 distributed/posted
Pop Up events	September 6 - 16, 2017	1,400 community member
Direct mail	September 6, 2017	7,800 project area residents
		375 project area businesses
Stakeholder emails	August 9 - September 14, 2017	219 stakeholders
	December 18, 2017	284 stakeholders
	January 31, 2018	315 stakeholders
Print media (news & ads)	August 30 - September 18, 2017	480,000 total readership
	Feb 3 - 4, 2018	
Youtube videos	September 2017	2,985 views
	January - February 2018	2,924 views
City of Winnipeg public engagement newsletter	September 7, 2017	sent to over 5,300 subscribers
	September 21, 2017	
	February 2, 2018	
	February 15, 2018	

### 3.1 Project Advisory Committee



The PAC helped ensure that local perspectives were represented and clearly heard at all stages of the project. Members of the committee represented a broad range of views, including community residents and business-owners, children and youth, seniors, Indigenous Peoples, cyclists, newcomer Canadians, people living with disabilities, and organizations that help support area needs.

PAC members were committed community partners representing a variety of neighbourhood interests who collectively spent hundreds of hours on the project. The PAC was originally formed in 2014 and met regularly throughout earlier design phases to help develop the vision and goals for the project as well as provide input on how a Better Bridge for Arlington could help their communities thrive.

As part of the engagement process on the preliminary design, the project team re-engaged the same PAC members, and the committee met formally four times from 2017 to 2018 and participated in numerous other stakeholder meetings and events. They provided input on project design criteria, community engagement, key design considerations, and the proposed design.

The committee worked closely with the project team to identify other organizations or groups that were not directly represented on the committee who may be impacted by the project and had helpful perspectives or insights to share, including community, Indigenous, business, and resident groups.

### Project Advisory Committee Members

Name	Organization
Ken Shachtay	Access Advisory Committee, City of Winnipeg
Mark Cohoe	Bike Winnipeg
Gord Dong	Centennial Community Improvement Association
Wayne Wyke	Dufferin School
Carole Frechette	Indigenous Relations Division, City of Winnipeg
Ken Kollinger	Health Sciences Centre/Winnipeg Regional Health Authority
Aaron Benarroch	King Edward Community School
Dustyne Lefurgey	Ndinawe
Richard Gilbert	North End Community Renewal Corporation
Mike Pagtakhan	Councillor, Point Douglas
Phil Chiappetta	Rossbrook House
Pam McConnell, Samantha Rodeck	Transportation Options Network for Seniors
Tricia Wasney, Alexis Kinloch	Winnipeg Arts Council

### 3.2 Events



Several events and meetings were held between August 2017 and February 2018 to provide multiple, convenient opportunities for community members and bridge users to participate. Questions and discussion were consistent with questions and topics included in the online survey and focused on aspects of the project where public input would help shape the bridge design. These included: uniqueness of bridge design, bridge features, land development opportunities, and public art. Events are described in the following sections and included:

- **7 pop-up events** to raise awareness of the project and public engagement opportunities;
- **3 public forums** to address key issues and impacts arising from the project;
- **7 community conversations** to engage with harder-to-reach residents and community members impacted by the project and who had critical insights to share on specific aspects of the bridge design;

- **In-person outreach (including but not limited to 7 meetings and 25 in-person conversations)** with local businesses and communities affected by specific aspects of the project, such as changes in road access, property requirements, and bus stops; and,
- **The Spaghetti Bridge Challenge** in four schools to promote learning about the community and bridge design, and to provide area families with information about the project.

Input from events and in-person outreach were grouped according to similar themes and is presented in [Section 5.2](#).

### 3.2.1 Pop-up events

Pop-up events to promote the project and gather input were held in a number of locations in the project area. Locations were chosen to reach commuters (drivers, pedestrians, and cyclists), area residents, and a variety of demographics and ages reflective of the local communities. Project team members handed out project information and interacted with more than 1,400 individuals, recording feedback on iPads and survey forms. The following are the dates and locations of the pop-up events:

- September 6, 2017 – Freight House Grill n’ Chill
- September 9, 2017 – Sherbrook Street Festival
- September 10, 2017 – Ciclovía (Manyfest)
- September 12, 2017 – North Centennial Recreation and Leisure Facility
- September 13, 2017 – Health Sciences Centre
- September 16, 2017 – Save on Foods, Northgate Mall
- September 29, 2017 – Arlington Bridge at Dufferin Avenue

### 3.2.2 Public forums

Public forums were held on three specific aspects of the design where the project team needed to test ideas and concepts: land use, owned property impacts, and traffic and construction design. Forums were promoted in advance via social media, email, advertising, posters, pop-up events, and mailers. In addition, letters were sent directly to property owners potentially affected by the project inviting them to a property owners forum. All forums included an overview presentation followed by facilitated discussion. In total, 25 people attended.

- September 19, 2017 – Land Use Forum – North Centennial Leisure Centre
- September 26, 2017 – Connections & Construction Forum – North Centennial Recreation and Leisure Facility
- September 27, 2017 – Property Owners Forum – Burton Cummings Community Centre

### 3.2.3 Community conversations

In September and October 2017, the engagement team worked with several community partners in the area to host conversations with their members and networks. Partner organizations suggested times, locations, and formats that would be most convenient and effective. Community conversation events addressed the same topics covered at public forums in the online survey, but the events also addressed topics of relevance to each group, including accessible design, housing, traffic access, and safety. More than 125 people participated in these sessions.

- September 15, 2017 – Access Advisory Committee Meeting
- September 15, 2017 – Meet Me at the Bell Tower
- September 20, 2017 – Bear Clan Walk
- September 28, 2017 – Faculty of Social Work (Selkirk Avenue)
- October 12, 2017 – Bear Clan Walk
- October 18, 2017 – Dufferin Residents Association of Winnipeg AGM
- October 19, 2017 – National Microbiology Lab Staff Meeting

### 3.2.4 In-person outreach with local businesses and community

Individual meetings and outreach supplemented the engagement events for this project to help ensure groups, businesses, and individuals directly affected were aware of the impacts and had an opportunity to provide feedback. Invitations were provided to property owners, area businesses, and community group leaders to explain potential property impacts, key aspects of the design, and to gather feedback. Invitations were sent by mail and email. Team members delivered information door-to-door to businesses on Dufferin Avenue to seek feedback on both design options and the potential closure of Dufferin Avenue at Arlington Street. In total, 25 interactions and seven meetings were held.

- June 27, 2017 – Door-to-door, in-person conversations Dufferin Avenue businesses
- January 16, 2018 – Property owners meeting (residential)
- January 16, 2018 – Property owners and area businesses meetings
- January 17, 2018 – Property owners meeting (National Microbiology Lab)
- January 18, 2018 – Property owners meeting (Winter Collision Repair)
- January 22, 2018 – Area stakeholder meeting

### 3.2.5 Spaghetti Bridge Challenge youth outreach



Presentations were given to area youth about the project. Students at four schools brainstormed ideas for the bridge, drew blueprints, and constructed their own spaghetti bridges, which were shown at the February 8, 2018 community information session event. Spaghetti and supplies for the bridges were donated by Sobey's Cash and Carry and the Stantec project team. Students who participated in the challenge were entered to win a Winnipeg Jets prize package. In total, 50 elementary school students participated in the Spaghetti Bridge Challenge.

- October 30, 2017 – Spaghetti Bridge Competition – King Edward School
- November 9, 2017 – Spaghetti Bridge Competition – Dufferin School
- December 1, 2017 – Spaghetti Bridge Competition – Samuel Burland School
- December 6, 2017 – Spaghetti Bridge Competition – Sir Isaac Newton School

### 3.3 Engagement Videos



#### Arlington Bridge - Beyond the Bridge

Videos were produced to highlight different aspects of the project for community feedback and to increase awareness of the opportunities for engagement. Videos are available on YouTube: [youtube.com/channel/UClbGHHM4vS\\_wK9tVdOld8pQ](https://youtube.com/channel/UClbGHHM4vS_wK9tVdOld8pQ)

### 3.4 Online Survey

An online survey was available from August 30, 2017 to September 30, 2017. Questions focused on aspects of the project where there was an opportunity for public input that would assist the project team in the design process and were consistent with questions and topics discussed at in-person events. Topics included uniqueness of bridge design, bridge features, land development opportunities, and public art. Hard copies of the survey were also made available at events and pop-ups. More than 500 online and in-person survey responses were submitted. A full list of survey questions is provided in Appendix A.

### 3.5 Community Information Session and Celebration



A community event was held at the former North Centennial Recreation and Leisure Facility (now named Sergeant Tommy Prince Place) at 90 Sinclair St. on February 8, 2018 to share the recommended preliminary design for the Arlington Bridge and corridor. The event featured information boards, maps, a brief presentation and thank-yous, videos, scale models of the current and proposed bridges, and a display of spaghetti bridges. Information shared at the event was also posted on the project website, and participants were encouraged to complete a comment form. More than 150 people attended.

## 4.0 Analysis

Input was received in different ways during the public engagement process. Survey data was analyzed directly in the online survey tool and is presented in [Section 5.1](#). Input from events and in-person outreach was grouped according to similar ideas and is presented in [Section 5.2](#). Similarly, key themes emerging from project advisory committee feedback is presented in [Section 5.3](#).

## 5.0 Results

Results from the online survey, community events, and PAC are highlighted in the following sections. Full results and notes associated with each activity are available in the appendices: Appendix D – Online Survey; Appendix C – Events; and Appendix A – Project Advisory Committee.

### 5.1 Online Survey

#### Level of support

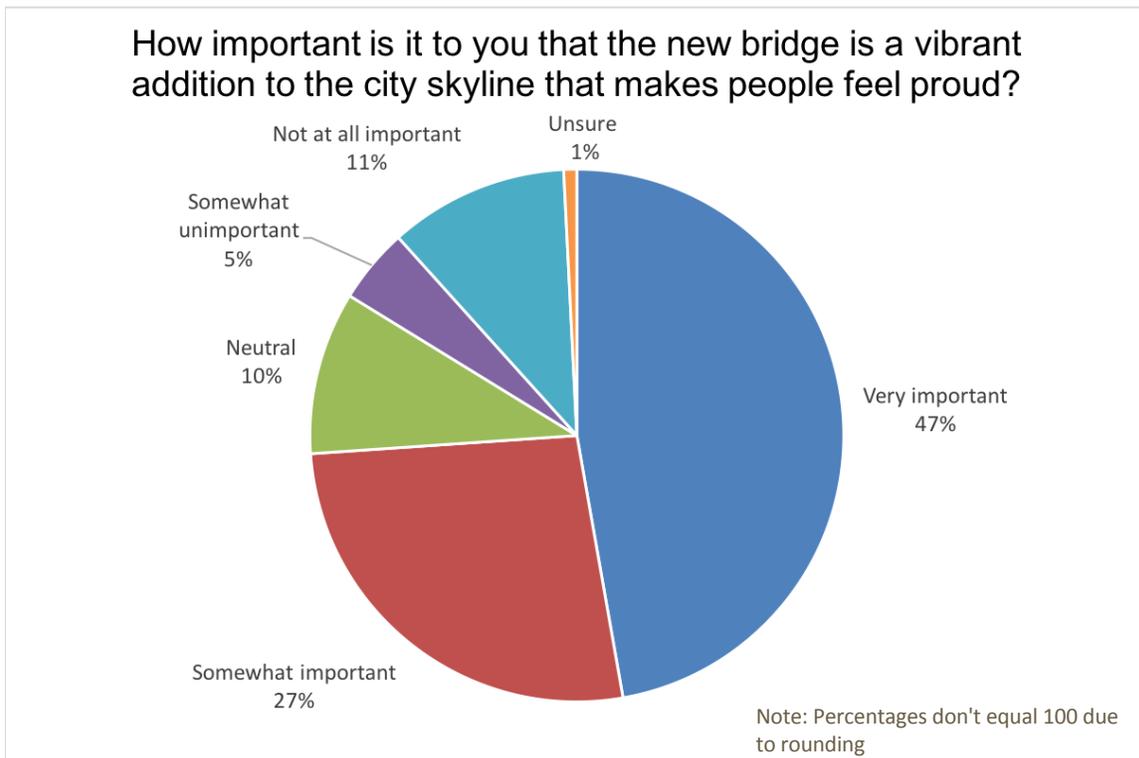


Figure 5-1

Respondents were asked to indicate how important a new bridge would be to the city’s skyline and identity. The majority of respondents (74%) said it was important and 11% had a neutral response or were unsure. Only 15% said it was unimportant.

**Beneficial use of open land near the new bridge**

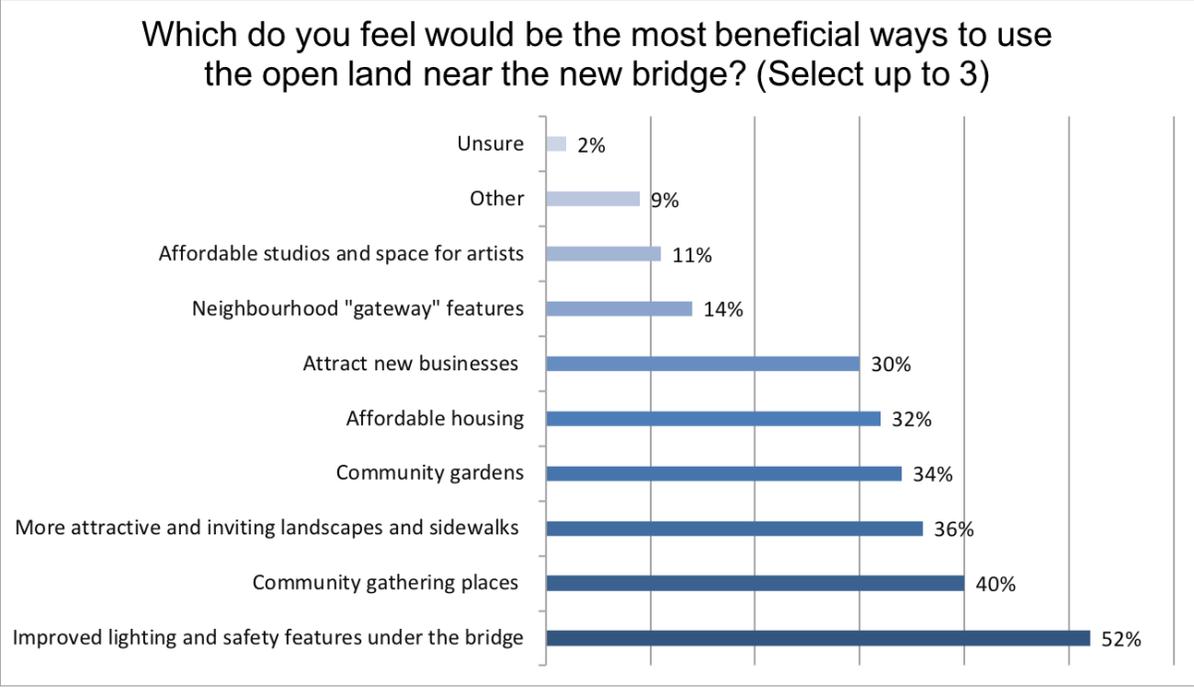


Figure 5-2

Respondents were asked to indicate which uses of open land near the new bridge would be beneficial. Fifty-two percent (52%) of respondents indicated improved lighting and safety features under the bridge would be most beneficial; 40% chose community gathering places; and 36% chose more attractive and inviting landscaping and sidewalks on routes to existing local businesses and community destinations.

Respondents were also given the opportunity to provide other ideas and suggestions. More than 100 respondents shared a wide array of ideas. The most commonly shared ideas related to:

- Green space and public parks (17)
- Youth sports facilities and outdoor rinks (14)
- Community gardens for producing food (10)
- Affordable housing and social services (7)

## Features on the new bridge

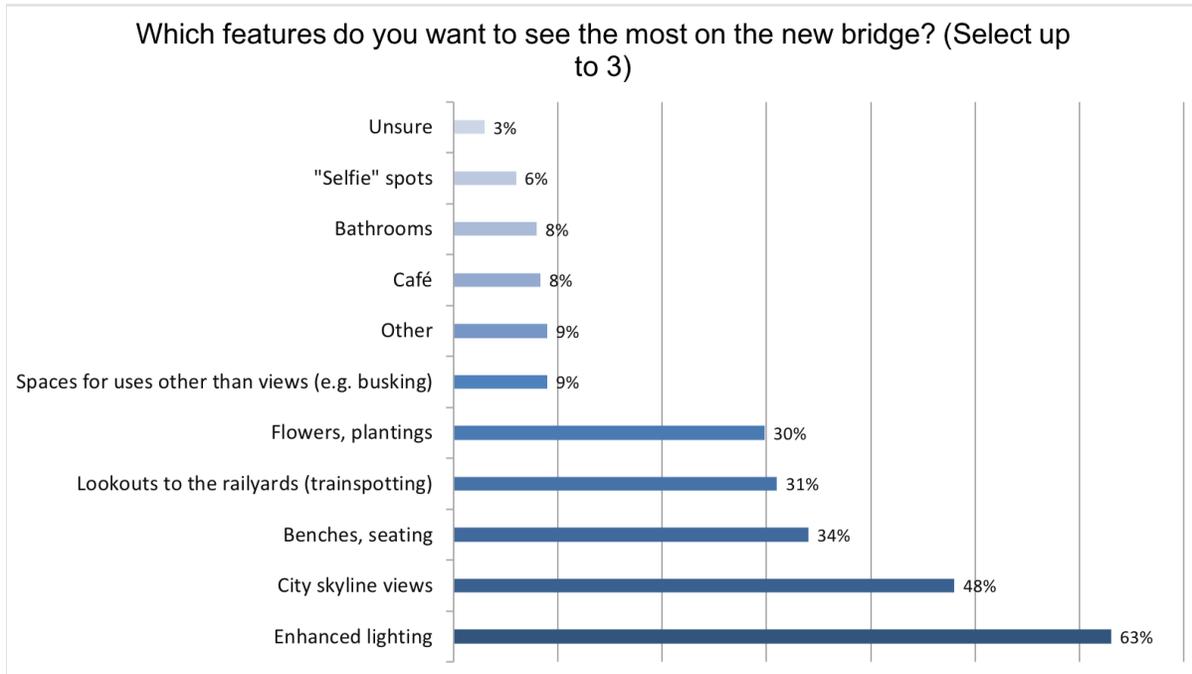


Figure 5-3

Respondents were given the opportunity to identify which features they wanted to see most on the new bridge. The primary feature selected was enhanced lighting (63%), followed by city skyline views (48%), benches and seating (34%), lookouts to railyards (31%), and flowers, plantings (30%).

Respondents were also given the opportunity to provide other ideas and suggestions. Over 100 respondents shared a wide array of ideas. The most commonly shared ideas related to:

- Safety features and lighting a need-to-have; others are nice-to-haves. (17)
- Conservative use of construction budgets and consideration for spending; “we don’t need any extras.” (15)
- Biking ideas – from a bicycle repair station to wide, dedicated paths and public washrooms for people who use active transportation. (9)
- Sheltering spots to escape the worst of the elements – protection from wind, rain, and snow. (3)

## Themes for public art

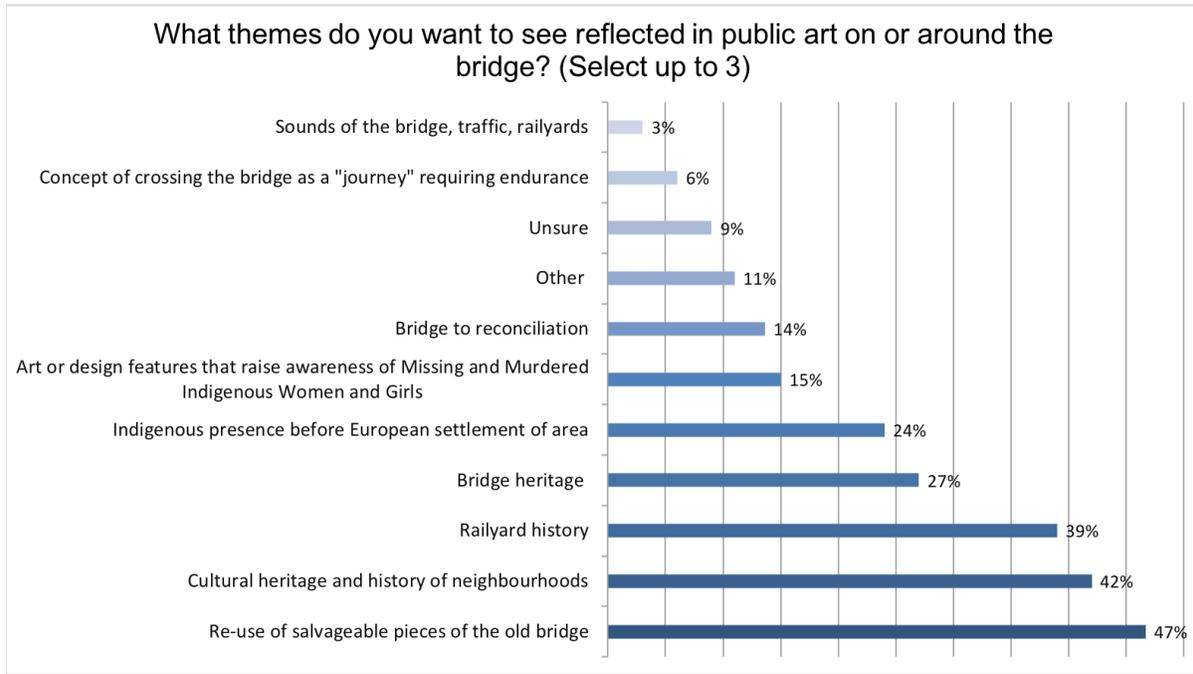


Figure 5-4

Respondents were asked which themes they wanted to see reflected in public art incorporated on or around the new bridge. Re-use of salvageable pieces of the current bridge was selected by 47% of respondents. Forty-two percent (42%) indicated they would like cultural heritage and history of the neighbourhoods past and present as a theme for public art, and 39% chose rail yard history.

Respondents were also given the opportunity to provide other ideas and suggestions. Nearly 90 respondents shared a wide array of ideas. The most commonly shared ideas related to:

- Indigenous presence, noting both cultural heritage and recognition for missing and murdered Indigenous women (11)
- Multiculturalism, diversity, reflected as the spirit of the North End (7)
- Public art that preserves/repurposes the bridge (6)
- Public art should be cost effective (4)

## Priorities

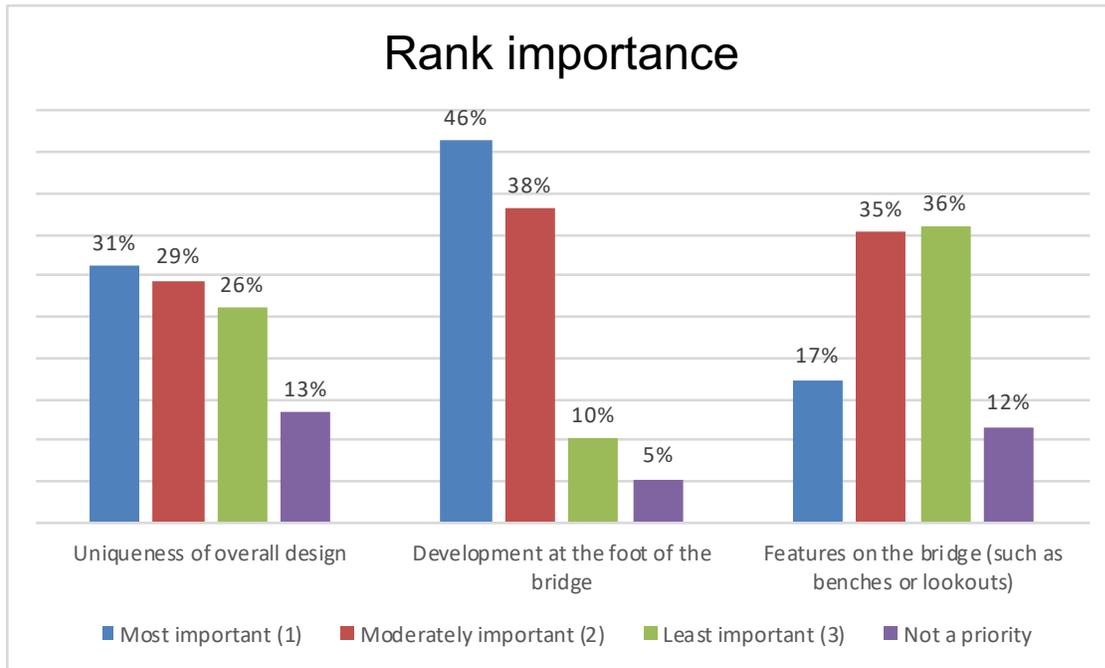


Figure 5-5

Respondents were asked to rank aspects of the bridge design in order of importance. Development at the foot of the bridge garnered the highest number of responses as “most important” at 46%, followed by uniqueness of the bridge (31%). Features on the bridge were viewed as least important (36%). Some respondents felt one or all aspects of the project were unimportant (5–13%).

Respondents were also given the opportunity to provide other ideas and suggestions. Nearly 200 respondents shared a wide array of ideas. The most commonly shared ideas related to:

- Function over aesthetics. (26)
- The most important thing is safety. In order for pedestrians to come to the bridge and cross it, they need to feel safe. This starts with the surrounding area. (18)
- Unique and pleasing design is more important than creating an architectural showpiece. (10)
- Green space to encourage revitalization of the neighbourhood. (5)

### **Reason for response to survey**

At the end of the survey, respondents were asked why they took the time to respond, and 341 responses were received. A wide array of feedback was shared. The most commonly shared responses related to:

- The bridge is important to help improve the community and the city as a whole. (53)

- The project will affect those who regularly and frequently use the bridge to commute to work and/or other areas of the city. (49)
- Appreciated an opportunity to have a say in the project. (48)
- Concerned about how the project will affect residents nearby. (15)
- Concerned about how taxes were being used. (12)

## 5.2 Events

Key themes that emerged at public events are summarized, by event, in this section. It is important to note that participants offered a wide array of perspectives and opinions, and the key themes presented are not intended to reflect consensus view points.

**Land use forum:** Safety is key – lighting, sightlines on and under the bridge; tunnel at Dufferin Avenue not safe; Logan Avenue corner good opportunity for mixed development; arts- and culture-related and recreational development favoured north of bridge.

**Property owners forum:** Specific questions from affected property owners were addressed; no strong opposition noted.

**Construction and connections forum:** Access to Health Sciences Centre, North Centennial Recreation and Leisure Facility essential; desire to maintain bus route/stop at North Centennial Recreation Centre; strong desire by businesses to see vehicle access maintained on Arlington Street during construction; tunnel at Dufferin Avenue not supported; pedestrian ramps well received; safe active transportation crossing at Stella Avenue and/or Flora Avenue desired.

**Meet Me at the Bell Tower:** Uniqueness of bridge was very important; arts and cultural and community uses were most desired land uses, followed by landscaping and improvements to attract/support businesses; skyline views was most desired feature; favoured themes for public art were area history and missing and murdered Indigenous women.



**Bear Clan Walk:** Route included inspection under Salter Bridge and Arlington Bridge. Feedback included need for bright lighting around/under bridge; tunnel at Dufferin Avenue not supported; open sightlines under bridge desired.



**Dufferin Residents AGM:** Presentation was given by Stantec and discussion and questions focused on: project timing, how need for housing in the area could be accommodated; number of lanes and plans to safely accommodate cyclists. There was strong desire for the new bridge, and the suggestion was made that a new bridge might also receive a new name.

**Faculty of Social Work on Selkirk Avenue:** Tunnel at Dufferin Avenue not supported; concern about traffic accommodation during construction, particularly for Pinkham School. Desire for arts and cultural and community uses north of bridge, and desire to establish connections between existing green spaces. Safety is key – clear sightlines and security cameras suggested.

**Microbiology Lab:** Plans for traffic accommodation during construction were of concern; suggestions made for Logan Avenue, particularly Pinkham School. Concern over loss of on-street parking on Arlington Street, suggestions for parking lot development in adjacent lands (Tecumseh at William) . Suggestion for pedestrian crossing on Arlington Street between William Avenue and Logan Avenue.

**Access Advisory Committee:** Intersection design should reduce conflicts between pedestrians, cyclists and vehicles – suggested raising cycle path to height of curb with rumble strips to avoid challenges for those who are visually impaired or in wheelchairs; tunnel at Dufferin Avenue not supported; desire for snow clearing on pedestrian ramps from bridge; suggested signalized pedestrian crossing of Arlington at Stella Avenue.

**Stakeholder outreach on property impacts and design changes at Dufferin Avenue:** No significant opposition to anticipated property impacts were received; minor concern over potential closure of Dufferin Avenue to vehicle traffic; tunnel at Dufferin Avenue was not supported; re-designed crossing at Dufferin Avenue, open to vehicles and pedestrians, was well-received.

**February 2018 information session:** From feedback forms (in-person and online) , the majority of people (73%) found the information they were looking for. The majority of people who filled out the exit survey said they were there to see the bridge design (75%) , with nearly as many

coming for information on construction and timing (68%) . Following that, people were interested in vehicle traffic improvements/changes (61%) , improvements for cyclists and pedestrians (50%) , and the use of open land (39%) . Twelve people expressed that they specifically wanted to see how public input was incorporated. Additional questions and comments or concerns included concerns over number of lanes; project expense; questions about community space use; pedestrian and cyclist crossing concerns; and intersection use.

A few ideas were shared for improving project information and public engagement – and they also included recognition of a good job to date. Ideas included exit packages for sharing with neighbours, a more user-friendly website and presentations at different times of day. Social media was the most mentioned by attendees as the place they heard about the event, followed closely by media coverage, email, and the City website.

### 5.3 Project Advisory Committee

The PAC was involved in review and feedback at every key juncture of the bridge project. The following is a summary of key feedback received from the committee that have helped shape the project, design, and the public engagement process, and is not intended as an exhaustive list of feedback received.

#### **Project vision, goals, design values, and criteria**

The committee was asked to confirm the vision and goals that would guide the project. These were based on the vision and goals originally developed with committee input in a 2014 CP yards crossing study.

Vision: A safe, convenient and well-situated crossing(s) that connects the north and south communities and supports social interaction, healthy lives, economic stability and growth, and well-managed traffic flow, with accessible and connected transportation options for all ages and abilities.

#### Goals:

- To be technically sound.
- To be environmentally responsible.
- To be cost effective.
- To reflect needs of the local community as well as the city in general.
- To be generally understood and accepted by most of those affected.

#### Design values and criteria:

The committee confirmed these remained valid and important guideposts for the project.

Additional early feedback focused on suggested design values and criteria:

- **Year-round use:** Winnipeg has four distinct seasons – the Arlington Bridge corridor should work well at all times of year.
- **All modes of travel treated equally:** The bridge is not only an important connector for vehicle commuters, but also for local residents travelling on foot or by bike. Choices to facilitate vehicle access should be balanced with equal consideration of pedestrian and cycling traffic.
- **Transformative investment:** An investment of this size should be transformative, improve the experience and function for all bridge users, and consider benefits to the local community (e.g., use of open land) .
- **Statement to the city:** The current Arlington Bridge is iconic; a new bridge should make a positive statement and addition to Winnipeg’s skyline.

Important design evaluation criteria highlighted as important by the committee included life-cycle cost, safety, impact on community and businesses, accommodating cyclists and pedestrians, impact on property and aesthetics.

These suggestions and themes were incorporated into project materials.

#### Public engagement

The committee was asked for input on the public engagement process itself, to ensure the engagement plan would reflect the needs of community members most directly impacted by the project in the neighbourhoods north and south of the bridge. Highlights of the feedback received is summarized below:

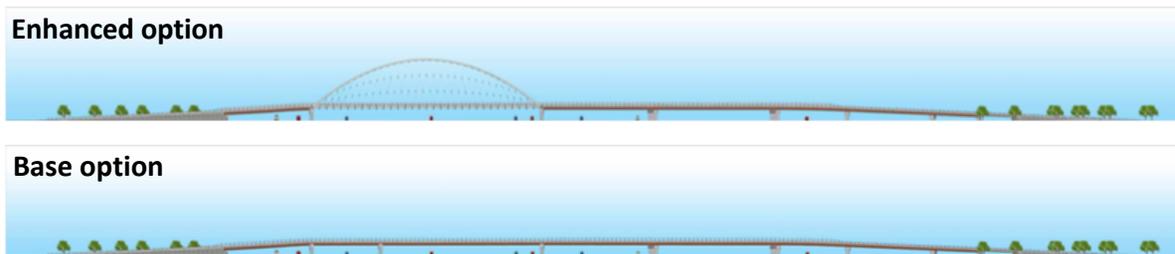
- This is an important connector in this area; clearly communicate why a new bridge is needed, the challenges and shortcomings of the current bridge, and ensure Winnipeggers understand what is happening on this project and why.
- Ensure there is broad awareness of the project and that engagement is accessible, easy to find, and concise.
- Use social media, advertising, and media coverage to create awareness with all Winnipeggers.
- Use signs/posters on the bridge and mailings to reach commuters and people in local neighbourhoods.
- Work with area schools to reach youth.
- Work with local partners, networks, agencies, and resource coordinators to reach people who may be harder to engage in the neighbourhoods north and south of the bridge.

This feedback and many other specific ideas and suggestions were directly incorporated into the engagement process. For example, the committee reviewed and provided feedback on videos, promotional materials, survey questions, event dates and locations, community partners and dates, and many other specific suggestions that were incorporated into the public engagement plan.

### Bridge and corridor design

At the conclusion of the project, the committee had an opportunity to review the final bridge concepts in detail – the arch concept and the girder concept – and how stakeholder and public input had been incorporated into the design. Specific feedback received from the committee on key aspects of the design is summarized below.

### Bridge concepts



Feedback on the two bridge concepts (Enhanced Option/arch bridge and Base Option/girder bridge), which are included in the final preliminary design report that will be provided to Council, included:

- Both designs responded to public feedback by including key safety features, lookouts, and opportunities to incorporate public art.
- Support for both concepts being presented to Council for consideration.
- Support for recommending the Enhanced Option in the final report.
- The Base Option would require further development in order to meet the desire for an iconic structure and addition to the Winnipeg skyline, as identified from public consultation.
- The majority of committee members preferred the Enhanced Option based on aesthetics, input from the community and comparative ease of construction, with less impact on CP Rail and less potential for delays/increased cost of construction over the Base Option.

### Safety and land use at Dufferin Avenue

For Dufferin Avenue under the Arlington Bridge, the committee felt the proposed design successfully addressed community concerns about safety at that location, and a desire to see community uses of open lands at that location.



**Dufferin looking North**

### Active transportation and transit

Bus stop locations and design to accommodate all users along Arlington Street was well received by members.

Members felt that the project improved upon the City's cycling network by providing dedicated pedestrian and cycling routes, facilities and connections to existing corridors, and to major recreational facilities.

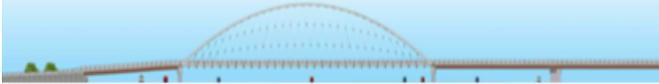


**Dufferin looking Northwest**

The feasibility of a half-signal crossing at Alexander Avenue was suggested for assessment during the next phase of the project (detailed design).

## 6.0 Conclusion

Thousands of Winnipeggers took time to learn more about the Better Bridge for Arlington project, and to share their views. The project team worked with stakeholders and the public throughout the project to ensure input received was considered and meaningfully addressed wherever possible. The following summarizes the key themes that were heard through engagement events, pop-ups, meetings, and the survey, and how the project team was able to use it in the preliminary design or why a suggestion was not incorporated.

What we heard	How feedback was incorporated*
<p>Project Advisory Committee advised both the Enhanced Option (based on aesthetics, comparative ease of construction, less impact on rail yard activity, and less potential for delays/increased cost of construction over the girder concept) and Base Option (with further development in order to meet the desire for an iconic structure and addition to the Winnipeg skyline) be presented to City Council for consideration.</p> <p><b>Enhanced Option</b></p>  <p><b>Base Option</b></p> 	<p>Both proposed options provide identical functionality with three permanent traffic lanes (two northbound and one southbound), two unidirectional bike lanes, and two sidewalks. Both options allow the bridge to be reconfigured into four traffic lanes, which is only anticipated for temporary situations when major reconstruction work is required on adjacent crossings of the CP Yards.</p> <p>The only difference between the Base and Enhanced options is that the Enhanced option replaces two spans of typical structural steel bridge girders with a single span steel arch. The steel arch offers a dramatic skyline feature offering additional aesthetic value.</p> <p>The Public Service is recommending the proposed Base Option be approved for further consideration and prioritized amongst other unfunded major capital projects through the development of a City Infrastructure Plan. This is based on a significant budget requirement difference of \$24.1 Million from the Base to Enhanced option.</p>
<p>Safety is very important. People need to feel safe and secure both on and around the bridge. Better lighting is essential.</p>	<ul style="list-style-type: none"> <li>• Better lighting and sightlines on, under, and around the bridge and Arlington Street.</li> <li>• Protected bike lanes and sidewalks.</li> <li>• Intersections designed to minimize potential for conflicts between people walking, cycling, and driving.</li> </ul>
<p>The bridge should be a vibrant addition to the city skyline that makes people feel proud. More than 70 percent of respondents to the Better Bridge for Arlington survey said it was important that the new bridge add to the skyline and make people feel proud.</p>	<ul style="list-style-type: none"> <li>• While the Enhanced Option directly responds to public feedback that the proposed bridge design should be a distinct addition to the Winnipeg skyline, it is not part of the Public Service's recommendation due to the additional budget required.</li> <li>• Both the base option and enhanced option respond to public feedback by including features like rest stops, lookouts and public art, and attractive lighting along the bridge sidewalks.</li> <li>• Attractive landscape, streetscapes, and boulevards on Arlington Street.</li> </ul>

	<ul style="list-style-type: none"> <li>Plans to incorporate public art that draws on neighbourhood culture, history.</li> <li>Planning for community use spaces around the bridge.</li> </ul>
The bridge should be easier to cross for all users.	<ul style="list-style-type: none"> <li>Less steep than current bridge from 7.1 percent grade to 5 percent grade, with rest areas.</li> <li>Access for emergency vehicles and other large vehicles .</li> <li>Protected bike lanes and sidewalks.</li> </ul>
Improve traffic flow. There was a strong desire for improved travel times and traffic flow, particularly during rush hour, to and from Health Sciences Centre and other key destinations.	<ul style="list-style-type: none"> <li>Intersection and travel lane improvements, including dedicated turning lanes.</li> <li>Significant traffic capacity increases at Logan Avenue and Arlington Street.</li> </ul>
Maintain access during construction. Business owners and bridge users requested bridge and road closures to be minimized as much as possible during construction.	<ul style="list-style-type: none"> <li>On-street traffic flow to be maintained with one lane in each direction or in a single unrestricted lane to lessen impact on users and maintain access to businesses.</li> <li>Bridge to remain open to vehicles for two thirds of the construction period (all but one construction season) ; pedestrians and cyclists may be able to use the bridge even longer.</li> </ul>
Closing Dufferin Avenue to vehicles and installing a pedestrian tunnel was not preferred. There was strong opposition to a proposed pedestrian tunnel at Dufferin Avenue for safety reasons.	<ul style="list-style-type: none"> <li>Plans for a pedestrian tunnel were scrapped in favour of keeping Dufferin Avenue open to all traffic, so vehicles, pedestrians, and cyclists are able use the underpass effectively.</li> <li>Improvements to sightlines, lighting, and other safety features were added to discourage gathering under the bridge.</li> </ul>
<b>What we heard</b>	<b>Why feedback was not incorporated*</b>
Create affordable housing on open land around the bridge. Residents told us there is a need for affordable housing in the area. This has also been documented in area housing plans. Many people indicated they would like to see surplus land from this project turned into housing.	<ul style="list-style-type: none"> <li>The Federation of Canadian Municipalities recommends a 300-metre setback for housing development projects located next to rail yards. This guideline makes housing or mixed-use development unlikely within the developable parcels. To construct housing on these properties, prohibitively expensive crash walls would need to be installed.</li> </ul>
Preserve and repurpose the bridge for another use. Participants suggested the current bridge be preserved, and reused for something (e.g., green space, or a cycling and pedestrian bridge).	<ul style="list-style-type: none"> <li>Although there are examples of other bridges that have been repurposed as pedestrian and cycling routes, those bridges are typically old railway structures that are stronger and in better condition than the Arlington Bridge. The condition of the Arlington Bridge means it cannot be preserved and reused. Very few components of the bridge are still in usable condition and are not up to today's design and accessibility standards. As a result, the Arlington Bridge cannot be preserved and reused. However, there was also significant public interest in reusing parts of the bridge for public art or sculpture, an idea to be considered as part of a process managed by the Winnipeg Arts Council.</li> </ul>

Change the name of the bridge or take suggestions on a new name.	<ul style="list-style-type: none"> <li>• Not addressed in preliminary design, but an idea that could be considered as part of detailed design.</li> </ul>
Conduct a ceremony/blessing before new bridge is constructed.	<ul style="list-style-type: none"> <li>• Not addressed in preliminary design, but an idea that will be considered as part of detailed design.</li> </ul>

\*The preliminary design is currently being finalized and may be subject to change in the next steps in the process.

The results presented in this public engagement report have helped shape the design of a Better Bridge for Arlington. The report with the recommendations for the preliminary design of the bridge will be presented in a final report to Council.

The Public Service is recommending the Base Option be approved for further consideration and prioritized amongst other unfunded major capital projects through the development of a City Infrastructure Plan. This is based on a significant budget requirement difference of \$24.1 million between the Base Option and the Enhanced Option. The Class 3 Estimate (-20% to +30%) includes \$34.6 million in financing, assuming that the City will solely fund the project and utilize 100 percent debt financing.

**Appendix A – Project Advisory Committee meeting summaries**



## Project Advisory Committee meeting – Let’s get (re)started!

Wednesday, April 5, 2017

### Attendance

Organization	Representative	Role	Attendance
Bike Winnipeg	Mark Cohoe	Executive Director	Yes
City of Winnipeg Access Advisory Committee	Ken Shachtay	Member	Yes
Health Sciences Centre/Winnipeg Regional Health Authority	Ken Kollinger	Regional Director of Capital Planning, WRHA	Regrets
King Edward Community School	Aaron Benarroch	Principal	Regrets
Rossbrook House	Phil Chiappetta	Executive Director	Yes
Dufferin Residents Association of Winnipeg/William Whyte Residents Association	Jessie Leigh	Community Development Coordinator	Regrets
Transportation Options Network for Seniors	Pam McConnell	Coordinator	Yes
Ndinawe	Dustyne Lefurgey	Programs Manager	Yes
Dufferin School	Wayne Wyke	Principal	Yes
Centennial Residents’ Association	Gord Dong	Resident	Yes
Winnipeg Regional Health Authority	Claire Meiklejohn	Community Facilitator, Downtown Community Area	Yes
North End Community Renewal Corporation	Richard Gilbert	Manager, Citizens’ Bridge	Yes
Winnipeg Arts Council	Tricia Wasney	Manager, Public Art	Yes
City of Winnipeg Indigenous Relations Division	Carole Frechette	Indigenous Community Projects and Initiatives Assistant	Yes
City of Winnipeg	Councillor Mike Pagtakhan	City Councillor – Point Douglas Ward	Yes
City of Winnipeg	Darren Burmey	Project Manager	Yes
City of Winnipeg	Vaibhav Banthia	Deputy Project Manager	Yes
Stantec	Mike	Project Manager	Yes

	Boissonneault		
First Person Strategies	Michelle Kuly Holland	Facilitator	Yes
First Person Strategies	Lise Fenton	Note Taker	Yes

## Agenda

A meeting of the Project Advisory Committee (PAC) was held on April 5, 2017. The purpose of the meeting was to:

- Inform the Committee on the status of the project including what factors will influence the bridge design
- Discuss public engagement for the project and establish how the PAC will be involve
- Confirm the plan for the PAC moving forward

### 1. Welcome and introductions

Introductions were made, committee purpose and meeting guidelines were reviewed.

Note: *Suggestions and recommendations offered by PAC members throughout the meeting are captured in a master “feedback tracker” document (attached) that the project team will reference and respond to throughout the duration of the preliminary design phase of work.*

### 2. Project update

Stantec gave a presentation on the functional design stage of the project, as well as the current status of the project. The presentation is attached as Appendix A. Accessibility of the bridge and property impacts were discussed.

The group reviewed the project vision and goals (developed with PAC direction as part of the functional design phase of the project).

Committee feedback included:

- Non-motor vehicle traffic could be specifically mentioned to emphasize pedestrians, cyclists, and accessibility
- Be clearer about healthy lives and neighbourhoods, and mitigating impacts
- Reflect the needs and identity of the community
- Ensure environmental goals are consistent with overall climate goals external to the project

**Action item:** First Person Strategies to add suggestion that the ground be blessed at the site prior to construction to list of future project considerations.

### 3. Community engagement plan

First Person Strategies presented a draft plan to engage the community, which included a description of tactics that could be used. The presentation is attached as Appendix B. Committee members were asked to discuss the best way to use each public engagement tactic with the person next to them. The tactics were then reviewed and discussed as a group.

Key themes:

- The need to highlight the shortcoming of the current bridge, and ensure Winnipeggers understand what is happening on this project and why
- Use the web and other tactics to highlight the connection between north and south, and feature the communities near the bridge
- Ensure engagement is accessible, easy to find, and concise
- Use paid advertising including billboards, bus ads, and newspaper ads
- Work with schools and youth
- Utilize existing networks, agencies, and resource coordinators
- Draft some social media posts to help the PAC share information about the project
- Change the name of the bridge, or take suggestions on a new name

**Action item:** First Person Strategies to circulate a template for PAC members to complete and submit by April 18, 2017, including: what they do, who their stakeholders are, and how they engage with their communities and networks.

**Action item:** First Person Strategies to review specific suggestions for engagement tactics with project team, record and report back in feedback tracking document.

#### **4. "What we know": Early technical evaluation and review**

Stantec gave a presentation on the constraints of the project, as well as the criteria and weighting that will go into the technical team's work on the project. The presentation is attached as Appendix C.

The Committee was asked to take a look at the criteria, record some notes on which were most important to them, and share with the group.

Criteria highlighted as important included life-cycle cost, safety, impact to community and business, cycling and pedestrians, impact to property and aesthetics.

#### **5. Session closing and next steps**

Meeting notes and agenda will be circulated prior to the next meeting on Thursday, April 20.



## Project Advisory Committee meeting – What’s in a bridge?

Thursday, April 20, 2017

### Attendance

Organization	Representative	Role	Attendance
Bike Winnipeg	Mark Cohoe	Executive Director	Yes
City of Winnipeg Access Advisory Committee	Ken Shachtay	Member	Yes
Health Sciences Centre/Winnipeg Regional Health Authority	Ken Kollinger	Regional Director of Capital Planning, WRHA	Yes
King Edward Community School	Aaron Benarroch	Principal	Yes
Rossbrook House	Phil Chiappetta	Executive Director	Yes
Dufferin Residents Association of Winnipeg/William Whyte Residents Association	Jessie Leigh	Community Development Coordinator	Regrets
Transportation Options Network for Seniors	Pam McConnell	Coordinator	Regrets
Ndinawe	Dustyne Lefurgey	Programs Manager	Yes
Dufferin School	Wayne Wyke	Principal	Regrets
Centennial Residents’ Association	Gord Dong	Resident	Yes
Winnipeg Regional Health Authority	Claire Meiklejohn	Community Facilitator, Downtown Community Area	Yes
North End Community Renewal Corporation	Richard Gilbert	Manager, Citizens’ Bridge	Yes
Winnipeg Arts Council	Tricia Wasney	Manager, Public Art	Yes
City of Winnipeg Indigenous Relations Division	Carole Frechette	Indigenous Community Projects and Initiatives Assistant	Yes
City of Winnipeg	Councillor Mike Pagtakhan	City Councillor – Point Douglas Ward	Yes
City of Winnipeg	Darren Burmey	Project Manager	Yes
Stantec	Mike Boissonneault	Project Manager	Yes

Stantec	Rebecca Hewett	Transportation Engineer	Yes
HTFC	Glen Manning	Principal	Yes
Cibinel	Michael Robertson	Partner	Yes
Winnipeg Arts Council	Carol Phillips	Executive Director	Yes
First Person Strategies	Michelle Kuly Holland	Facilitator	Yes
First Person Strategies	Lise Fenton	Note Taker	Yes

## Agenda

A meeting of the Project Advisory Committee (PAC) was held on April 20, 2017. The purpose of the meeting was to:

- Discuss and gather input on bridge corridor, design, landings and public art
- Identify, record list of community needs, important design values

### 1. Welcome and introductions

Introductions were made, committee purpose and meeting guidelines were reviewed.

Note: *Suggestions and recommendations offered by PAC members throughout the meeting are captured in a master “feedback tracker” document that the project team will reference and respond to throughout the duration of the preliminary design phase of work.*

### 2. Committee business

First Person Strategies reviewed some outstanding committee business. Committee members were asked to give permission for some information as well as photos and videos of PAC meetings to be shared as part of public engagement for the project.

The Committee agreed:

- Committee Terms of Reference to be adopted for the duration of the preliminary design phase
- Meeting notes and tracking document adopted
- Materials for the Committee to be shared through Dropbox
- The Committee was told about efforts to date to include additional perspectives in public engagement for the project. These perspectives were identified by current and former PAC members, as well as the project team, and include indigenous and local business perspectives and additional residents organizations. It was agreed that, given that two meetings have now been completed, the focus on engaging these groups should shift away from the PAC, and towards other tactics such as targeted meetings

**Action Item:** First Person Strategies to continue work to connect with additional stakeholders, identify opportunities to engage, and report back to the Committee at the next meeting.

The Committee was also informed of the status of the project webpage, which went live that day.

**Action Item:** Committee members to review website at <http://www.winnipeg.ca/publicworks/construction/projects/arlingtonBridge.stm>, identify any gaps in information, and send to FPS.

### **3. Bridge project opportunities and community needs**

The Committee received four presentations on key elements of the project where there are opportunities for input:

- The bridge corridor, access and movement for users, construction phasing (Stantec)
- Bridge landings, community use (HTFC)
- Bridge architecture, design (Cibinel)
- Public art (Winnipeg Arts Council)

These presentations are attached as appendix A, B, C and D, respectively.

Following the presentations, the Committee was split into four discussion groups and the group then came back together to debrief.

The goal of these discussions was to generate a list of values or principles, to be reviewed and refined via community consultation led by First Person Strategies and PAC members, and used by the project team to inform the design process. A document including the outcomes of these discussions will follow.

**Action Item:** First Person Strategies to circulate values/principles document along with next steps to confirm with networks.

### **4. Session closing and next steps**

Meeting notes and agenda will be circulated prior to the next meeting, which will likely be held in late May or June – date TBD.



## **PD Decision Matrix – Input from the Project Advisory Committee**

Last updated May 26, 2017

*This document captures discussion in meetings 1 & 2 of the Project Advisory Committee (PAC) during the preliminary design phase, and is intended to highlight key criteria identified by the PAC, and their understanding of, and input on, those criteria. In some cases PAC feedback included modifying or moving criteria to a different category - those suggestions are reflected in this document. This document also includes a first draft of some core values that are emerging as the engagement process moves forward.*

### **Financial criteria**

#### **Lifecycle cost**

- Durability - last as long as possible
- Minimize caretaking

#### **Capital cost**

- Getting the right land use (at the ends of the bridge) is much more important than maintaining City ownership of those lands

### **Environmental criteria**

#### **Aesthetic**

- Unique
- Statement to the city from the community
- Take advantage of functional features (e.g. rest areas)
- Views to the bridge
- Views from the bridge
- The experience of the bridge for everyone – 2 minutes in a car, 5 minutes on a bike, 10 minutes on foot
- Compliment, don't compete
- Durability
- 4 season

#### **Landscape aesthetic**

- Lots of existing greenspace
- There is an opportunity in the northeast corner of the bridge

### **Environmental impacts**

- Ensure environmental goals are consistent with overall climate goals external to the project

## **Social criteria**

### **Impact to North End community (formerly "Impact to the community")**

- Distinct opportunity/benefit re: housing (resident population)
- Use bridge to spur investment
- Avoid perception/reality of 'taking from the north to give to the south'

### **Impact to South End community (formerly "Impact to the community")**

- Distinct opportunity/benefit re: commercial connector/stimulator
- 9 to 5 crowd (e.g. Virology lab, Giant Tiger is a big draw down Logan to McPhillips)

### **Impact to businesses**

- North of the bridge businesses see primarily local traffic (walking, biking, etc.)
- Through traffic/commuters not viewed as significant economic driver currently (but perhaps potential in future)
- Don't want gentrification
- Attract arts community

### **Transit services**

- YES!

### **Vehicular traffic services**

- Access to East-West network (especially McPhillips)
- Access to community destinations (e.g. community centres)

### **Impact to properties**

- Be sensitive to property concerns/takings, but don't minimize for the sake of minimizing – think through opportunities
- Think bigger, take more if it can be transformative for the community (e.g. housing north of bridge, Dufferin closure is opportunity)

### **Safety**

- Under the bridge, over the bridge, at the ends of the bridge
- For everyone, recognizing different experiences and perspectives of users
- A vibrant, well used bridge will be safer
- Year round
- Lighting

### **Pedestrian and cyclist transportation**

- Connect to existing and planned networks
- Connect to key destinations (e.g. community centres, Giant Tiger)
- Good active transportation infrastructure can serve as an incentive for walking, cycling, local traffic
- Dufferin – pedestrian and cyclist access a priority over vehicular access, but not if safety is an issue

## **Emerging core values \*draft\***

*First draft prepared by FPS based on discussion and key themes at PAC meetings. Still to be tested, refined and confirmed before being finalized – this should include input from youth, who make up a large proportion of the people living near the bridge, and who the PAC have identified as key to the project's success.*

### **Year round use**

- Winnipeg has four seasons – wind, snow, drastic changes in temperature, etc. are a reality
- The corridor function and design should work at all times of year

### **All modes treated equally**

- Use of bridge important connector for commuters (in vehicles, pedestrians, cyclists)
- Use of corridor essential for local traffic using services, community places and businesses, which is primarily walking, cycling
- Choices to facilitate vehicle access should be balanced with equal consideration of pedestrian and cycling traffic (there or elsewhere)

### **Transform, activate**

- An investment of this size should be transformative, and the community wants that (e.g. importance of land use)
- The south end of the bridge and north end of the bridge offer different opportunities (South – commercial, 9-5 focused; North – housing, local resident-focused)
- Transform, improve the experience and function of the bridge for all users

### **Statement to the city**

- The bridge design, aesthetic should be a statement to, and for, the City – from, of the community
- We may typically think of bridge experience as North to South commute – the PAC see an opportunity to shine a light on the community, draw the South to the North via it's aesthetic, design – reinforcing positive “Welcome to the North End”
- Sense of ownership will be key achieving this - design, aesthetic, function, gateways of the bridge should encourage ownership of the bridge, particularly by the community, and by extension by Winnipeggers at large

- There is rich history and culture in this area – a sense of ownership could be encouraged by a bridge design that makes room for many, diverse stories



## Project Advisory Committee meeting

Friday June 9, 2017

### Meeting details

Location: Burton Cummings Community Centre  
960 Arlington Street, <https://goo.gl/maps/p7zMHWsNyj62>  
Arrival: 12:45PM (lunch will be served)  
Meeting start: 1:00PM sharp  
Meeting end: 4:00PM sharp

If you have not yet confirmed attendance, please RSVP to Tamara at [tamara@firstpersonstrategies.com](mailto:tamara@firstpersonstrategies.com) or 204.287.8820.

### Draft Agenda

1. Welcome and introductions
2. Committee business
3. Project team update, including:
  - a) L –Lead Removal Update
  - b) Review of Road Design
  - c) Bridge Options
4. Presentations
  - a) Area history (Murray Peterson, Urban Design Division, City of Winnipeg)
  - b) City finances (Councillor Pagtakhan)
5. Discussion
  - a) PAC input to-date: Key themes
  - b) Pre-summer community update
  - c) Fall public awareness & engagement campaign
6. Session closing and next steps



## Arlington Project Advisory Committee Meeting #3

Friday, June 9, 2017

### Attendance

Organization	Representative	Role	Attendance
Bike Winnipeg	Mark Cohoe	Executive Director	Yes
City of Winnipeg Access Advisory Committee	Ken Shachtay	Member	Yes
Health Sciences Centre/Winnipeg Regional Health Authority	Ken Kollinger	Regional Director of Capital Planning, WRHA	Regrets
King Edward Community School	Aaron Benarroch	Principal	Regrets
Rossbrook House	Phil Chiappetta	Executive Director	Yes
Dufferin Residents Association of Winnipeg/William Whyte Residents Association	Jessie Leigh	Community Development Coordinator	Regrets
Transportation Options Network for Seniors	Pam McConnell	Coordinator	Regrets
Ndinawe	Dustyne Lefurgey	Programs Manager	Yes
Dufferin School	Wayne Wyke	Principal	Regrets
Centennial Residents' Association	Gord Dong	Resident	Regrets
North End Community Renewal Corporation	Richard Gilbert	Manager, Citizens' Bridge	Yes
Winnipeg Arts Council	Tricia Wasney	Manager, Public Art	Yes
City of Winnipeg Indigenous Relations Division	Carole Frechette	Indigenous Community Projects and Initiatives Assistant	Yes
City of Winnipeg	Councillor Mike Pagtakhan	City Councillor – Point Douglas Ward	Yes
City of Winnipeg	Darren Burmey	Project Manager	Yes
City of Winnipeg	Murray Peterson	Historian	Yes
Stantec	Mike Boissonneault	Project Manager	Yes
First Person Strategies	Michelle Kuly Holland	Facilitator	Yes
First Person Strategies	Tamara Kuly	Facilitator	Yes

### Agenda

A meeting of the Project Advisory Committee (PAC) was held on June 9, 2017. The purpose of the meeting was to:

- Inform the Committee on the status of the project and evaluation process
- Provide an area history
- Confirm pre-summer engagement materials and requirements for PAC members

- Build PAC fall campaign event/activity plan

## 1. Welcome and introductions

Introductions were made, meeting purpose and guidelines were reviewed.

2023 was confirmed as the new bridge completion date that PAC members can use in communicating with the community.

Note: *Suggestions and recommendations offered by PAC members throughout the meeting are captured in a master “feedback tracker” document (attached) that the project team will reference and respond to throughout the duration of the preliminary design phase of work.*

## 2. Committee Business

All meeting documents will be distributed via email and made available on the PAC DropBox.

Revised photo/video waivers were distributed and signed by all meeting attendees.

## 3. Project Update

The Committee received a presentation from Stantec on the project recommendations including L-Lead removal and roadway design. In addition, the bridge design decision matrix and bridge structures still under consideration were reviewed.

### Benefits of the L-Lead removal:

- shortens the bridge and reduces the build cost
- allows the old bridge to stay open during construction of the new bridge

PAC noted that L-Lead removal will not likely have any cross-impact on AT plans.

### Roadway Design:

- Detailed overview of roadway design between McDermot and Selkirk available as of June 9 were presented and discussed, including operation of intersections and benefits of three-travel lane roadway configuration.

PAC confirmed by consensus understanding and agreement with the roadway design as presented and suggested similar opportunity to understand roadway operation, benefits, impacts be made available to businesses on Arlington between McDermot and Selkirk.

**Action Item:** First Person Strategies to engage directly with businesses on Arlington to ensure all perspectives are included in the feedback.

**Action Item:** First Person Strategies to ensure information on roadway design is available on project webpage through an FAQ.

### Decision matrix and bridge structure assessment process:

- The decision matrix used to narrow the bridge structure options was outlined along with the 2 bridge structure still being assessed by the project team.

PAC confirmed by consensus an understanding of the bridge structure considerations and process presented in the meeting.

**Action Item:** First Person Strategies to engage directly with businesses on Dufferin and Selkirk to ensure all perspectives are included in the feedback.

**Action Item:** Stantec to provide PAC with details of how Arlington bridge project integrates with existing and planned active transportation routes and connections when available. First Person Strategies to ensure this information is also available on project webpage.

#### **4. Presentations**

Councillor Mike Pagtakhan provided an overview of the project evaluation process at the City and his role as the Project Management liaison for council. He noted that the Arlington Bridge project is at the top of the ranked list based on current criteria. He is currently meeting with other government stakeholders (Provincial – complete; Federal - upcoming) and that a P3 model is being sought for funding.

Murray Peterson (City Historian) provided an area history to PAC.

PAC noted that this content will be a good addition to the November 2017 community celebration event, and that indigenous peoples' history needs to be incorporated into the story.

**Action Item:** Murray Peterson to provide historic documents presented to First Person Strategies to distribute to PAC via DropBox.

#### **5. Discussions – Community Engagement**

First Person Strategies outlined emerging themes from PAC feedback that are now being validated through stakeholder engagement and will be built on in the larger engagement campaign in September 2017.

First Person Strategies presented draft creative materials for a pre-summer project update and summer PAC engagement activities. PAC provided review and comment in two working groups. Feedback on the materials was captured and will be integrated into the materials.

Engagement requirements (materials, format, due date) for each PAC member were captured for both pre-summer/summer use and fall campaign events and activities.

**Action Item:** First Person Strategies will provide custom packages of materials to PAC members for pre-summer/summer use; map fall campaign opportunities and work over the summer to confirm events.

#### **6. Session closing and next steps**

Meeting notes, updated suggestion matrix and summer engagement materials as requested will be circulated before the end of June 2017.

# Blueprint.

## Arlington Project Advisory Committee Meeting #4

Tuesday, January 23, 2018

Burton Cummings Community Centre

Organization	Representative	Role	Attendance
Bike Winnipeg	Mark Cohoe	Executive Director	Yes
City of Winnipeg Access Advisory Committee	Ken Shachtay	Member	Yes
Health Sciences Centre/Winnipeg Regional Health Authority	Ken Kollinger	Regional Director of Capital Planning, WRHA	Yes
King Edward Community School	Aaron Benarroch	Principal	Regrets
Rossbrook House	Phil Chiappetta	Executive Director	Yes
Ndinawe	Dustyne Lefurgey	Programs Manager	Regrets
Dufferin School	Wayne Wyke	Principal	Yes
Winnipeg Arts Council	Alexis Kinlock	Manager, Public Art	Yes
Centennial Residents' Association	Gord Dong	Resident	Yes
North End Community Renewal Corporation	Richard Gilbert	Manager, Citizens' Bridge	Regrets
City of Winnipeg Indigenous Relations Division	Carole Frechette	Indigenous Community Projects and Initiatives Assistant	Yes
Transportation Options Network for Seniors	Samantha Rodeck	Coordinator	Yes
City of Winnipeg	Councillor Mike Pagtakhan	City Councillor – Point Douglas Ward	Yes
City of Winnipeg	Darren Burmey	Project Manager	Yes
City of Winnipeg	Vaibhav Banthia	Deputy Project Manager	Yes
Stantec	Mike Boissonneault	Project Manager	Yes
HTFC	Glen Manning	Project Team	Yes
Blueprint	Michelle Kuly	Facilitator	Yes
Blueprint	Tamara Kuly	Note Taker	Yes

### Agenda

A meeting of the Project Advisory Committee (PAC) was held on January 23, 2018.

The purpose of the meeting was to:

- Recap the Fall Engagement Campaign
- Review bridge design and how public input was used

# Blueprint.

- Thank members of the PAC for their contributions.

## **1. Welcome and Introductions**

Introductions were made, and meeting purpose and guidelines were reviewed.

## **2. Fall Engagement Campaign Recap**

Members were briefed on what was done, who participated and what was heard during the fall engagement campaign.

## **3. Final Design and How Public Input was Used**

The following timeline updates were provided to the PAC:

- The Arlington Bridge project is ranking high on the City decision matrix for city projects.
- The schedule has been delayed by one year (funding 2019; completion 2024)
- The current bridge will remain open as long as possible, with decommissioning planned for 2024. The bridge is under constant maintenance, with a risk of closure due to age and condition of the structure.

Members of the PAC and the project team reviewed and discussed the bridge and Arlington corridor design, and how public input was considered, in detail. Main discussion points have been captured below.

### Bridge design concepts

- Preference is for the arch concept based on aesthetics, input from the community and comparative ease of construction, with less impact on CP and less potential for delays/increased cost of construction over girder concept
- The committee supports recommending the arch design concept in the final report (unanimously supported).
- Both the arch and girder concepts should go before City Council (unanimously supported).
- The girder concept requires further development in order to meet the desire for an iconic structure and addition to the Winnipeg skyline, as identified from public consultation (unanimously supported).
- Discussion surrounded finding innovative ways to arrive at a design for an concept that would not result in a \$30 million cost disparity between the two proposed designs.

It was suggested that concepts should be developed, for example, a girder bridge with a 10% premium for additional lighting/art for improved aesthetics

Adjustments should be made to reduce the premium for the arch bridge design to 5%

- The potential need for an extra pier in the girder bridge design should be included in the decision matrix

# Blueprint.

## Land Use and Changes at Dufferin

- Members felt the design elements (e.g. Dufferin crossing, land use, etc.) would be embraced by the community
  - Changes at Dufferin were well-received by the committee, specifically, eliminating the tunnel and maintaining access to North Centennial Leisure Centre. The changes integrated feedback received from the community during the engagement phase of the project. The feasibility of provision of a bike lane under the bridge on Dufferin was suggested and will be assessed during detailed design.
  - North and South open area land development were supported by PAC. The development potentials for lands at the base of the bridge were well received.

## Active Transportation and Transit

- Members felt that the project improved upon the City's cycling network by providing dedicated pedestrian and cycling routes, facilities and connections to existing corridors, and to major recreational facilities.
- The feasibility of a half signal crossing at Alexander (like at Osborne and Assiniboine) was suggested and will be assessed during the detailed design phase of the project.
- The provision of unconstrained bus stops along Arlington was also well received.

## Public Art

- Integrating artists' plans into the design of the bridge as early as possible can help reduce the overall cost of public art. Art will be integrated as early as possible in next project phase.

## **5. Thank You to PAC**

Members of the PAC were recognized for their efforts on the project.

## **6. Next Steps and Session Closing**

Action items were recorded, and details of the Feb. 8 public information session were shared.

## Summary of action items

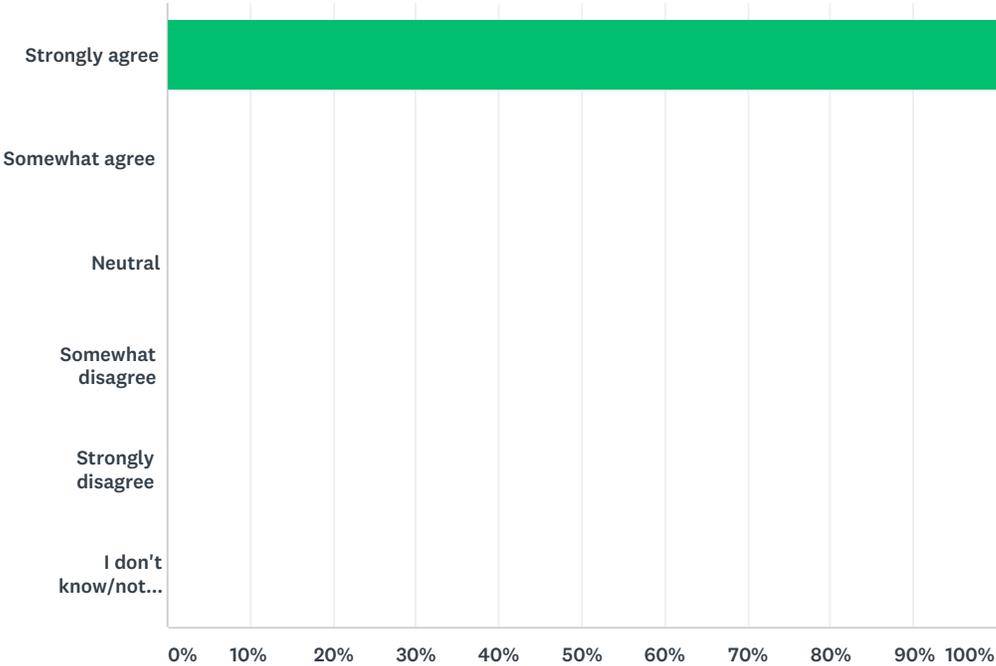
- M. Boissionneault to provide info on what section of Logan will be closed during construction to MK to distribute to the PAC members.
- T. Kuly to send calendar invitation to PAC for Feb 8 event.
- PAC to complete engagement survey via link from M. Kuly.

Meeting was adjourned.

PAC Engagement Evaluation Survey

Q1 I understood my role on the PAC and what I was there to do.

Answered: 8 Skipped: 0

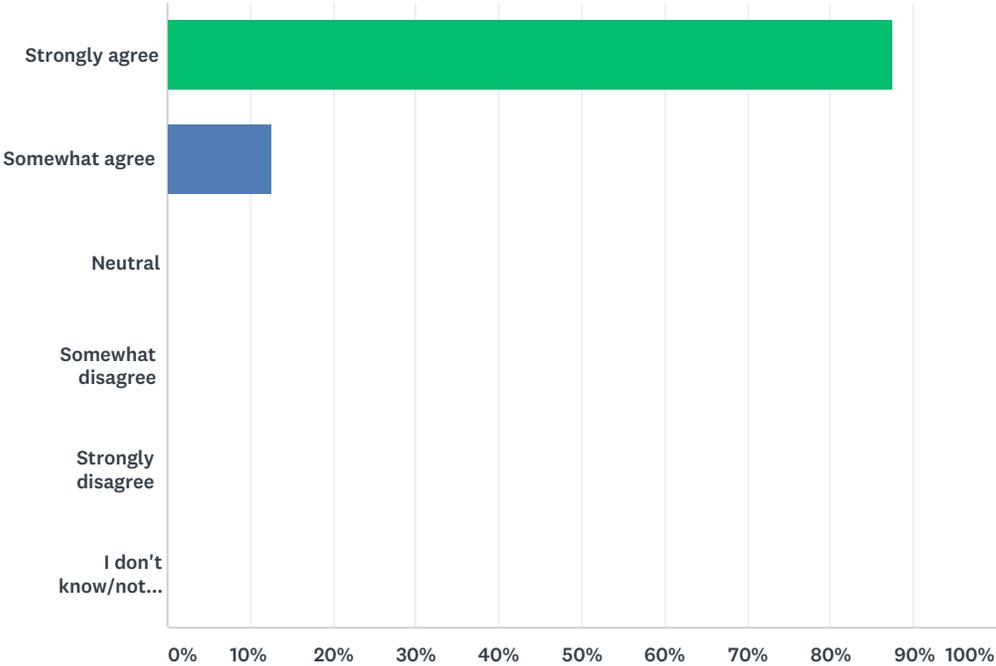


ANSWER CHOICES	RESPONSES	
Strongly agree	100.00%	8
Somewhat agree	0.00%	0
Neutral	0.00%	0
Somewhat disagree	0.00%	0
Strongly disagree	0.00%	0
I don't know/not applicable	0.00%	0
<b>TOTAL</b>		<b>8</b>

PAC Engagement Evaluation Survey

Q2 I felt like my opinion was respected and important.

Answered: 8 Skipped: 0

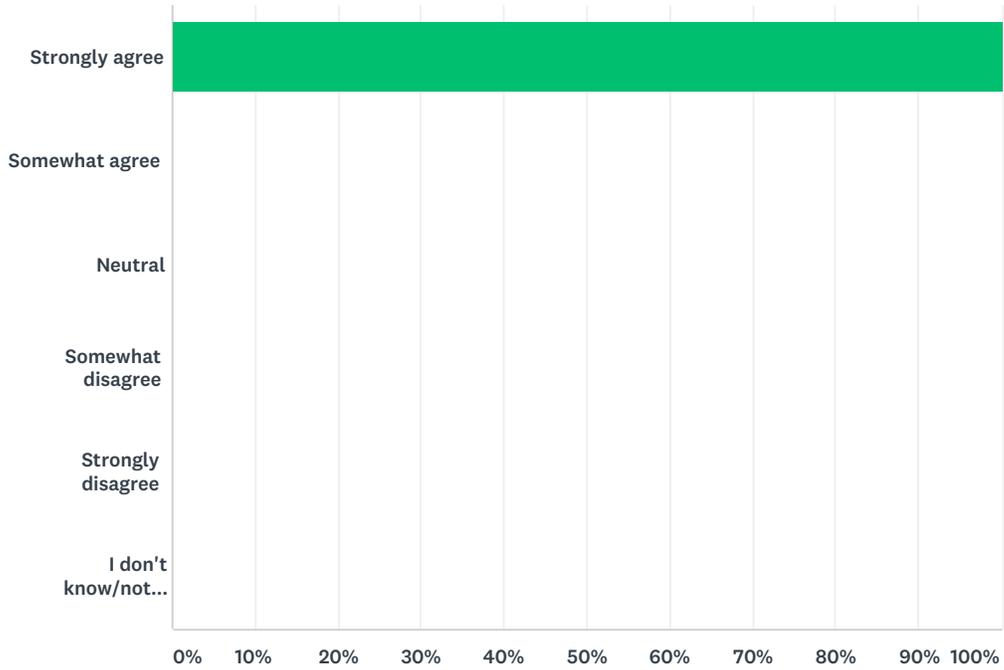


ANSWER CHOICES	RESPONSES	
Strongly agree	87.50%	7
Somewhat agree	12.50%	1
Neutral	0.00%	0
Somewhat disagree	0.00%	0
Strongly disagree	0.00%	0
I don't know/not applicable	0.00%	0
<b>TOTAL</b>		<b>8</b>

PAC Engagement Evaluation Survey

Q3 The project information provided was easy to understand.

Answered: 8 Skipped: 0

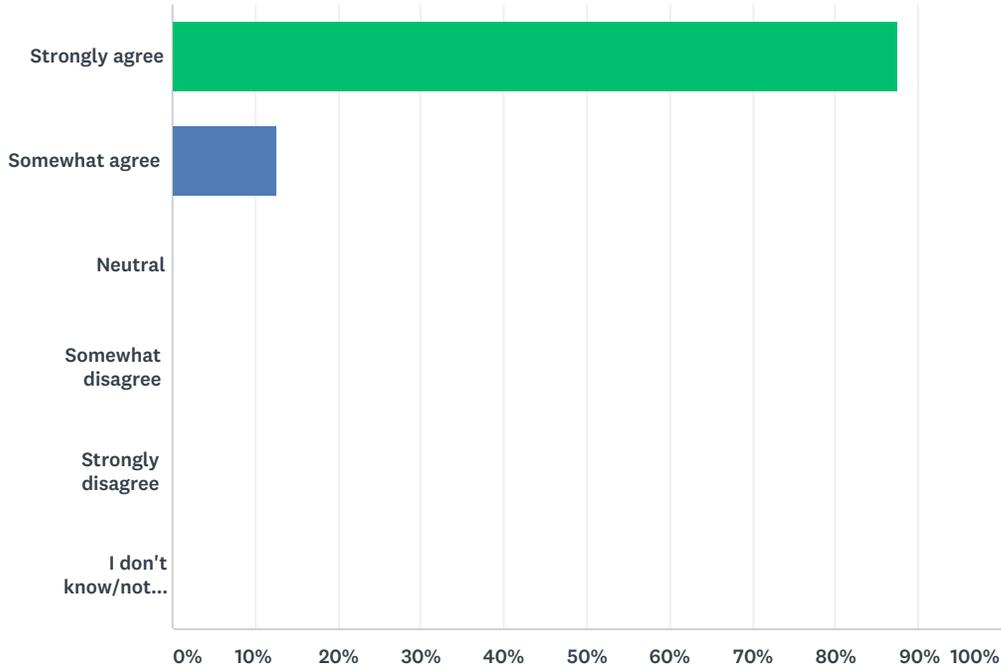


ANSWER CHOICES	RESPONSES	
Strongly agree	100.00%	8
Somewhat agree	0.00%	0
Neutral	0.00%	0
Somewhat disagree	0.00%	0
Strongly disagree	0.00%	0
I don't know/not applicable	0.00%	0
<b>TOTAL</b>		<b>8</b>

PAC Engagement Evaluation Survey

Q4 The communication of information (including events) increased awareness about the project for the people it impacted most.

Answered: 8 Skipped: 0

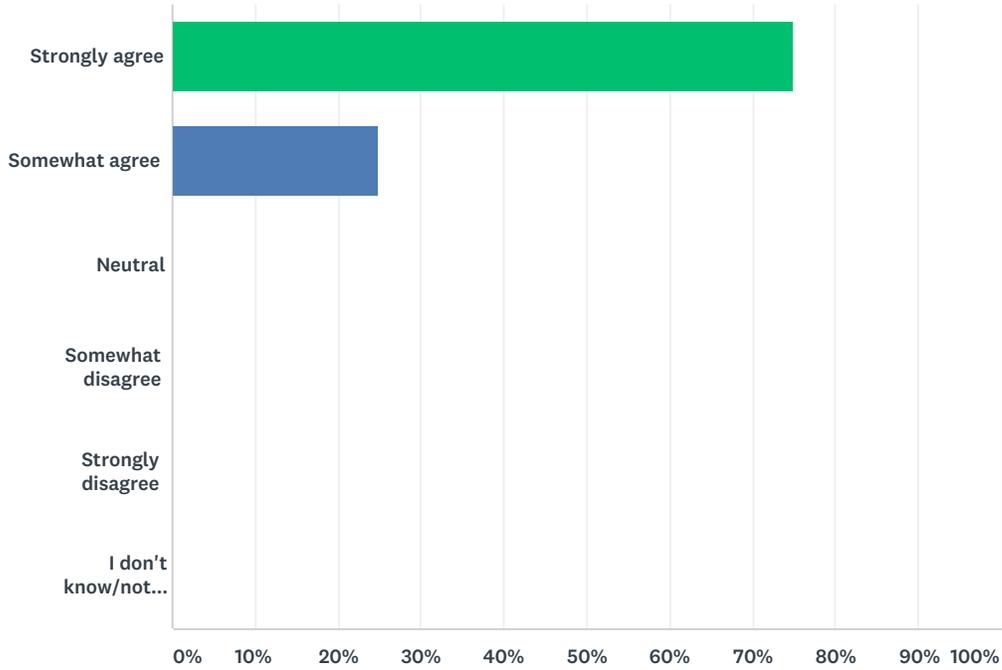


ANSWER CHOICES	RESPONSES	
Strongly agree	87.50%	7
Somewhat agree	12.50%	1
Neutral	0.00%	0
Somewhat disagree	0.00%	0
Strongly disagree	0.00%	0
I don't know/not applicable	0.00%	0
<b>TOTAL</b>		<b>8</b>

PAC Engagement Evaluation Survey

Q5 Input from the PAC and the public made a difference in decisions that were made about the project.

Answered: 8 Skipped: 0



ANSWER CHOICES	RESPONSES	
Strongly agree	75.00%	6
Somewhat agree	25.00%	2
Neutral	0.00%	0
Somewhat disagree	0.00%	0
Strongly disagree	0.00%	0
I don't know/not applicable	0.00%	0
<b>TOTAL</b>		<b>8</b>

PAC Engagement Evaluation Survey

**Q6 Do you have any comments about the questions, or other information you would like to share with us about your experiences as a PAC member?**

Answered: 4 Skipped: 4

#	RESPONSES	DATE
1	The bridge models were a fantastic conclusion to this engagement. The facilities and engagement were well selected.	2/23/2018 9:31 AM
2	It was an honour to be part of this team! Excellent work on coordinating and keeping everyone on task!	2/23/2018 8:41 AM
3	Meetings were focused and time frames respected. Much was accomplished in a relatively short period.	2/22/2018 4:57 PM
4	I enjoyed working with the group and the facilitators were terrific!	2/22/2018 3:55 PM

## **Appendix B – Engagement materials**



# A BETTER BRIDGE FOR ARLINGTON

## WELCOME

### THANK YOU FOR ATTENDING TODAY'S INFORMATION SESSION

The following materials and resources are available to you:



A large-scale map and model, offering a view of the entire project.



Information boards with details of the project, such as traffic improvements, construction timelines and bridge features.



Videos demonstrating how input from community members and the public was used in the design.



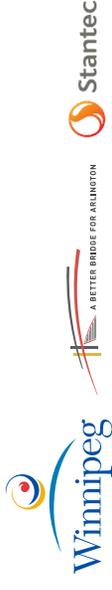
Knowledgeable project representatives who will be happy to answer questions about any element of the project.

Materials shared today are also available at [winnipeg.ca/betterarlington](http://winnipeg.ca/betterarlington)



# 2

A BETTER BRIDGE FOR ARLINGTON



## VISION STATEMENT

“A safe, convenient and well-situated crossing that connects the north and south communities, supports social interaction, healthy lives, economic stability and growth and well-managed traffic flow and offers accessible, connected transportation options for all ages and abilities.”

The vision for a Better Bridge for Arlington was developed by the project team with its community partners, the Project Advisory Committee (PAC).

### PAC

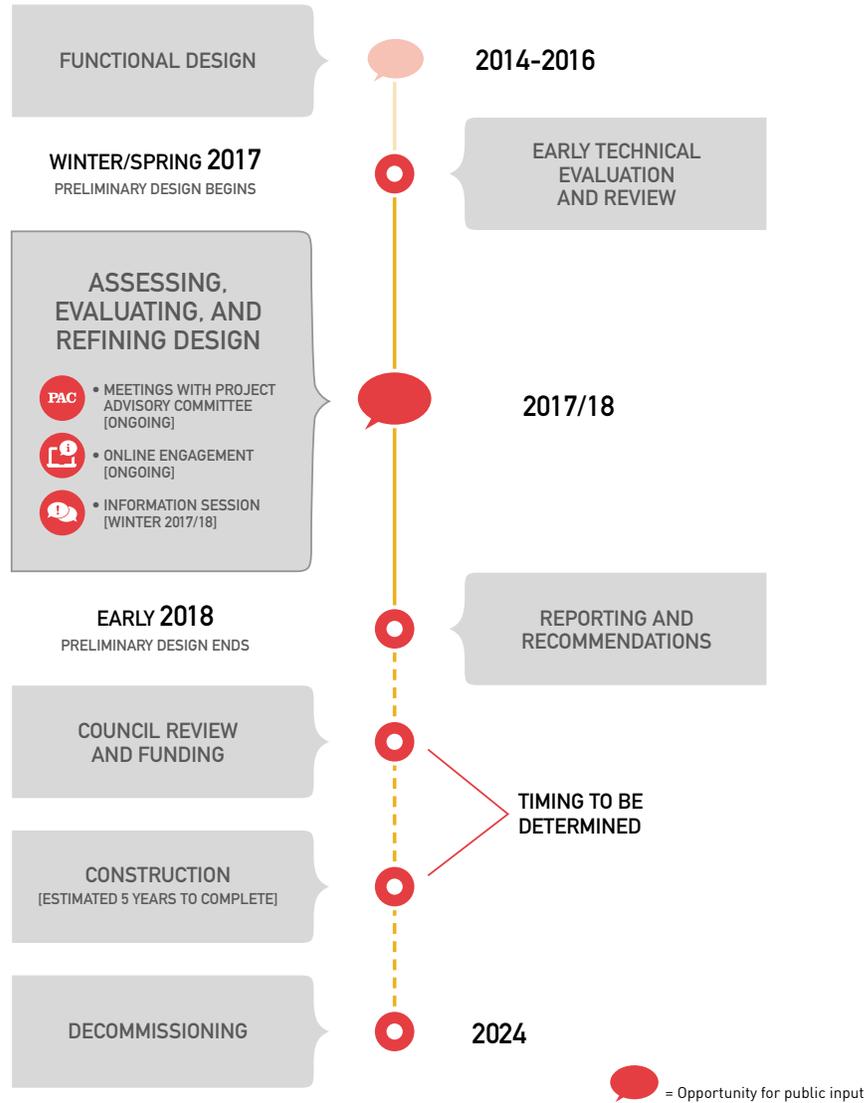
In addition to other engagement activities, the PAC was regularly engaged at all stages of this important project to ensure that local perspectives were clearly heard, and to provide input on how a new bridge for Arlington could help their communities thrive. Members of the PAC represent a broad range of views, including community residents and businesses, children and youth, seniors, Indigenous peoples, cyclists, newcomer Canadians, people living with disabilities, as well as organizations that help support area needs.



# 3

## A BETTER BRIDGE FOR ARLINGTON

# TIMELINE



The new bridge is currently slated for completion in 2024, although specific timing for construction of a new bridge at Arlington is yet to be determined.

Construction would begin only after detailed planning and design is complete, City Council has approved a final design and funds have been allocated for construction. The current cost of the bridge is estimated at \$330 million, but that amount may be adjusted depending on final design. A Class 3 cost estimate will be provided to Public Works for review and submitted to Council.



# 4

A BETTER BRIDGE FOR ARLINGTON

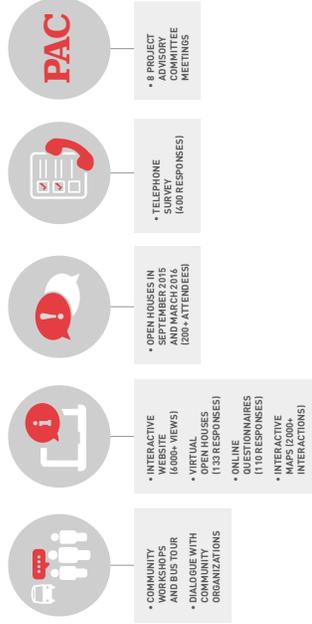


## ENGAGEMENT AT-A-GLANCE

The City and project team have worked with the public throughout the life of this project. Thank you for your input!

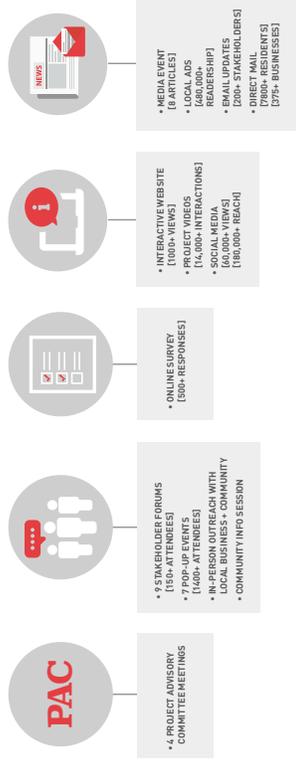


### FUNCTIONAL DESIGN



The Functional Design was a study to choose a location for a crossing of the CPR Yards between McPhillips Street and Salter Street. The study concluded it was best to replace the Arlington Street Bridge in its current location. The study also defined functional items for the replacement such as number of lanes, accommodation of cyclists and pedestrians, etc.

### PRELIMINARY DESIGN



The Preliminary Design builds on the functional design by investigating technical issues in more detail, gathering targeted feedback on specific features and elements that could be included in bridge design and creating more accurate cost estimates. This phase will result in a proposed bridge design for Arlington, which will be provided to Council for approval before the project can proceed to construction.

# 5

## A BETTER BRIDGE FOR ARLINGTON

# COMMUNITY INPUT

WHAT WE HEARD	HOW IT WAS USED
<p><b>Safety is really important.</b> People need to feel safe and secure both on and around the bridge. Better lighting is essential.</p>	<ul style="list-style-type: none"> <li>• Better lighting and sightlines on, under and around the bridge and Arlington Street</li> <li>• Protected bike lanes and sidewalks</li> <li>• Intersections designed to minimize potential for encounters between people walking, cycling and driving</li> </ul>
<p><b>The bridge should be a vibrant addition to the city skyline and a source of pride for the neighbourhood.</b> More than 70% of respondents to the Better Bridge for Arlington survey said a new Arlington Bridge should make people feel proud, and incorporate ways to reflect the neighbourhood's past and present.</p>	<ul style="list-style-type: none"> <li>• New signature arch, lit up at night</li> <li>• Features like rest stops, lookouts, public art and attractive lighting along the bridge sidewalks</li> <li>• Attractive, landscaped streetscapes and boulevard on Arlington Street</li> <li>• Plans to incorporate public art that draws on neighbourhood culture and history</li> <li>• Planning for community use spaces around the bridge</li> </ul>
<p><b>The bridge should be easier to cross for all users.</b></p>	<ul style="list-style-type: none"> <li>• Less steep: From 7.1% grade to 5% grade, with rest areas</li> <li>• Access for emergency vehicles and transit</li> <li>• Protected bike lanes and sidewalks</li> </ul>
<p><b>Improve traffic flow.</b> There was a strong desire for improved travel times and traffic flow, particularly during rush hour, to and from Health Sciences Centre and other key destinations.</p>	<ul style="list-style-type: none"> <li>• Intersection and travel lane improvements, including dedicated turning lanes</li> <li>• Significant traffic capacity increases at Logan Avenue and Arlington Street</li> </ul>
<p><b>Maintain access during construction.</b> Business owners and bridge users want bridge and road closures to be minimized as much as possible during construction.</p>	<ul style="list-style-type: none"> <li>• Traffic flow on Arlington Street will be maintained with one lane in each direction or a in a single unrestricted lane to lessen impact on users and maintain access to businesses.</li> <li>• Bridge will remain open to vehicles for all but one construction season; pedestrians and cyclists may be able to use the bridge even longer</li> </ul>
<p><b>Closing Dufferin Avenue to vehicles and installing a pedestrian tunnel wasn't preferred.</b> Many people were strongly opposed to a proposed pedestrian tunnel at Dufferin Avenue for safety reasons.</p>	 <ul style="list-style-type: none"> <li>• Plans for a pedestrian tunnel were scrapped in favour of keeping Dufferin open to all traffic, so vehicles, pedestrians and cyclists are able use the underpass effectively</li> <li>• Improvements to sightlines, lighting and other safety features have been added to discourage gathering under the bridge</li> </ul>
WHAT WE HEARD	WHY THE SUGGESTION COULDN'T BE INCORPORATED
<p><b>Create affordable housing on open land around the bridge.</b> We frequently heard there is a strong need for affordable housing in the area. This has also been documented in area housing plans. Many people indicated they would like to see surplus land from this project turned into housing.</p>	<ul style="list-style-type: none"> <li>• In 2013, The Federation of Canadian Municipalities recommended a 300-metre setback for housing development projects located next to rail yards. This guideline makes housing or mixed-use development unlikely within the developable parcels. In order to construct housing on these properties, prohibitively expensive crash walls would need to be installed.</li> </ul>
<p><b>Preserve and repurpose the bridge for another use.</b> Many Winnipeggers suggested the current bridge be preserved, and reused for something [e.g. green space, or a cycling and pedestrian bridge].</p>	<ul style="list-style-type: none"> <li>• Although there are examples of other bridges that have been repurposed as pedestrian and cycling routes, those bridges are typically old railway structures that are stronger and in better condition than the Arlington bridge. The condition of the Arlington bridge means it cannot be preserved and reused. Very few components of the bridge are still in usable condition and are not up to today's design and accessibility standards. However, there was also significant public interest in reusing parts of the bridge for public art or sculpture, an idea that will be considered as part of a process managed by the Winnipeg Arts Council.</li> </ul>

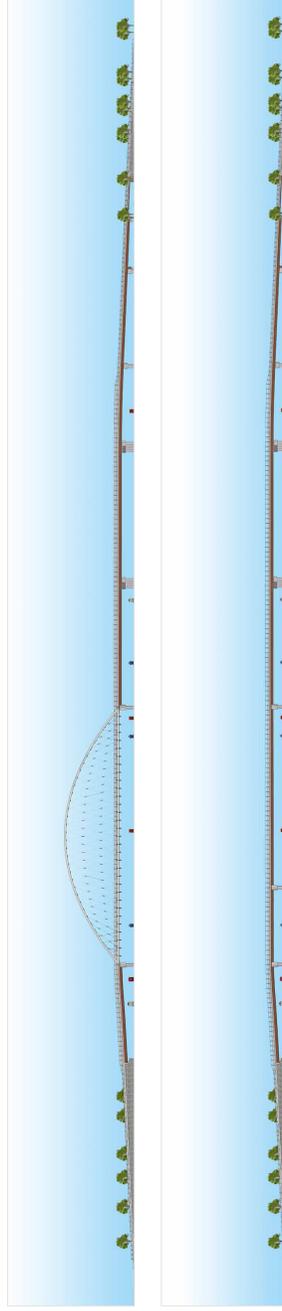
# 6

A BETTER BRIDGE FOR ARLINGTON



## BRIDGE CONCEPTS

The project team has designed and evaluated the following bridge concepts as part of their final report. Both bridge concepts scored similarly on project evaluation criteria and respond to public feedback by including key safety features, lookouts, and opportunities to incorporate public art. Both concepts will be provided to Council, which will decide which bridge will be approved and constructed.

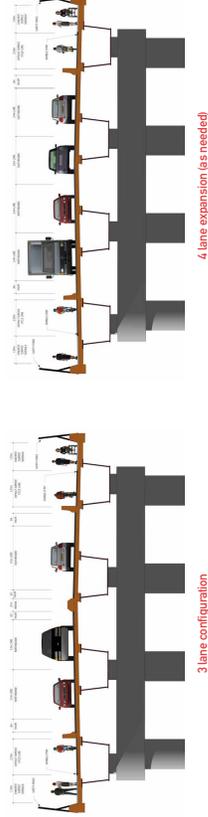


### Network Arch Bridge

- Directly responds to public feedback that the proposed bridge design should be a distinct addition to the Winnipeg skyline
- Allows the bridge to span the main line with less disruption and risk to CPR (a critical transportation link for eastern and western Canada)
- Scored slightly higher on overall project evaluation criteria

### Tub Girder Bridge

- Reflects a lower estimated project cost
- More difficult to construct and an increased risk of additional project costs/delay because it requires an additional pier to support the bridge over the rail yard
- Scored slightly lower on overall project evaluation criteria

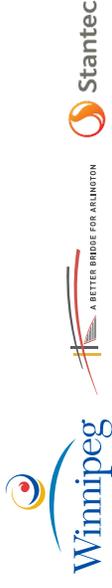


3 lane configuration

4 lane expansion (as needed)

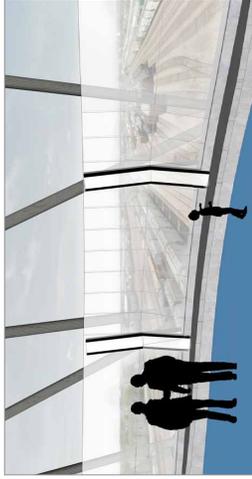
# 7

A BETTER BRIDGE FOR ARLINGTON



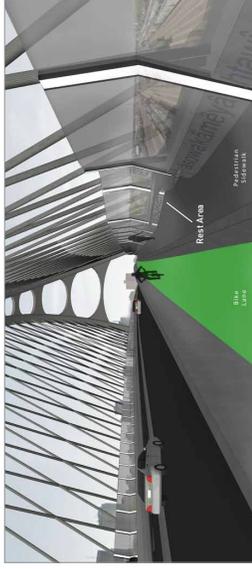
## BRIDGE FEATURES

The following key features are common to both bridge designs:



**View from the bridge**

Vantage points and rest areas will be an enjoyable place to take in city views.



**View of sidewalk and bench area**

Durable stainless-steel mesh safety fencing on the bridge will visually blend with the structure. Pedestrian paths and rest areas will meet universal design standards and discourage loitering, as they can be seen and monitored by traffic moving over the bridge.

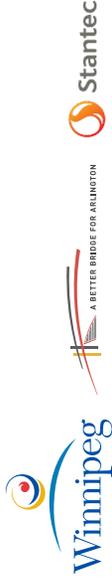


**Enhanced, integrated lighting**

Well-lit from end-to-end and includes opportunities to incorporate public art.

# 8

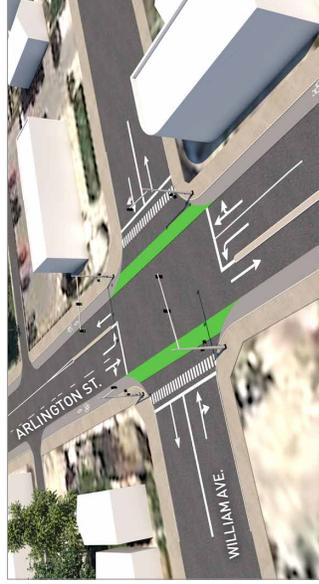
## A BETTER BRIDGE FOR ARLINGTON



# TRAFFIC IMPROVEMENTS



Arlington St. and Logan Ave.



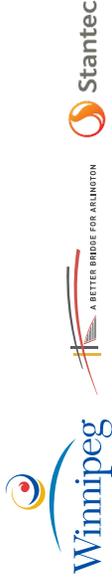
Arlington St. at William Ave.

The following improvements are designed to improve traffic flow, reduce congestion and improve connections to the Health Sciences Centre and other key destinations:

- Removing traffic lights at the top of the structure
- Adding an extra lane on the new bridge to meet current traffic needs
- The bridge can be expanded to four lanes to accommodate future traffic volumes, if needed
- Southbound at Logan Ave: two lanes southbound plus a dedicated left turn lane onto Logan to help address morning rush hour congestion
- Northbound at Logan Avenue: two through lanes northbound, plus a dedicated left turn lane onto Logan to help address afternoon rush hour congestion
- Southbound at William Avenue: a dedicated left turn lane at William to help address morning rush hour congestion and improve through traffic to Notre Dame

# 9

## A BETTER BRIDGE FOR ARLINGTON



# DUFFERIN UNDERPASS



Dufferin looking West



Dufferin looking North



Dufferin looking Northwest

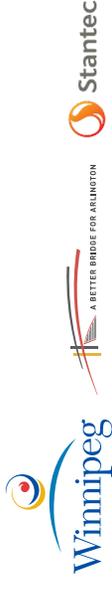
- A vehicle underpass was considered during a previous phase of the project, but was not possible due to existing City clearances
- Project team considered a pedestrian underpass as an alternative, but that option was not favoured by the community due to safety concerns
- Modified clearances have since been permitted and now allow enough space for vehicles to travel safely under the bridge
- Underpass design has been updated to account for new clearance as well as consider community feedback so that everyone – vehicles, pedestrians, cyclists and the community – can use the underpass effectively

### SAFETY FEATURES INCLUDE:

- Convenient and safe pedestrian crossings
- Textured surfaces under the bridge to discourage loitering
- Enhanced lighting
- Clear sightlines and open views from Dufferin to Jarvis so you can see the whole path, at-a-distance, as you approach

# 10

A BETTER BRIDGE FOR ARLINGTON

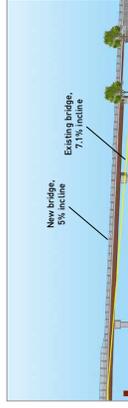


## PEDESTRIAN/CYCLING IMPROVEMENTS



### CONNECTING COMMUNITY ACTIVE TRANSPORTATION ROUTES

The McDermot bike lane will connect to the northbound Arlington bike lane. A sidewalk switchback will connect pedestrian traffic from Dufferin to the Arlington Bridge.



### LESS STEEP INCLINES

The proposed ramps are far less steep than the existing ramps, from a grade of 7.1% grade to 5%.



### SAFER CROSSINGS

The Arlington and Alexander intersection includes a pedestrian crossing with a signal, as well as a centre median safe for pedestrians waiting to cross. The Logan intersection will have a separated bike and pedestrian crossing with safe sightlines for drivers.



### SAFER BUS STOPS

Arlington will be able to support dedicated bus routes, and improved bus stops locations are designed for all users, protecting pedestrians and allowing both pedestrians and cyclist to use the area safely.

# 11

## A BETTER BRIDGE FOR ARLINGTON



# SAFETY

We heard clearly that people need to feel safe and secure on and around the bridge.

“It must be well-lit and safe.”

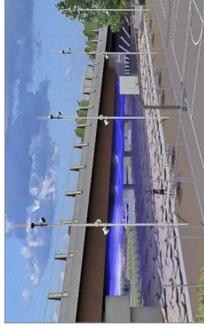
“Better lighting and safety features are key.”

“Safety is No. 1. Lighting and visibility to deter people from undesirable activity or violence against others.”

“Well and brightly lit – a beacon to the North End.”

Excerpts and comments from public feedback, Fall 2017

### SAFETY FEATURES OF THE NEW BRIDGE WILL INCLUDE:



Decorative and safety lighting and SafeWalk call stations on, under and around the bridge



Textured surfaces to discourage loitering under the bridge



Separation between cyclists, pedestrians and vehicles

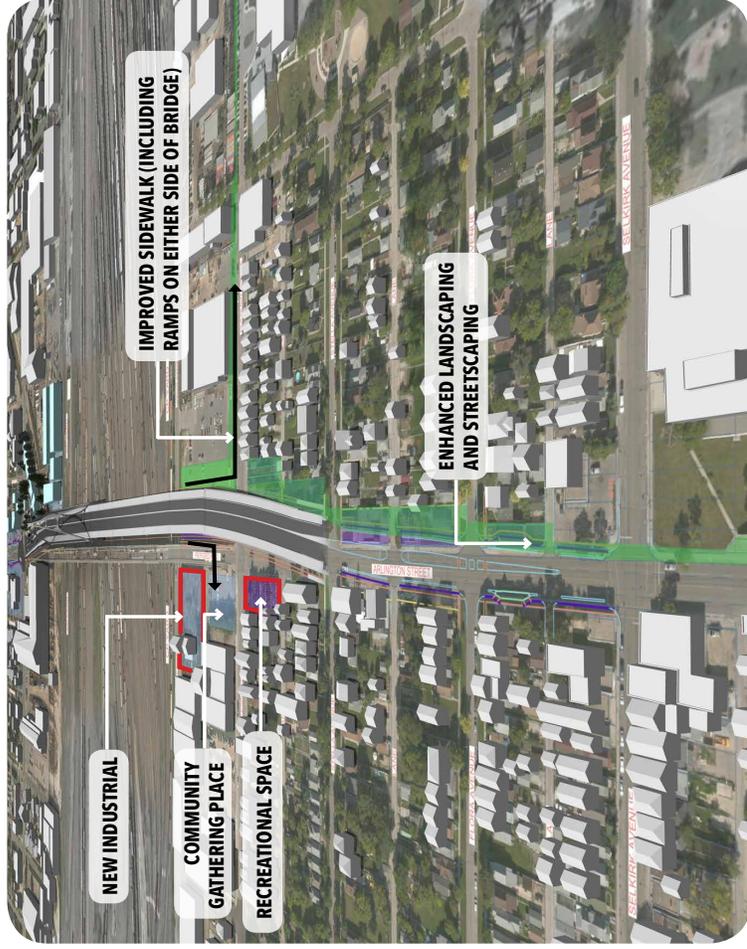


Eliminating blind spots and improving sightlines



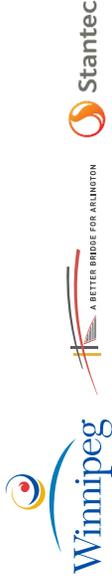
# REDEVELOPMENT OPPORTUNITIES (NORTH)

The proposed redevelopment of the land north of the bridge will include enhanced landscaping, streetscaping and improved sidewalks at Dufferin and recommend future opportunities for commercial, retail and industrial use that may be considered as part of future planning in the area. All opportunities are subject to Council approval and when appropriate funding.



# 14

## A BETTER BRIDGE FOR ARLINGTON



# COMMUNITY SPACES AND PUBLIC ART

Ensuring land around the bridge could be developed for community use and benefit and include more attractive and inviting landscaping was a frequent theme in feedback from community members. We also heard public art should incorporate ways to reflect the neighbourhood's past and present.

### COMMUNITY SPACES

Recommended community uses for lands around the bridge:



Multi-purpose space for community use could be developed south of the bridge.



Recreational basketball courts, community gardens and arts centre could be developed north of the bridge.

"Make it unique to the community, that it represents the community."

"From a historical point of view, I'd like to see some of the public art and the space around the bridge afterward reflect some of the history of the area."

"Include recreation items or areas like walking paths and basketball courts."

"Have some vision to make this a showpiece and multi-use community space."

### PUBLIC ART

Public art can be incorporated in several ways and in several locations: lighting, bridge walls, earthworks and sculpture. The Winnipeg Arts Council will prepare a public art plan that identifies specific locations and themes for public art, an adequate budget and the artists who will be selected to create it. The plan will consider feedback received from the public. Public art examples:

From Here Until Now  
Eduardo Aquino & Karen Shanski,  
Osborne Bridge, Winnipeg



Bike Rack  
Jess Korosoff,  
Winnipeg



Tempe Light  
Rail Transit Bridge  
Buster Simpson,  
Tempe Arizona



Light Through  
Bernie Miller,  
Doraell Active  
Transportation Bridge,  
Winnipeg



# BALANCING PROPERTY NEEDS

We heard clearly from stakeholders that housing in the area is important. We also heard the bridge should be kept open to traffic for as long as possible while a new bridge is constructed.

Keeping the route open during construction means the new bridge will be constructed beside the old one, which creates some property impacts. The new bridge design balances the need to improve traffic flow on Arlington, keep the bridge open for as long as possible while a new bridge is constructed, and minimize the acquisition of existing affordable housing in the area.



In total, 55 properties, including vacant and CPR land, are affected in part or full. Redevelopment opportunities will exist after bridge construction is complete. All impacted landowners have been contacted.

# 16

A BETTER BRIDGE FOR ARLINGTON

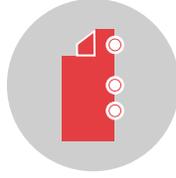


## DURING CONSTRUCTION

The new bridge will be built alongside the existing bridge in order to keep it open as long as possible – potentially for as much as 2/3 of the construction period. Once construction begins, the Arlington Bridge will be closed over one construction season (May to October) to complete the road tie-ins that are needed for the new bridge.



Traffic flow on Arlington will be maintained with one lane in each direction or in a single unrestricted lane to lessen impact on users and maintain access to businesses.



Logan will be open for truck traffic for the duration of the construction period, aside from one construction season (May to October).



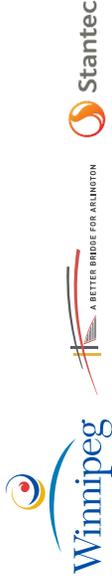
Pedestrians and cyclists will be able to access the bridge through most of the construction period.



Inspections of the bridge are conducted regularly, which may result in temporary or permanent closure of the bridge for safety reasons before 2024.

17

A BETTER BRIDGE FOR ARLINGTON



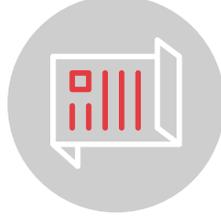
# THANK YOU FOR COMING!



View info boards online.



Fill out a comment form online or in person at the registration table.



A public engagement report will be available online in Spring 2018.



Reach us by phone at 204-928-8691 or email at [arlington@winnipeg.ca](mailto:arlington@winnipeg.ca).

Visit us online at [winnipeg.ca/betterarlington](http://winnipeg.ca/betterarlington)

## **Appendix C – Public engagement event summaries**

Pop-Up (Freight house Grill & Chill)

4:00 p.m. to 6:00 p.m.

September 6, 2017

Freight House field

Participant attendance:

~ 500 event attendees (60% kids, 40% adults), 20-25 surveys completed, 10 videos filmed

Notes, observations:

1. Event type/venue:
  - a. Good venue to chat with targeted community members.
2. Other:
  - a. Made connections with stakeholders (IRCOM, social work prof, city community services) that assisted in connecting with even more community members at subsequent events.
  - b. Raised awareness with key demographics (indigenous, youth, new immigrants/refugees)
  - c. No business owners present
3. Feedback from the team:
  - a. Need a bigger set up next time
  - b. Signs out front and in back
  - c. Use balloons or bubble machines with kids
  - d. Traffic video on iPads
4. Project feedback: Captured in surveys

Pop-Up (North Centennial Leisure Centre)  
5:30 p.m. to 7:30 p.m.  
September 12, 2017  
North Centennial Leisure Centre, 90 Sinclair Street

Participant attendance:  
~100 walked through the front doors during our pop-up event; 17 completed surveys

Notes, observations:

1. Event type/venue:
  - a. Most people accepted info and also asked questions if they had any, opted to complete survey on their own.
  - b. Good location to promote other events where feedback and discussion could be had more easily.
  - c. A few (at least 4) community members attended as a result of the online promotion of this event.
  - d. NCLC was very welcoming.
  - e. Busy location with Zumba and open swim for adults with mobility and developmental challenges, seniors meeting and youth from the local area attending the gym.
  - f. Would definitely use this location again.
  
2. Bridge/project feedback: Captured in surveys

Pop-Up (HSC)

11:00 a.m. to 1:00 p.m.

September 13, 2017

HSC Mall near William entrance

Participant attendance:

250-300 HSC visitors and staff walked through the mall, many who use the bridge on a regular basis

Notes, observations:

1. Event type/venue:

- a. People were willing to accept info and also asked questions if they had any; opted to complete survey on own.
- b. Good location to promote other events where feedback and discussion could be had more easily.
- c. One community member (real estate agent) attended as a result of the online promotion of this event as he had concerns about the land acquisition of one of his clients.
- d. Delivered the Property Owners Forum invitation to this location and other owners in the South West corner of the bridge project (Logan and Trinity area) to ensure they had info and a venue to ask questions.
- e. HSC communication was a bit convoluted (multiple contacts and no signage), but they were helpful day of in getting us to the right spot.
- f. VERY busy location – good for promotion but not for in-depth conversation.

2. Bridge/project feedback: Captured in surveys

Access Advisory Committee – Policy Subcommittee Meeting Notes  
September 15 2017, Millennium Library

Participant attendance:

9 - Committee Members/Coordinator/other guest presenters

Notes, observations:

1. Logan intersection design – height of cycle path:
  - a. Suggest raising the cycle path to the height of the curb through the island portion to avoid visually impaired/wheelchairs going up and down through curb. Suggest adding some kind of delineation/rumble strip at this transition so visually impaired pedestrians know they are entering a cycle path.
  - b. The City requested project team follow up on details of intersection design including audience signals, placement, curbs, etc.
2. Logan intersection design – safety concerns for cyclist/right-turning vehicle collision:
  - a. Suggest signalization for cyclists crossing Logan on cycle path or other measure to avoid vehicles turning right turn potentially hitting cyclists.
  - b. Noted City AT group has some lessons learned from issues at Assiniboine Ave at Main St. with conflict between cyclists crossing and vehicles turning right.
3. Grade:
  - a. In response to a question, project manager confirmed no slope is greater than 5% and intent is to have rest areas.
4. Benches:
  - a. The City confirmed benches must be incorporated on bridge as per City standards, approx. every 400m (approx. the distance between bus stops) and can't impede sidewalk traffic (i.e. use "bump outs" of some sort)
5. Tunnel/corridor:
  - a. Concern expressed about tunnel at Dufferin, noted very dangerous/not safe for pedestrians entering and exiting.
6. Transit:
  - a. In response to a question, the project manager confirmed the bridge design would accommodate transit. Interested in location of bus stops; not yet known.
7. Stairs/ramp on either side of bridge near Dufferin:
  - a. The City confirmed that a ramp is required if there are stairs.
  - b. Noted both stairs and ramp are preferable, but could be just a ramp. Either way, snow clearing is important.
  - c. Recognition that cyclists crossing sidewalk to use ramp isn't ideal, but it works.
8. Pedestrian crossing north of bridge:

- a. Noted lots of pedestrian traffic across Arlington (north of the bridge) to access North Centennial Leisure Centre on Sinclair. For visually impaired pedestrians (and children), group expressed that signalization makes for safest crossing. Suggest considering a pedestrian crossing at Stella Ave.

Meet Me at the Bell Tower  
6:00 p.m. to 8:00 p.m.  
September 15, 2017  
470 Selkirk Avenue, Selkirk at Powers

Participant attendance:  
30-35

Notes, observations:

1. Event type:
  - a. Very informal, community-hosted event.
  - b. Excellent opportunity to promote the campaign and to have the group receive information; video with Q& A worked really well.
  - c. Group discussion was challenging due to noise levels.
  - d. Having project team in attendance to answer questions made for a much better event and provided the project team with insight into community concerns/needs.
2. Venue:
  - a. MMATBT crew was very welcoming and promoted this project and our attendance at the Bell tower very well ahead of time.
3. Bridge/project feedback:
  - a. Uniqueness:
    - i. 7 of 12 felt this was Very Important
    - ii. 3 of 12 were neutral
    - iii. 1 of 12 felt this was not at all important
    - iv. 1 of 12 was unsure
    - v. Ideas: Stroller accessible, lights, keep current structure as shape is iconic, side rails to prevent jumpers, purple lights
  - b. Land Use:
    - i. Affordable Housing #2
    - ii. Affordable Studio Space for artists (and dance and music) #6
    - iii. Community Gardens #7
    - iv. Community gathering places #2
    - v. Improved lighting and safety features under the bridge #1
    - vi. Inviting Landscaping & sidewalks # 5
    - vii. New business and services #4
    - viii. Ideas: Shopping Plaza, parent centre, food bank, bank, clinic, North End Museum, tax free tobacco, safety cameras, get rid of Lipstixx, more lighting
  - c. Features:
    - i. City Skylines #7
    - ii. Enhanced lighting #2
    - iii. Benches/seating #1

- iv. Flowers/plants #2
  - v. Bathrooms #2
  - vi. Café #6
  - vii. Spaces for other #5
  - viii. Ideas: space for jogging strollers, monument, hangout space on the bridge
- d. Public Art:
- i. Key themes: Missing & Murdered Indigenous Women & Girls, Indigenous presence before European settlement in the area, Railyard History (including slavery), Cultural History of the area – past & present
  - ii. Would like to see Graffiti Art as a part of the bridge

Pop-Up (Save-on-Foods)  
10:00 a.m. to 12:00 p.m.  
September 16, 2017  
North Gate Shopping Mall

Participant attendance:  
~250 walked through the front doors during pop-up

Notes, observations:

1. Event type:
  - a. Most people were willing to accept info and also asked questions if they had any; opted to complete survey on their own.
  - b. Good location to promote other events where feedback and discussion could be had more easily.
  
2. Venue:
  - a. Save on Foods was very welcoming.
  - b. Very busy store with a demographic mix of community members.
  
3. Bridge/project feedback:
  - a. North end constantly gets shortchanged at City Hall (this came from multiple people).
  - b. The bridge should be made of concrete to reduce costs.
  - c. Additional feedback captured in survey.

Land Use Forum  
5:30 p.m. to 7:30 p.m.  
September 19, 2017  
North Centennial Leisure Centre, Seniors Hall

Participant attendance:  
6 - community members

Notes, observations:

1. Event/venue:
  - a. Small community turnout, but excellent discussion in 2 small groups.
  - b. More directional signage (front door, etc.) requested from the team.
  - c. Lapel mic requested by one community member
  
2. Questions/comments:
  - a. Why not 4 lanes?
  - b. Bridge closure being constrained is very much appreciated
  - c. Dislike any ideas of stay and playing on the bridge – unsafe and also leaves out people with mobility issues;
  - d. Bird cage – prevents jumpers
  - e. Balance accessibility concerns with safety concerns (tunnel, benches/gathering spots)
  
3. Lighting & Safety:
  - a. Safety is key; without this none of the other elements matter
  - b. Key elements of safety are: lighting, visibility/sightlines and escape routes
  - c. Tunnel at Dufferin is not safe/not supported; discussion of options identified connection to Jarvis and sidewalk built there and ½ tunnel (to reduce distance for those with mobility concerns);
  - d. Pedestrian walkways staying in sight of/with vehicle traffic offers additional safety
  - e. Not having a shortcut option will incent people to run across Arlington in an unsafe way to get from East to West side and vice versa
  - f. Ensure no ledges as these are spots used to leave drugs/money on a drop (salter bridge)
  - g. New police station location near the foot of the bridge?
  
4. Arlington Street corridor north & south of the bridge
  - a. Logan connection to Giant Tiger at McPhillips is a neat opportunity for development
  - b. Like the connection to Selkirk
  - c. Arts & Culture related development on the North side of the bridge
  - d. Need basketball courts in the area
  - e. All development should be easy to maintain and keep clean
  - f. Murals/graffiti art

5. Movement and links to important destinations:
  - a. Access to HSC and health services is key
  - b. Need safe/kid friendly access to NCLC
  
6. Housing, community & economic development
  - a. Job training program
  - b. No additional housing on the North side (but ensure a limited take)
  - c. Need single family/owner occupied housing supports
  - d. Get rid of board-ups; infill with 4plex/6 plex
  - e. Need to identify and fund a leader to develop a comprehensive plan for Selkirk & Dufferin for small business development, Arts programming and services; heavily supported by local community orgs.

Bear Clan – Walkalong Notes  
September 20 2017, 472 Selkirk Ave.

Participant attendance:  
Approx. 25 people on the walk

Notes, observations:

1. Under the Salter Bridge:
  - Route included inspection walk under the Salter Bridge along Sutherland.



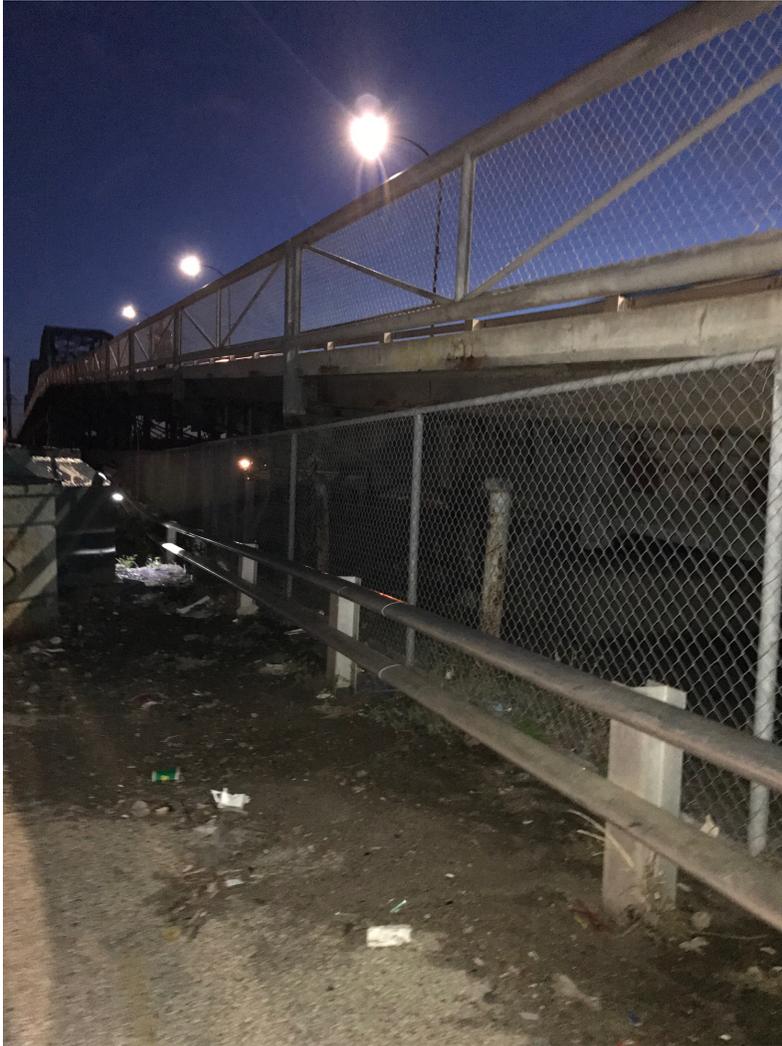
*Underneath Salter Bridge is open – lots of height, wide open views. Group noted in daylight hours, felt safe walking under the bridge along the roadway.*



*At narrowest point where bridge meets ground there was clear evidence of people gathering - needles, feces, and stashed bags found. Did not feel safe.*

2. Beside/under the Arlington Bridge:

- Route included Dufferin up to the Arlington Bridge. Group inspected east side of bridge along fencing to Jarvis (where new roadway connection from Dufferin to Jarvis is proposed).



*People lean mattress against fence to climb over, gain access to under bridge area behind fence. Clear evidence of people gathering under the bridge - needles, feces, and stashed bags found. Did not feel safe.*

#### 4. Conclusions & feedback from small group\*

- Bright lighting on sides, under bridge are absolutely critical in new design.
- Under bridge tunnel not safe option, nor is routing pedestrians and bikes closer base of bridge in open space.
  - o Evidence/observations from Salter Bridge cited as rationale for this – at that location even with steep gradient, people still gather at the top where there is cover and they can't be seen. If beneath the bridge is open – and the land is flat and accessible - likely very attractive for people to gather there as well.
- Pedestrian and bike access on Jarvis/adjacent sidewalk cited as safest/preferred option but would be important to have open views between Jarvis and Dufferin/surrounding area (e.g. ability to see Jarvis from Dufferin).
- Details of routes covered attached.

Connections & Construction Forum  
5:30 p.m. to 7:30 p.m.  
September 26, 2017  
North Centennial Leisure Centre, Seniors Hall

Participant attendance:

6 - community members and local business representatives (Don Vito, Microbiology Lab, local Automotive business)

Notes, observations:

1. No parking on Arlington:
  - a. This will impact visitors to the Microbiology Lab as well as students who cannot access lab parking. These groups will be forced to park on already busy residential streets.
2. Dufferin Closure:
  - a. Concerned that people wanting to access North Centennial Leisure Centre will not know how to do that; education of new routes (driving, walking, biking) from the bridge to NCLC is required.
3. Transit changes:
  - a. Assumed #71 will be impacted. Maintaining the stop near to North Centennial Leisure Centre (Sinclair & Dufferin) is essential.
4. Construction:
  - a. 2022 proposed closure of all but one lane in one direction on Arlington from McDermot to Alexander will “absolutely destroy” businesses along Arlington – specifically Don Vito raised concerns with this plan. Preferred solution is to have one lane in each direction open during the bike/sidewalk/road construction in both 2022 and 2023.
  - b. Access into business parking lots during the proposed 2022 and 2023 road construction between McDermot and Alexander is also very important.
5. Tunnel/corridor:
  - a. Concern expressed about tunnel and half/tunnel ideas for Dufferin, noted very dangerous/not safe for pedestrians entering and exiting. Taking pedestrian traffic away from vehicle traffic isolates the pedestrian traffic and creates a safety issue. Preferred solution is to connect to Jarvis and build a well-lit sidewalk that follows the road traffic.
  - b. Land development in the area can also impact safety by ensuring buildings face the walkways.
6. Land acquisition:

- a. Concern that this process has not yet started and has killed projects in the past.
  - b. Concern that property owners who have boarded up their property for yard will be paid well for their ignored and unsafe properties.
7. Stairs/ramp on either side of bridge near Dufferin:
- a. Very well received as these will provide pedestrian access to Jarvis/Dufferin area without having to walk all the way to Stella.
8. Impact on residential street traffic
- a. Concerns both on the North and South sides of the bridge. This needs to be studied as residential streets will see higher traffic volumes (Stella, Tecumseh, etc.)
9. Bike lanes
- a. Same safety concerns as sidewalks on off ramps/ need to connect to other well-lit, high traffic streets vs tunnel.
  - b. Adoption of the riding direction will see slower adoption; education required.
  - c. Confirmed that bike crossings will be possible (across Arlington at Stella and Flora) as medians will have a break at those corners. Intersection controls will be based on traffic study results (traffic lights/pedestrian crossings/ bike lights/ etc.)

Faculty of Social Work Class  
11:15 a.m. to 12:30 p.m.  
September 28, 2017  
485 Selkirk Avenue, Faculty of Social Work satellite campus

Participant attendance:

~30 students of a community based social work class located on Selkirk Avenue.

Notes, observations:

1. Dufferin Closure:
  - a. Concerned that a dead-end street will create a low-traffic hang out for prostitution
  - b. Concerned that this will leave the community/housing left on Dufferin even more isolated
2. Construction:
  - a. Closure of Logan intersection will create traffic issues near Pinkham School (this has happened before). School safety during this time needs to be addressed.
3. Tunnel/Corridor/Ped Exit ramps
  - a. Agreed that tunnel is not safe
  - b. Will there be a pedestrian off ramp on both east and west of the bridge? Want to see BOTH stairs and ramp for pedestrians to exit the bridge near Dufferin on BOTH sides of the bridge.
  - c. Safety issues with isolated areas and blind corners; design needs to ensure this is addressed.
  - d. Possibility of creating a skate park UNDER the bridge to take up that space.
  - e. Security Cameras in addition to lights and proximity to vehicle traffic will create a safer space
4. On the bridge:
  - a. Garbage and recycling along the bridge to reduce garbage on the bridge
  - b. Security Cameras on the bridge to dissuade undesirable behaviour
5. Impact on residential street traffic
  - a. Concerns on the North side of the bridge...will this impact the livability of the area? Noise studies will help understand the impacts and are planned.
  - b. Will traffic increase on Stella?
  - c. How will the increased demand in traffic and potential "speeders" impact the surrounding community? Increased noise? What other noxious impacts?
6. Traffic Impacts
  - a. Questioned 3 lanes vs 4 and asked about switching N/S centre lane but understood the explanation and were glad the project is looking forward to consider development north of the city driving possible traffic flow increases

- b. Explanation of the impact of the intersections to address traffic issues (vs building more lanes) given.

7. Bike lanes

- a. People support the bicycle lane addition on the Arlington Bridge.

8. Land Acquisition Process

- a. Some commentary about the potential impacts of residents on Dufferin. It was noted from the City representation that negotiation is preferred as opposed to expropriation.

9. Land Use

- a. A comment was raised about murals and artwork. An individual cited the success of a local initiative (Wall-to-Wall festival) as a way to engage residents and create artwork true to the fabric of the community. / Plan murals (similar to those going up on Main Street now) that represent the areas residents and use local artists. This will help humanize any concrete structure related to the bridge construction.
- b. Links to other green spaces in the area is desired (regardless of how the land is developed).
- c. Skatepark was suggested as a possible use on the lands adjacent to the Arlington Bridge
- d. Displacement of sex trafficking activity – where will it go and how will this be addressed?

Dufferin Residents' Association of Winnipeg (DRAW) AGM – Notes  
October 18 2017, North End Recreation and Leisure Centre

Participant attendance:  
~50 members of the DRAW

Notes, observations:

1. DRAW boundaries
  - a. McPhillips to Salter; Selkirk to Jarvis
2. Brief presentation was given by project manager followed by Q&A
3. Questions & comments
  - TIMING:
    - a) When will houses be coming down to construct new Bridge?
    - b) Why was someone permitted to build a new house this year when it would just be torn down to make way for the bridge?
    - c) Is this project being considered in relation to other city projects that may impact traffic?
  - HOUSING:
    - d) DRAW completed a housing study that shows a need for MORE housing in the area, not less yet that's exactly what this project will mean. What will be done to help renters or others who need housing?
  - TRAFFIC:
    - e) How many lanes will there be?
    - f) Will there be bike lanes?
  - FUTURE:
    - g) What will the new bridge be named?
    - h) What is the plan for the old bridge components?
    - i) Thank goodness there will be a new bridge.

National Microbiology Lab

12:00 to 1:00 p.m.

October 19, 2017

1015 Arlington Street (and 745 Logan Avenue location joined via video conference)

Participant attendance:

30-35 staff at Arlington location in-person, 2-5 staff via video conference from Logan location.

Notes, observations:

1. Bridge Inspection Video:
  - a. Great addition to this type of community conversation, particularly in the lecture theatre environment at the lab.
2. Construction:
  - a. What, if anything, might be expected impacts to the water supply and sanitary and storm sewer service during all phases of the implementation of the Arlington Bridge project?
    - i. Apparently, a sub-contractor of the consulting engineers engaged by the City needed access to the grounds while they took video footage inside the storm sewer “catch basins” that catch rain water and snow melt. Those run into a storm sewer main running North/South along Arlington. That needed to be checked as the bridge construction will impact the sewer/water services fed from/to us from Arlington.
  - b. Access to the Arlington lab location from the North side of the tracks as there will be no left turns during the proposed 2022 and 2023 construction seasons on Arlington:
    - i. Use Salter bridge during constructions
    - ii. Drive past the Lab and turn left after McDermot; backtrack.
  - c. What engagement has there been with residents/school that will be impacted by the construction detours?
    - i. Minor engagement on this to date as the detour plans are not complete. Once the plans are complete, the affected groups will be advised.
  - d. Closure of Logan intersection and resulting detour in 2023
    - i. Pinkham School speed zone and Alexander Ave/Tecumseh speed bumps will need to be addressed to accommodate increased traffic and large truck traffic
    - ii. Large truck traffic between Arlington and Logan locations will need a workable route
  - e. IDEA: open up Tecumseh at William (currently a dead end where city trucks park) to through traffic as a part of the detour.
3. Important destinations/routes:
  - a. Magic Thailand (Logan)
  - b. South-bound Arlington for south-bound traffic leaving the lab parking lot either from the Arlington exit or the Elgin-Tecumseh-William-Arlington route.

4. Bridge/corridor design feedback:

- a. Less parking (losing street parking on Arlington) is not good; possible solutions to adding back parking options (e.g., develop the empty city lot that gets overgrown with weeds; developing the free lot at Tecumseh and William that is sitting empty; adding a pedestrian crossing between William and Logan on Arlington to facilitate safe crossing without having to walk to the lights for those who park on residential streets on the west side of Arlington, etc.) should be developed and in place BEFORE the roadwork on Arlington starts.
- b. Will there still be lights on the top of the bridge?
  - i. NO. these are not needed in the new design as the grade has been adjusted with the raising of Logan and extension of the bridge over Dufferin to come down at Stella.
- c. Are there updated bridge renderings available?
  - i. Not yet, but they will be made available to the public and updated in the fly over video when complete.
- d. Why isn't the bridge 4 lanes?
- e. Like the inclusion of bike lanes as a support to AT.

## **Arlington Public Information Session**

5:00 to 8:00 p.m.

February 8, 2018

North Centennial Recreation and Leisure Centre

90 Sinclair Street

**Participant attendance:**150+ (106 official sign-ins on the sign-in sheets and 44 completed exit surveys). Majority adults, but also schoolchildren accompanied by parents and teachers.

### **Notes/observations:**

#### 1. Event type/venue

- a. Nice size room to accommodate info boards, tables and snack station

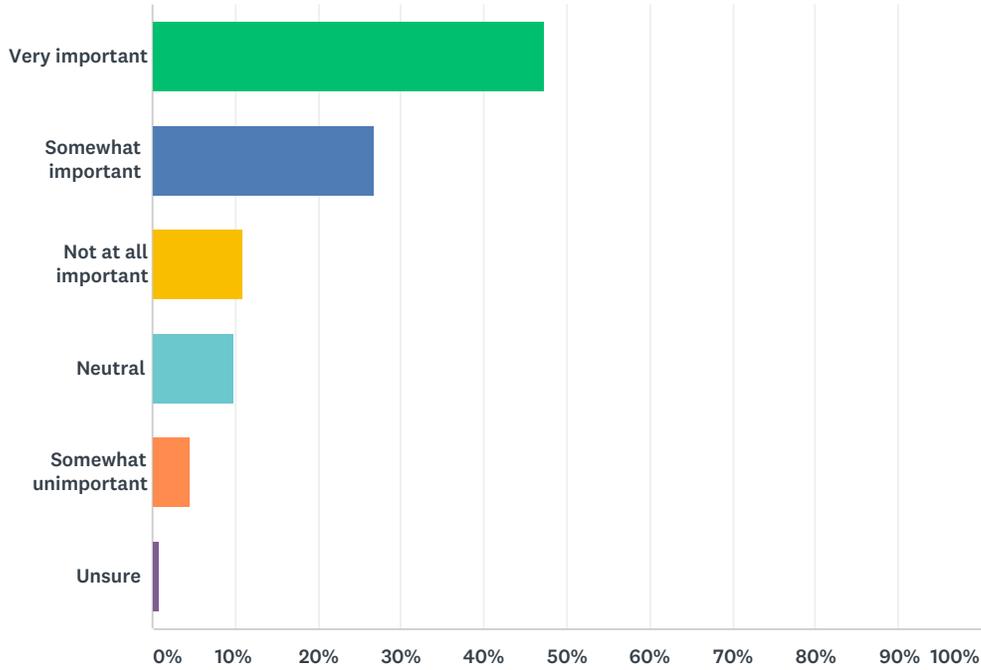
### **Summary of feedback from Exit Surveys:**

- The majority of people found the information they were looking for, with 39 respondents replying yes or somewhat. No one answered Not really/Not at all/Unsure or NA.
- The majority of people who filled out the exit survey said they were there to see the bridge design, with nearly as many coming for construction and timing information. Following that, people were interested in vehicle traffic improvements/changes, improvements for cyclists and pedestrians and the use of open land. Twelve people were there to see how public input was incorporated.
- Responses to question 3 regarding questions, comments or concerns not addressed were varied and are captured in the attachments. Themes from such a small sample are hard to capture, but things that came up at least two times each included concerns over number of lanes, project expense, community space use, pedestrian and cyclist crossing concerns, and intersection use.
- A few ideas were shared for improving project information and public engagement - and they also included recognition of a good job to date. Ideas included exit packages for sharing with neighbours, notices through the schools, a more user-friendly website and presentations at different times of day.
- Social media was the most mentioned by attendees as the place they heard about the event, followed closely by media coverage, email and the city website

## Appendix D – Survey results

# Q1 How important is it to you that the new bridge is a vibrant addition to the city skyline that makes people feel proud?

Answered: 506 Skipped: 1



ANSWER CHOICES	RESPONSES
Very important	47.23% 239
Somewhat important	26.68% 135
Not at all important	10.87% 55
Neutral	9.88% 50
Somewhat unimportant	4.55% 23
Unsure	0.79% 4
<b>TOTAL</b>	<b>506</b>

#	ARE THERE ANY SPECIFIC ELEMENTS THAT WOULD HELP MAKE THE NEW BRIDGE UNIQUE?	DATE
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## A Better Bridge for Arlington

1	<b>Comments Question 2</b> A contemplative viewpoint, less noise, the sound of jingling bicycles, walkways wide enough to hold hands, lighting that doesnt block out stars, lots and lots of flowers, good separated bicycle infrastructure aling Arlington all the way to the Assiniboine river and well into the north end on the say it opens (bridges that go nowhere aren't nearly as useful), protected bike paths wide enough so that two people can have a conversation because, well, cars get this space and people tend to travel in them alone (the ratio os about 1:16 by our counts) and saving all that room simply isn't fair for those who prefer a good bike and a healthy planet, clean and clear pathways all winter, with the right kind of granular material to keep it from being slippery, (thise wide bike paths will help because you can use a cheap, plain old truck to do the maintenance). In fact, with clineate change such a factor and the only thing to keep us from driving less being go make ot at least somewhat less inconvenient, we'd recommend foregoing allowing automobiles on the bridge altogether. Oh and please make sure that an architect is leading this project. Engineers are great, but aren't trained in placemaking. Outside expertise is also good for the city, so make sure it is designed so that the best apply. We've got great architects here too, but make them work for it. :)	9/30/2017 4:06 PM
2	<b>Comments Question 2</b> Suspension bridge elements would be interesting and unique. Open spots for pedestrians and cyclists to stop and observe the rail yards from above would also be interesting. Some nod to the components of the old bridge.	9/30/2017 7:31 AM
3	<b>Comments Question 2</b> Yes, bring it in at or under budget. That would be quite unique!	9/30/2017 2:58 AM
4	<b>Comments Question 2</b> Walkable bridge to connect the city and not just for motorized vehicles.	9/29/2017 11:45 AM
5	<b>Comments Question 2</b> The current bridge is so beautiful. It tells a story about our city - about our railroad history and the tragic ways that we divided ourselves by class. If lots of money is going to be spent on design, I would prefer that money be spent on preserving the existing bridge.	9/29/2017 9:56 AM
6	<b>Comments Question 2</b> Tunnel or covered foot and bike path across the tracks	9/28/2017 9:56 PM
7	<b>Comments Question 2</b> A (post)modern design that reflects Winnipeg's growing image into an attractive, progressive city that caters to a diverse set of interests; and one that reflects the diverse Indigenous culture of the North End	9/28/2017 9:38 PM
8	<b>Comments Question 2</b> incorporate art installations from various cultural groups	9/28/2017 4:29 PM
9	<b>Comments Question 2</b> Wall space for community art, synonym art consultation, art city, etc. Local commissions	9/28/2017 2:16 PM
10	<b>Comments Question 2</b> Historical monuments for workers who used the original bridge	9/28/2017 2:08 PM
11	<b>Comments Question 2</b> bridging cultures via art/ indigenous/ eastern european	9/28/2017 1:55 PM
12	<b>Comments Question 2</b> Unique, bright lighting	9/28/2017 1:52 PM
13	<b>Comments Question 2</b> The plans I have seen indicate that that the bridge will have 2 northbound lanes and 1 southbound lane. This is fine for evening rush hour when people are going home after work, but not for morning rush hour when people are going to work. I feel that it would be more useful to have either 4 lanes or alternating 1/2 lanes controlled by overhead lights to take account of these different traffic patterns. I did notice, on the diagram, that there appears to be divider separation northbound and southbound traffic. If my suggestion is to be considered, it would make this unnecessary.	9/26/2017 2:50 PM
14	<b>Comments Question 2</b> e.g. multiple coloured lighting options, ornate lanterns (Alexandre III style, perhaps?), sunset viewing area, sculpted/carved side walls along walkway	9/24/2017 7:50 PM
15	<b>Comments Question 2</b> I like the style it is now. I think if you can salvage the metal beams/girders and use them in the new design I think it would be unique and eco friendly.	9/23/2017 4:47 PM
16	<b>Comments Question 2</b> Functionality. Make it last long, and require as few updates As possible. While it should be visually pleasing, don't place so much emphasis on art when the focus shoul be on the durability of the bridge. We got 100 years out of the last one, we should aim to make this one as durable	9/23/2017 2:19 PM
17	<b>Comments Question 2</b> That it be made with 4 traffic lanes not 3	9/23/2017 10:22 AM
18	<b>Comments Question 2</b> It can be made to look gorgeous, but is it still going to be going over the old train yard? It will look like wearing a tiara with your ripped up jeans. The two things just don't compliment each other. The whole area should be redesigned, not just the bridge.	9/23/2017 8:58 AM
19	<b>Comments Question 2</b> Wider bridge for 2 cars on both sides, more night lights along the bridge for safe driving and people walking.	9/23/2017 12:58 AM

## A Better Bridge for Arlington

20	<a href="#">Comments Question 2</a>	Lighting and plants	9/22/2017 3:27 PM
21	<a href="#">Comments Question 2</a>	Efficiency for all citizens, not just drivers.	9/22/2017 12:30 PM
22	<a href="#">Comments Question 2</a>	The yards should be moved out of the city. The cost for this will only go up. There is a lot of land that could be used and trains carrying increasing amounts of hazardous goods will be diverted from the city centre.	9/21/2017 3:38 PM
23	<a href="#">Comments Question 2</a>	I think that the existing bridge should remain as a foot and cycling bridge. Similiar to the old ElmPark bridge in St Vital. The new cars only bridge should of course make us proud of it. This bridge could then be built while the old one is still being used. The designers wouldhave more flexibility is being really creative to location & design. Once the new one is built the Old one could be developed for everything from kiosk shops, art displays, community event displays, busking stations, etc.	9/21/2017 2:56 PM
24	<a href="#">Comments Question 2</a>	Provencher is an excellent example of a beautiful, well-functioning bridge.	9/21/2017 9:06 AM
25	<a href="#">Comments Question 2</a>	Greenspace on it and around it Lighting Accessible, safe and beautiful cycling and walking infrastructure	9/21/2017 8:59 AM
26	<a href="#">Comments Question 2</a>	Yes - build an functional bridge that pedestrians, cyclists and all vehicular traffic can use at a cost that isn't prohibitive. I'm tired of hearing about million dollar bike paths. I attended one bridge meeting where some ixxx thought the bridge should have washrooms like the Provencher Bridge. Winnipeg ain't rich and we can't afford stupidity. Let's walk on the wild side and design something that may not be breathtakingly beautiful but does the job, doesn't need repairs in five years and doesn't bankrupt the city. If we could build it out of recycled materials I'd be even more impressed.	9/20/2017 12:50 PM
27	<a href="#">Comments Question 2</a>	Do not build a bridge. Dig a tunnel. You then avoid huge temperature fluctuations plus 35 to minus 35. That's a 70 degree temperature change. You don't need to salt or sand in the winter, you can still have pedestrian traffic (cycling or walking) and it will be easier to maintain. I believe there will be less maintenance. Also, how many suicide attempts by people jumping from the bridge. You also will never need to deal with the rail lines again.	9/19/2017 2:05 PM
28	<a href="#">Comments Question 2</a>	Creative lighting on the bridge	9/18/2017 6:02 PM
29	<a href="#">Comments Question 2</a>	-More accessible to the public -Better angle	9/18/2017 12:30 PM
30	<a href="#">Comments Question 2</a>	We work with newcomers close to the bridge -New comers + indigenous significant and growing -The bridge could be an important symbol of bridging those communities - Elements to reflect both cultures -Built in interactive, cool discoveries, show commonalities - drums	9/18/2017 12:25 PM
31	<a href="#">Comments Question 2</a>	-Remove tracks -Replace with housing	9/18/2017 12:17 PM
32	<a href="#">Comments Question 2</a>	Framework (act like other bridge), keep parts of it	9/18/2017 12:08 PM
33	<a href="#">Comments Question 2</a>	-Lighting -Vibrant colours -Representation of diverse communities/connecting -Biking paths -Walking paths -Symbolism of opportunities -Historical information for education -Walking bur along N to S and S to N -Educated experiences -Railroad - Natural flew over barriers -At each end, a walking map of the area (both north and south) -Buses (indented with booth)	9/18/2017 12:00 PM
34	<a href="#">Comments Question 2</a>	Functionality is most important	9/18/2017 11:40 AM
35	<a href="#">Comments Question 2</a>	-Driving more important than vibrant -Traffic -Traffic is an issue that should be addressed Think about future traffic needs and future generations not just current	9/18/2017 11:37 AM
36	<a href="#">Comments Question 2</a>	-Bicycle -Pedestrian -Access -Bike rent	9/18/2017 11:24 AM
37	<a href="#">Comments Question 2</a>	Girder design	9/18/2017 11:07 AM
38	<a href="#">Comments Question 2</a>	Statement architecture (Provencher Bridge)	9/18/2017 10:55 AM
39	<a href="#">Comments Question 2</a>	-Nice pedestrian walk across -Good lighting -Information panels -Spot for street vendors	9/18/2017 10:41 AM
40	<a href="#">Comments Question 2</a>	-Impact the community in a positive way -Encourage walking, feel safe	9/18/2017 10:31 AM
41	<a href="#">Comments Question 2</a>	-Protection for jumpen -Kid style -Lighting -Safe walking	9/18/2017 10:19 AM
42	<a href="#">Comments Question 2</a>	-Lighting -Solar lighting -"Green" environment	9/18/2017 10:11 AM
43	<a href="#">Comments Question 2</a>	Lights that can make it a unique one	9/16/2017 11:04 AM

## A Better Bridge for Arlington

44	<b>Comments Question 2</b> While it would be nice to have a bridge that is unique. The most important feature for me is functionality. Quick route for vehicles and a safe route for pedestrians and cyclists.	9/16/2017 9:56 AM
45	<b>Comments Question 2</b> Lighting; design	9/16/2017 9:41 AM
46	<b>Comments Question 2</b> Something that reflects the history of the north end and /or Indigenous culture	9/16/2017 9:20 AM
47	<b>Comments Question 2</b> Give it a cycling lane	9/16/2017 8:51 AM
48	<b>Comments Question 2</b> Save money	9/15/2017 3:23 PM
49	<b>Comments Question 2</b> Unique and better lighting. Accessibility for all citizens. Thoughtful and innovative ideas regarding the realities of Winnipeg winters. Representation of the neighborhoods surrounding the bridge.	9/15/2017 3:19 PM
50	<b>Comments Question 2</b> Yes, improve traffic flow is key. Esthetics are secondary!	9/15/2017 11:15 AM
51	Do we really need a bridge there? Update easier flow to Salter & McPhillips. i used to use the bridge to walk to work (from bus) but believe now that traffic is limited on this street.	9/15/2017 9:38 AM
52	What I've seen so far is fantastic, I was concerned about pedestrian but the final design brought clarity.	9/15/2017 9:17 AM
53	Green elements! Living garden walls, and bicycle & walking corridors!	9/14/2017 7:27 PM
54	Build it to resemble the original if viable financially	9/13/2017 2:03 PM
55	unique architectural design. Lights.Glass. Metals.	9/13/2017 11:41 AM
56	what happened to this idea? <a href="http://www.cbc.ca/news/canada/manitoba/railway-move-winnipeg-1.3321053">http://www.cbc.ca/news/canada/manitoba/railway-move-winnipeg-1.3321053</a> makes much more sense to have a 4 lane thoroughfare instead of a bridge doesn't it?	9/13/2017 11:35 AM
57	It would be good to have a vibrant addition to the city skyline; however, we much ensure that function trumps form and not have the form impede the function in any manner. Good looks are good, but not useful if it takes away from the purpose of building the bridge. Christopher Robinson R2W 1E6	9/13/2017 8:59 AM
58	Lighting and safety	9/12/2017 9:13 PM
59	good lighting for both cars & people , covered & high railings for safety reasons for people walking or biking, room for planters or the lighting could be ornamental	9/12/2017 5:49 PM
60	Structural elements	9/12/2017 4:49 PM
61	We have no shortage of iconic bridge around the world ... Want a more human connection like cars. Make it unique to the community. Make sure the s represent s the community. Enclosed cafe with wifi.	9/12/2017 4:42 PM
62	Reflecting the prairie and Canadian Shield aspects of our region	9/12/2017 1:30 PM
63	movie crew from u.s. said don't touch the existing bridge unique more revenue will come..	9/12/2017 1:54 AM
64	Yes for me one of the key elements of the old bridge is the pigeon faeces everywhere it's unhealthy it's unsightly and downright disgusting I will not walk on the Arlington bridge for this reason please make the new bridge roostless make it so there's nowhere for those birds to seek shelter make it brighter maybe put a panic button here or there	9/11/2017 10:30 PM
65	Lighting, with some distinct visual elements that are visible from different viewing points in the area.	9/11/2017 3:06 PM
66	The bridge should not be replaced. Divert traffic elsewhere.	9/11/2017 2:06 PM
67	I would like to see the existing bridge kept as part of the design. It could be for pedestrians only like the older bridge by Bridge Drive In Ice Cream. I the new bridge could be built on either side of the existing bridge. This would give the neighbourhood a major attraction that would be a catalyst for neighbourhod improvement and safety.	9/10/2017 8:02 PM
68	Lights	9/10/2017 6:03 PM

## A Better Bridge for Arlington

69	Keeping the old bridge and making it a beautiful pedestrian/biking area. The old bridge is beautiful and has character. Take a lesson from other bridge revitalization. If the exchange keeps pushing west, eventually this will be considered a wonderful idea. Just like all Winnipeg projects that were hated at the time of building (provencher and cmhr, which are now our postcard shot,) Thai would be magical.	9/10/2017 5:28 PM
70	Completely lit up.	9/10/2017 4:26 PM
71	The design of the new bridge should reflect the history and heritage of this special area of the city. In addition, it is important that the design not block off the view of the city. On Canada Day, many times we have stood at the top of the bridge and enjoyed fireworks displays visible both from the Forks and from Assiniboine Park. Such a unique vantage point of our city should not be lost.	9/10/2017 9:09 AM
72	Unique design	9/10/2017 7:42 AM
73	Art. Representative of the neighbourhood.	9/9/2017 7:35 PM
74	Cool lighting.	9/9/2017 10:31 AM
75	Separation and buffering between people and cars: physically, psychologically, and noise-wise. One of the reasons the Esplanade Riel Footbridge is so great is that it feels so separate from the stress and noise of vehicular traffic. It doesn't need to be a separate bridge: just add a bigger barrier, make the sidewalk wider/grander, and include lookout points with seating to rest and/or enjoy the view.	9/9/2017 12:53 AM
76	Architecturally appealing and emphasizing aboriginal traditions that would be appealing and interesting to non aboriginal. Modern.	9/8/2017 9:54 PM
77	Good lighting so pedestrians and cyclists feel safe using the bridge at night.	9/8/2017 8:33 PM
78	City of Winnipeg wants your ideas for new Arlington Bridge.	9/8/2017 8:15 PM
79	Local art incorporating light and glass	9/8/2017 6:55 PM
80	Art commissioned by local newcomer and indigenous artists, bright and beautiful.	9/8/2017 2:16 PM
81	I think it is crucial that it is designed to function well for not only cars, but cyclists and pedestrians as well. It is less important that it stands out in the skyline from kilometres away, and more important that it feels safe and pleasant from a cyclist/pedestrian actually crossing the bridge. I would hate to see the city spend millions of dollars on building a glorified sculpture. Let's not forget.. this is a bridge. :)	9/8/2017 1:46 PM
82	Unique walkway, something to make it stand out, make it green (energy wise) as well	9/8/2017 11:21 AM
83	A great View from high up and walking and sitting space	9/8/2017 11:18 AM
84	Perhaps a covered walkway to shelter pedestrians from Winnipeg's cold winds would make it more appealing for pedestrians to walk. The bridge as it is now and in the future is a long walk from November to March.	9/8/2017 10:28 AM
85	Keeping the old one as a bike and pedestrian bridge.	9/8/2017 8:36 AM
86	Capitalize on the old bridge and its unique identity/history. Incorporate it into the new design as a public space and amenity. Have some vision to make this a showpiece and multi-use community space.	9/8/2017 7:30 AM
87	I've heard mention of keeping the old bridge and re-purposing it towards active transportation or public green spaces. I really like this concept, for the current bridge is iconic and one of a diminishing breed of bridge design.	9/8/2017 7:09 AM
88	Lights lots of lights	9/8/2017 12:37 AM
89	Ergonomic and environmentally friendly design that reveals that the values of Winnipeg's Citizens value design and materials used to Build the bridge and design & execute its entrances reflect supporting a clean, healthy and safe environment being sustained for at least The life of the bridge/overpass life (example: valuing safe passage for all kinds on commuters- cyclists, pedestrians, humans with children and/or pets, environmentally friendly forms of transportation other than petroleum fueled vehicles. Sensitivity to the need to restore massive concrete areas to include green space; perhaps, even on the bridge. )	9/7/2017 10:04 PM
90	Large structural supports/cables, lighting	9/7/2017 9:57 PM

## A Better Bridge for Arlington

91	Dedicated bike lanes that are protected from both pedestrians and cars	9/7/2017 9:35 PM
92	To look cool while you're crossing it on foot or bike, so people will want to take pictures on it. It will become a destination.	9/7/2017 9:14 PM
93	1) Wind protection that is vibrant and part of the design; not as an afterthought. 2) Non-harsh lighting that makes it safe yet pleasing to the eye.	9/7/2017 9:10 PM
94	LED lighting. Dynamic and capable changing colors	9/7/2017 8:43 PM
95	Keep the existing bridge and reuse it for cultural and recreational purposes	9/7/2017 8:35 PM
96	having the old one beside it as a park adding a layer to the story of our city its such an icon	9/7/2017 8:20 PM
97	Bright lights and inspiring design - I like the green one in the bottom left of your example pictures.	9/7/2017 8:19 PM
98	Green space, public art, pedestrian walkways and bike paths.	9/7/2017 7:30 PM
99	elements (spanning the whole bridge) . Cool led lighting at night. We should be proud when we look at our bridges. Not just a road that crosses over something.	9/7/2017 7:16 PM
100	Maybe some history storyboards along the pedestrian access ways telling the history of the neighbourhood. Local art installations that fit with the bridge design. For example how Edmonton had artists contribute to benches in one neighbourhood, described here: <a href="http://yegarts.tumblr.com/post/160777425458/benchmarking-alberta-avenue">http://yegarts.tumblr.com/post/160777425458/benchmarking-alberta-avenue</a>	9/7/2017 6:20 PM
101	On the open land, i suggest need to provide a recreation area or make a tourist spot area to make a one Land Mark of Winnipeg to be visited not only the Winnipeggers but also other tourists coming in our town or province. Make the BRIDGE be attractive specially during at night through colorful lights.	9/7/2017 6:18 PM
102	It is a bridge leading to one of the City's dirtiest and poorest areas. It doesn't need to be pretty. Save the money for actually improving the area itself, not to make a hunk of steel and concrete beautiful.	9/7/2017 5:50 PM
103	Integration of art; infrastructure that allows people to sit/gather and is pedestrian-scaled.	9/7/2017 5:06 PM
104	Showcasing local talent. Tapping into nature.	9/7/2017 4:53 PM
105	can look like a normal bridge as long as it works. don't spend money on vanity	9/7/2017 4:21 PM
106	solar powered lighting, maybe some interesting info about the train yard.	9/7/2017 4:19 PM
107	Incorporate a green-space element that is environmentally self-sustaining (solar-powered lighting fixtures, local plant varieties and a rain-dependent irrigation network).	9/7/2017 3:40 PM
108	The bridge should be safely accessible to cyclists and pedestrians as well as vehicles.	9/7/2017 3:29 PM
109	Just so traffic can flow in and out of North end smoothly	9/7/2017 3:14 PM
110	The picture with the spiral staircase accessing the bridge from below is unique and practical! Saves someone from having to walk to the end of the bridge to access it and adds a nice feature.	9/7/2017 2:43 PM
111	A strong relationship to, or re-use of parts of, the current bridge.	9/7/2017 2:29 PM
112	Ability to use as active transport focused but also special event friendly space	9/7/2017 2:29 PM
113	The old bridge is beyond it's useful life at over 100 years old; The new one should be built with materials and techniques that at least replicate that.	9/7/2017 12:51 PM
114	Lookout points and rest areas at the top for both pedestrians and cyclists.	9/7/2017 12:30 PM
115	Lighting, not always the same colour as we see the trend in new concepts.	9/6/2017 11:30 PM
116	Led lights dictating holidays and seasons. Orange for Halloween etc.	9/6/2017 4:31 PM
117	why must a bridge be unique ?	9/6/2017 7:34 AM
118	It would be good if it in some way reflected the North End community	9/5/2017 4:11 PM
119	Do nothing special for this bridge it crosses the train yard in a undisirable place to live. People don't care what the bridge looks like just as long as you can drive over it.	9/5/2017 1:13 PM
120	Function over form is key for this particular location. With all the other infrastructure repair required in the City, let's keep the costs reasonable.	9/5/2017 12:06 PM

## A Better Bridge for Arlington

121	I think beautiful lighting (think of Saskatoon) is something we are missing for our bridges in Winnipeg. Adding lighting on all new bridges is important in my mind.	9/5/2017 11:54 AM
122	incorporating shapes and colours to lively up the area. also think of the location, very resilient materials should be utilized, not glass or plastics.	9/5/2017 6:25 AM
123	Keep the existing bridge as a cultural and recreational resource	9/4/2017 10:50 AM
124	Nice lights to lite the bridge with.	9/2/2017 8:50 PM
125	well protected from the elements and useable in winter for the many people who have little access to vehicles	9/2/2017 9:56 AM
126	Led lighting like casino that can be changed. For special events of the months etc along bothsides.	9/1/2017 11:37 PM
127	It has been a landmark for almost 100yrs and it should be spectacular with all the changes/upgrades in our city. Its a large bridge that joins 2 sections of city together.... it should be luminated!!	9/1/2017 7:20 PM
128	Having some parking along the top deck for a short period of time would allow people to view the rail yards and see how this has added to Winnipeg's transportation hub for Canada and potentially to the USA.	9/1/2017 12:57 PM
129	Something that captures the heritage feel of the original bridge. Maybe even a plaque somewhere with a little bit of the history surrounding the original bridge.	9/1/2017 12:04 PM
130	design the bridge structure to contain elements previously unseen on any city bridges, like a suspension feature, or a twisted arch, something that will be featured in the top 10 unique bridges in the worlds	9/1/2017 9:59 AM
131	This bridge is so connected to the railway yards - it would be nice to feature some unique design elements that reflect its tie to the trains that move underneath. The elements don't have to be very obvious, but when looking at the bridge, could invoke thoughts of the trains, yards, history of the yards.	9/1/2017 9:58 AM
132	A public art installation. I like what was done with the Osborne Street bridge with the words/phrases that acknowledge the past.	9/1/2017 7:41 AM
133	I don't want to see a lot of money being poured into a design that is too unique and abstract. I just want to see an esthetical pleasing bridge.	9/1/2017 6:50 AM
134	Get rid of the tracks and don't build a bridge! That rail yard needs to go and become part of Centreport. Nothing else will fix that neighbourhood where most of us dare not go. The federal government pays half, and Winnipeg will be much better off.	8/31/2017 9:13 PM
135	Can we use pieces of the old historical Arlington bridge as an art piece or focal point when crossing the bridge?	8/31/2017 9:11 PM
136	Make sure that it's a SAFE AREA for people to use - both walking and driving or cycling. It's located in the Core area, so the deterioration in this part of the city should be stopped and reversed.	8/31/2017 4:13 PM
137	It would be great if it celebrated the North End and or had some kind of green element.	8/31/2017 4:09 PM
138	Unique architecture and/or lighting	8/31/2017 3:22 PM
139	Less roosting areas for pigeons (it is currently a pigeon disaster area with large amounts of droppings, and pigeon carcasses)	8/31/2017 1:59 PM
140	Lights can serve dual purpose of safety and beautification. With Winnipeg being in a geographical position where we get a significantly long dark winter, it would be nice to have something bright and beautiful in the north end. The lights could also be changed in colour if there was a occasion or world issue that warranted it.	8/31/2017 1:17 PM
141	Who cares what it looks like?? As long as its functional and on or under budget! and of course, replaced in a timely manner. This bridge is an artery!	8/31/2017 12:43 PM
142	Natural plant elements - native grasses	8/31/2017 12:38 PM
143	fusion of old and new design	8/31/2017 12:36 PM
144	Safety is #1 ... so 1) lighting 2) making sure the pedestrian part is visible to others using the bridge (vehicles, bicycles, etc.) so it would deter people from undesirable activity (violence against others, etc.).	8/31/2017 12:36 PM

## A Better Bridge for Arlington

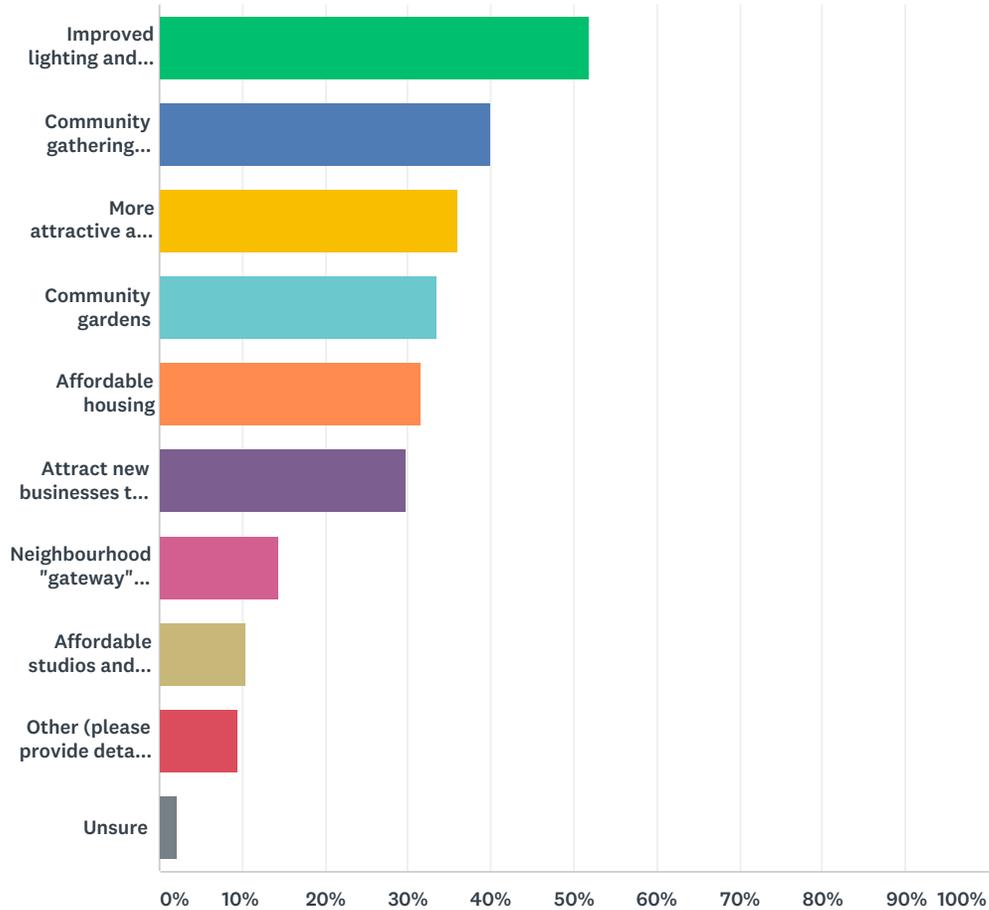
145	bike lanes; infrastructure investment so the whole adjacent areas are safe, not sketchy; vibrant addition to skyline as an entrance to the North End, which is always otherwise underserved and neglected.	8/31/2017 12:23 PM
146	Well and brightly lit - a beacon to the North End. This would welcome change from the dark passages that currently represent the transit to the much-maligned northerly neighbourhood (Main St and McPhillips underpasses, Slaw Rebchuk bridge) as well as providing a reassuring safety aspect.	8/31/2017 10:58 AM
147	Some design elements to reflect what the current bridge looks like.	8/31/2017 10:28 AM
148	Find ways to incorporate biophilic design into the bridge	8/31/2017 9:33 AM
149	Lighting, for decorative purposes as well as safety purposes. As shown in the pictures above, having a beautifully illuminated bridge welcoming our visitors to the north end is crucial. As well as green space. Having green space on, below, and at the entrance ways of the bridge is important for the connection and comfort of our community members.	8/31/2017 8:19 AM
150	not to make it feel like how the north end is	8/30/2017 10:36 PM
151	Bike lane, or the active transportation tunnel is a great idea.	8/30/2017 9:45 PM
152	One side for bikes and the other for walking with four lanes of traffic. Well lit and safe.	8/30/2017 9:39 PM
153	Four lanes only the way should be built	8/30/2017 9:35 PM
154	Do something that echos the original Arlington Bridge in the design. The current bridge is so beautiful, it's sad that it can't be saved.	8/30/2017 9:23 PM
155	Interesting lighting, a statue that represents Winnipeg in some way	8/30/2017 9:04 PM
156	I'd like to see wide walking and bike paths to connect the north end , make it easier to get to Assiniboine park etc.	8/30/2017 8:35 PM
157	Inclusion of biking options are important to me. I currently bike over the bridge each day to and from work and need to use the sidewalks in order not to hold up traffic.	8/30/2017 8:30 PM
158	I love the riveted steel structure and industrial feel of the current bridge.	8/30/2017 7:55 PM
159	Put our city flag and the Canadian flag.	8/30/2017 6:44 PM
160	Colourfull lights such as a northern light like how mcphillops casino is going with	8/30/2017 5:42 PM
161	A homage to our original Arlington Bridge would be nice.	8/30/2017 5:04 PM
162	Just get the job done since a new bridge should have been constructed at least 30 years ago.	8/30/2017 3:26 PM
163	No.	8/30/2017 3:12 PM
164	Just need it to work.	8/30/2017 3:02 PM
165	Unique lighting on the bridge that can be seen from a distance and identifies the bridge?	8/30/2017 2:51 PM
166	make it so some lowlife cant jump off the bridge.	8/30/2017 2:26 PM
167	solid construction to keep maintenance cost to a minimum	8/30/2017 2:25 PM
168	Most important is exceptional planning and execution. No design element is more important than the need to improve this piece of infrastructure in every way from the present bridge.	8/30/2017 2:09 PM
169	Lighting is number 1!	8/30/2017 2:08 PM
170	Well it really helps if you don't just copy a previous design, so the back room crew behind this disaster should take note: <a href="http://www.dialogdesign.ca/projects/walterdale-bridge-replacement/">http://www.dialogdesign.ca/projects/walterdale-bridge-replacement/</a>	8/30/2017 12:36 PM
171	please avoid designs that resemble anything phallic. this far too often seems to be the go to design and i think there are many other possibilities	8/30/2017 12:31 PM
172	Arches, towers and lighting	8/30/2017 12:00 PM
173	Uh, how about focusing on getting funding, and less on the aesthetics of what it will look like. Here's a tip, the fancier it looks, the more it will cost. Stop it, build a dang bridge like the Salter Street bridge, get on with it!	8/30/2017 11:15 AM
174	Incorporate the area during the time the bridge was developed 100yrs ago. The immigrants to the area and the railway connecting Winnipeg to all of Canada then and now.	8/30/2017 11:03 AM

## A Better Bridge for Arlington

175	lighting	8/30/2017 10:42 AM
176	lighting and natural landscape additions (planters etc.)	8/30/2017 10:40 AM
177	Lighting is an excellent way to add uniqueness to the bridge.	8/30/2017 10:28 AM
178	I think functionality is first and foremost the most important aspect. Attractive design is great, but will a bridge revitalize that area? Could the additional money be spent elsewhere?	8/30/2017 10:19 AM
179	I'd like to see an homage to the old bridge and its uniqueness.	8/30/2017 10:16 AM
180	Illumination at night	8/30/2017 7:21 AM

## Q2 Which do you feel would be the most beneficial ways to use the open land near the new bridge? (Select up to 3.)

Answered: 504 Skipped: 3



ANSWER CHOICES	RESPONSES	
Improved lighting and safety features under the bridge	51.98%	262
Community gathering places (e.g. skateboard parks, recreation)	39.88%	201
More attractive and inviting landscaping and sidewalks on routes to existing local businesses, community destinations	35.91%	181
Community gardens	33.53%	169
Affordable housing	31.55%	159
Attract new businesses that residents and workers in the area need	29.76%	150
Neighbourhood "gateway" features (e.g. "Welcome to the North End")	14.48%	73
Affordable studios and space for artists	10.52%	53
Other (please provide details below)	9.33%	47
Unsure	2.18%	11

## A Better Bridge for Arlington

Total Respondents: 504		
#	COMMENTS	DATE
1	You are on the right track! Keep going. Only comment is to think micro-business. Short term rentals diversity and being open to creative ideas are better than putting all eggs in one basket. Divvy up the land so this is possible. And do away with the parking minimums that ruin streetfacing, pedestrian, transit and bike friendly developments if you do housing. Winnipeg has enough parking.	9/30/2017 4:06 PM
2	Save money	9/30/2017 1:24 PM
3	A merge/yield lane. Keep the traffic flowing!	9/30/2017 2:58 AM
4	Mixed use buildings to promote commerce and build a sustainable community. This means a local grocery at the least.	9/29/2017 11:45 AM
5	Transit-oriented development - a couple towers with commercial retailers on the main floor would be awesome. Or maybe some some rows of townhouses with commercial outlets on either side. Please don't make parks - there are so many in the area and it will just spread the park user population thinner making each park less utilized and making them feel less safe.	9/29/2017 9:56 AM
6	Off-leash dog park	9/29/2017 9:35 AM
7	I believe attracting new business is auxiliary to the hereby selected, as local businesses are established, and are within walking distance of the site.	9/28/2017 9:38 PM
8	lots of trees (but open enough for safety), community green space, gardens	9/28/2017 2:21 PM
9	social support services in these spaces as opposed to businesses that might capitalize on the vulnerable populations	9/28/2017 2:16 PM
10	need to have more community based facilities for our youth	9/28/2017 1:52 PM
11	Treatment Centre	9/28/2017 1:48 PM
12	subsidized affordable housing, resource, treatment, social issue addressing needs. Homeless community (mini housing program, small housing; single person dwelling, managed criteria).	9/28/2017 1:27 PM
13	Bright, public spaces. Don't design and create spaces that are unsafe and become gang havens.	9/24/2017 1:25 PM
14	affordable assisted-living for seniors and the disabled would be very nice.	9/23/2017 3:08 PM
15	A safe place for pedestrians and cyclists. Cameras ideally and good lighting to ensure this is not a new place for the homeless to sleep or for criminals to loiter.	9/23/2017 2:19 PM
16	Having a covered underpass like the one in the picture is just asking for a place for people to get mugged. Places to shoot up, or huddle on a cold night. I wouldn't want to have to walk through there at night, even with it all lit up, someone can still break the lights.	9/23/2017 8:58 AM
17	There is should be an easy access to Jarvis and Dufferin as these are business areas. More lights should be provided to Jarvis area for safety	9/23/2017 12:58 AM
18	Please ensure there is good lightening in those spaces. The new bridge in Point Douglas did a poor job at that and now community members feel unsafe in those areas because they're isolated and dark.	9/22/2017 12:30 PM
19	The rail yard should be moved. There would be potential for large amount of affordable housing as well as community gardens and parks if the yards were moved out of the city	9/21/2017 3:38 PM
20	See my comments for Q1	9/21/2017 2:56 PM
21	Lots of green space, public parks and gathering spaces - trees and landscape that provide food, recreation and space to connect with nature.	9/21/2017 8:59 AM
22	Safety/dog-friendly with water	9/20/2017 8:16 PM
23	Emergency shelters	9/20/2017 8:11 PM
24	New bridge	9/20/2017 7:37 PM

## A Better Bridge for Arlington

25	Currently the area on the north side is home to the sex trade. Anything you put there should take that into account. Anything that isn't a 24/7 use (like residential homes) will probably be utilized after 9 or 10 pm for the sex trade. With that comes used condoms, needles, etc. I would suggest that whatever is put there has to have constant traffic or homeowners invested in their property who will "discourage" that type of traffic and contact police to ensure everyone in the neighbourhood is safe.	9/20/2017 12:50 PM
26	Go back to having more green space. Parks, grass, trees and picnic areas. How about a stage like Assiniboine Park?	9/19/2017 2:05 PM
27	-Free outdoor workout stations -Not a place people linger/get up to trouble -Must be safe first and foremost -No tunnel, not safe -Go to Jarvis instead with improved sidewalk, lighting + open sightlines	9/18/2017 12:38 PM
28	Close off Dufferin	9/18/2017 12:30 PM
29	-Community gardens -Community gathering places -Improved lighting and safety features under bridge Community gardens will bring together across ages + languages Rec- Youth they need things to do in the same levels as other parts of the city	9/18/2017 12:25 PM
30	-Sober socializing centre/meeting place (addictions recover focused) -NEWC + other common organizations -Strong support in community -Bear clan (James has details) -Minogizhiigun ("it's a good day")	9/18/2017 12:17 PM
31	-Affordable housing -Community gardens -Community gathering places -Water park to serve the families/ice rink -Parks with exercise machines near play areas/community/free access -Go up VS. go wide -Optimize the space -Parking, housing and gardens	9/18/2017 12:00 PM
32	Green space important especially in high area	9/18/2017 11:37 AM
33	Gardening to replace lack of stores	9/18/2017 11:07 AM
34	Growing food	9/18/2017 10:55 AM
35	-Girders are dangerous, that should change -Roof over sidewalks (bird poop)	9/18/2017 10:36 AM
36	Low income community organized sports, coaches	9/18/2017 10:31 AM
37	-Affordable housing for families with low income -Park for kids	9/18/2017 10:19 AM
38	-Recreation for youth in the area (hockey, soccer, baseball, football) -Safe place that's lit up	9/18/2017 10:11 AM
39	Youth Sports facility/Community center. Nomads Football. Basketball. etc...	9/18/2017 7:58 AM
40	Prefer old bridge	9/16/2017 10:45 AM
41	Don't waste money	9/15/2017 3:23 PM
42	And again, by adding the 3 items I selected, utmost importance is traffic movement.	9/15/2017 11:15 AM
43	The areas are more business related so community anything seems an odd fit. I think business/commercial nicer idea.	9/15/2017 9:38 AM
44	<a href="http://www.cbc.ca/news/canada/manitoba/railway-move-winnipeg-1.3321053">http://www.cbc.ca/news/canada/manitoba/railway-move-winnipeg-1.3321053</a>	9/13/2017 11:35 AM
45	It must be something well lit and safe, or it will extend the prostitution westward and northward that is already so rampant along Jarvis and Sutherland.	9/12/2017 9:13 PM
46	Amphitheater	9/12/2017 4:42 PM
47	permanent chessboards or game platforms	9/12/2017 1:40 PM
48	This will prove very important to the residents. As commuters rush along Arlington, they will not 'take in' the space. Residents take in the space daily and so its aesthetic and purposed-use impacts could really be a boost to the local atmosphere.	9/12/2017 1:30 PM
49	make more artist spaces studios..they have been pushed out by gentrification of riverfront, exchange, etc.ask the artists what they need..don't make it up yourselves.	9/12/2017 1:54 AM
50	The bridge should not be replaced. Divert traffic elsewhere. I doubt any businesses would want to be located here. There isn't space for a transport corridor here- too may houses close to a road which will become busier.	9/11/2017 2:06 PM
51	See earlier comments re keeping the current bridge as a tourist attraction.	9/10/2017 8:02 PM

## A Better Bridge for Arlington

52	Having a big free splash pad or pool similar to KP park and Transcona splash pad but free or KP prices. Maybe some restaurants that tailor to the residents and workers in the area. Tim Hortons, McDonald's, 7-11 or maybe a gas station on north side.	9/10/2017 6:03 PM
53	I really like the idea of an AT tunnel (well lit with clear sight lines) so that cyclists and pedestrians can easily travel under the bridge along Dufferin. The north end is already too bisected/interrupted by rail and road so we need to make sure that this new bridge doesn't add to those barriers for people on foot and bikes.	9/10/2017 9:09 AM
54	I would also expect that any gathering places would have some green space, able to incorporate community gardens. As for Gathering places, please include soccer fields (most played sport in the world, especially by newcomers), skateboard/BMX parks, and basketball courts	9/9/2017 11:13 AM
55	Listing only 3 is restricting. I think more than 3 of these things could be incorporated, e.g. greenspace with some community garden plots and good lighting with a few new businesses and good sidewalks/landscaping tying it all together. By greenspace I do not mean a sports field.	9/9/2017 12:53 AM
56	Please NO SKATEBOARD parks. The existing ones in the north end are barely used. They don't reflect what the kids are into.	9/8/2017 9:54 PM
57	City of Winnipeg wants your ideas for new Arlington Bridge.	9/8/2017 8:15 PM
58	I think affordable housing is a huge priority, but it would be nice to see more small scale developments rather than one big project.	9/8/2017 1:46 PM
59	Plant trees and such	9/8/2017 11:21 AM
60	Keeping the old one as a bike and pedestrian bridge.	9/8/2017 8:36 AM
61	No dumb signs. And while you're at it, move all the rail lines out of the city to center port and use the now abandoned lines for LRT and protected bike lanes...	9/7/2017 10:06 PM
62	It would have been better if all or a greater number of the listed items could have been chosen collectively as aspects of newly established communities at both sides of the new bridge/overpass.	9/7/2017 10:04 PM
63	Please no ugly signage such as that which welcomes people to Corydon avenue via confusion corner	9/7/2017 9:57 PM
64	Taking a lesson from the Netherlands, it is indeed possible to combine housing with gardens; community gathering places can be both safe and vibrant to ensure they are used by all ages. Active transportation is essential.	9/7/2017 9:10 PM
65	Green space in summer. Hockey rink with lights in winter.	9/7/2017 8:43 PM
66	reuse the existing bridge as a park space its self think new york highline park but in winnipeg with biking the potential for something special lies before us and as someone who's grown up going over it biked over it and even done the bike jam on it this icon should not come down lets repurpose it such a great icon not many cities have a bridge like this as long as this one is...	9/7/2017 8:20 PM
67	This area would benefit from implemented green space; trees, flowers, walkways, etc.	9/7/2017 8:19 PM
68	Re Community gathering places, not just artist studio space, but shared community work spaces that can be monitored and run by local people as a job. Could include space for farmer's market to help bring in fresh food options for the community. Could house a pop-up space for various businesses or services, including potential to partner with the faculty of health sciences at the University of Manitoba (near by on the south side of Arlington).	9/7/2017 6:20 PM
69	Creates area that makes attractive for the tourist and make a land mark of winnipeg that every people visiting manitoba always they included the area in their itinerary.	9/7/2017 6:18 PM
70	Year round markets where local people can sell goods and advertise local businesses. Somewhere to get groceries that is convenient and carries local produce.	9/7/2017 4:53 PM
71	Open space or park space will just become a spot for illegal activity; businesses or studios would be an excellent use of the space.	9/7/2017 2:25 PM
72	As long as the CPR yards are there, the noise levels are and will be prohibitive to housing. I live on Burrows Avenue and its barely tolerable. I'd say that land would be fine for warehouses or light industry zoning.	9/7/2017 12:51 PM
73	Rest areas for cyclists and pedestrians.	9/7/2017 12:30 PM

## A Better Bridge for Arlington

74	I would love to see the space around the Arlington bridge developed to meet the needs of those who live in the area. The idea of attractive landscaping & better access routes to nearby businesses is great. Additional ideas would be park/greenspace/sportsfields, although, with crime in the area fairly high, I'm not sure whether that is a good idea. Better lighting and safety features is also key. Bike paths and other measures to help connect this area to those surrounding it is also a good planning measure.	9/7/2017 10:04 AM
75	Dufferin should remain open. The north head of the bridge extends too far and divides the neighbourhood. The current proposed bridge design is not what divided community needs.	9/6/2017 2:54 PM
76	There is plenty of available land in and around Winnipeg. Why are you clouding the issue of bridge building with other issues?	9/6/2017 7:34 AM
77	Once again the bridge is in a place where most people in the middle income would rather not live. If you want to make the neighborhood nice spend money revamping the neighborhood. But were talking about a bridge that overlooks a train yard..yippy	9/5/2017 1:13 PM
78	While all of the above are important, affordable housing, community gathering places and ways to attract new businesses and workers to the area will help to revitalize the community. One way to attract new businesses is to have 'incubator' and start-up spaces available.	9/5/2017 11:54 AM
79	you definitely need the lighting and safety so I did not select, but having the ability to create spaces similar to what the exchange district has done, would help the area tremendously. spaces for artists, entrepreneurs (collaborative environments), community parks and recreation, etc. would help bring the community together. also maybe a design competition would work with the local designers and even international designers. Look what the ice huts have done for the forks, or even the cube (not sure if that was an award or not) has created for the exchange. Perhaps the north end community groups could team up to consider a design competition each year or even just one for the beginning of the end of the bridge project?	9/5/2017 6:25 AM
80	Anything to make it change from the dreggs that it currently is. Updating it to relocate the homeless and other transient people is a major concern.	9/3/2017 11:23 PM
81	better bike paths too connect to the Cpr railway tracks going north /south the Hydro lines pathway if possible	9/2/2017 9:56 AM
82	Make it benefical for are young growing people to feel proud to be apart of winnipeg canada. Show that we care.	9/1/2017 11:37 PM
83	- A better recreation centre for the kids in the community. - Expand north centennial to cater to the businesses and sport teams of the community and adult/senior leisure	9/1/2017 4:18 PM
84	Housing immediately to the north of the bridge is as old as the bridge. Is there any thought to assisting this area to be redeveloped. A new bridge will bring greater traffic flow to the North End and these buildings need to be improved/replaced. There is a school nearby and it will be of value. Consider how the children will cross Arlington to attend this school.	9/1/2017 12:57 PM
85	Please keep in mind that the area is not a safe, welcoming area. Many homeless/vagrant people hover in these areas and it would be nice to include more friendly businesses to modernize the area. I personally would not be going to a skate park or general park in that area as it would still feel unsafe due to the types of people vacating the area.	9/1/2017 12:53 PM
86	I do not like the idea of the bike/pedestrian walk way pictured above. I feel it would make for a dark, somewhat secluded area. The potential for crime against persons would be very high. try looking at something more safe? I also question what cutting dufferin off to vehicle traffic will do for existing businesses. The dufferin community has spent many hours working towards supporting and keeping businesses in the area. I feel this would force many businesses out of the area, an area that could be more accessible for customers, damaging the economy for the dufferin area. I also have great concern that some dufferin residents may be displaced with the removal of the already limited housing stock in the community. I do believe there is a manitoba housing complex located on Arlington, housing that is vital to the people living there!! I think you should be more concerned with the impact on people and community this bridge will have over "the aesthetics". Shame on you city of winnipeg, shame on you!!	9/1/2017 11:37 AM
87	I'd like to see a fifa sized soccer field in place where a fifa sized soccer field could fit. ;)	9/1/2017 9:59 AM

## A Better Bridge for Arlington

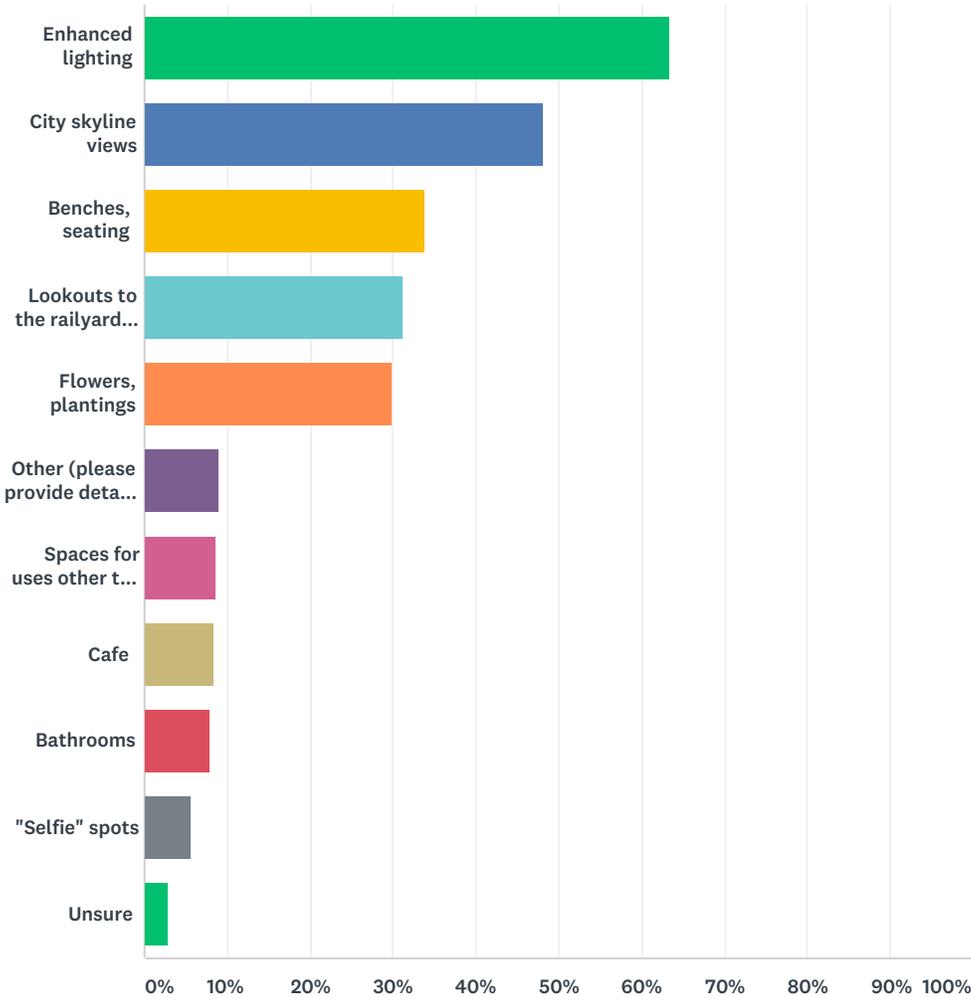
88	The north side space could be a mixed-use area on either side of the bridge. For instance, a historical park could be created that highlights the history of the north end, former bridge, rail yards, etc. Included could be spaces for a farmer's market, crafters and artists to showcase their products. On the Logan side of the bridge could be a skatepark (there are none in the area), and other recreation for kids and possibly some small, local retail outlet(s) that would cater to those in the area on foot (coffee shop, ice cream, art, etc.) The spaces should be graffiti and destruction-proof as there are still rough neighborhoods surrounding. Garbage and recycling containers should be available along the entire route, as well as provided at all major bus stops on Arlington (north and south). While it is good to create these spaces, they should be able to be managed so they do not deteriorate over time and should be modifiable should the original concept wane over the years.	9/1/2017 9:58 AM
89	Some green space would be nice	9/1/2017 7:38 AM
90	the area is very industrial. people and gardens would improve the look of the area.	9/1/2017 6:58 AM
91	Safety is my biggest concern. We need more places for kids to go that are safe from crime and drugs, even at night. Unfortunately parks have a way of turning into sites of drug use in the dark of the night.	9/1/2017 6:50 AM
92	None of these options are good because that is a terrible neighbourhood. Nobody wants to live or work there because of those tracks.	8/31/2017 9:13 PM
93	Is a man-made beach possible? A cool place for families to access through public transit instead of a long drive. The Oasis beach (off 59) could be a model for it. Could be a skating rink in winter? If not, a dog park?	8/31/2017 9:11 PM
94	I would hope that improved lighting and safety features would be automatic - not something that is an "option"!	8/31/2017 4:13 PM
95	The area north of Arlington is pretty rough, so bringing some bridge back into the area would be important	8/31/2017 1:59 PM
96	I like the idea of a soccer pitch, but it would have to be turf and lights for evenings (like Shaughnessy Park), because it would require no grass maintenance and lights for safety. Of course, parking for the users would have to be considered. The current field on Logan (the hill by the casino) does not have enough parking, the stands are dangerous and unmaintained, the field often has garbage and is not cut or lined properly, and we have numerous times found used condoms on the stairs up, under the stands and in the grass.	8/31/2017 1:17 PM
97	#1 is always safety. It should be #1, #2, #3, #4, and #5 in priority. It exceeds EVERYTHING else in priority. Example: A few years ago big, bright lights were added to the park at Stella & McKenzie ... it has made a BIG difference in the safety of the park and children / community members that use it. Anything safety related - such as lighting - is always #1. #2 Recreation items ... suggest things like: - Workout park (equipment) and walking path!! NOT the workout equipment that is dumb like I have seen other cities use (the outdoor stationary bike, elliptical, etc. ... these are mostly useless from a health perspective ... but the really simple structures that are used in circuit type training, like the City already has at Assiniboine Park and to a little lesser extent at Burton Cummings.... so put this on the NORTH side of the bridge so people in this area have something to do step-ups, chin-ups, rope climb, etc. on) - Skate / Bike park that's got more than just the Margaret Park on Arlington - Basketball courts (there are no outside basketball courts in the area ... but if you turned the tennis courts into basketball courts at North Centennial then no need to add basketball courts here) #3 Cool neighbourhood welcome sign (Welcome to the North End) ... it's just nice. Maybe others in the city won't have such a negative feeling about the North End and would be more likely to come here for recreation or some shopping.	8/31/2017 12:36 PM
98	Please do not close Dufferin to public transportation/vehicle access. Simply incorporate the idea of a tunnel for pedestrians and make sure vehicles can be included. I don't think it is safe to have a pedestrian tunnel in a neighbourhood with a so so reputation, if vehicles can be driving through the tunnel also it can reduce crime. Also consider adding local market space possibly, instead of another big box grocery store. Something like the forks possibly ... to make the north end more inviting and accessible. There are hardly any grocery stores in the north end, the nearest ones are very far away. And this includes Walmart.	8/31/2017 8:19 AM

## A Better Bridge for Arlington

99	I personally think that a park with a skating rink, a community center with a gym, a kids playground, an outdoor gym(with equipment that can withstand any temperature. I recently went to Fargo, ND, to visit a friend and we took a walk in his neighbourhood and on one of the walking trails there was an outdoor gym to provide a better quality of life for the residents. I can send you pictures of it! It's so neat) Something that people would have fun with would be really great. I don't think housing or studio space is needed there. There should be something everyone can enjoy. Maybe bring in some business that the residents require.	8/30/2017 9:04 PM
100	I love the idea of having community gardens to bring everyone together. Bright lighting with some sort of recreation would be great. Not just park benches or it will turn into a campsite.	8/30/2017 8:35 PM
101	bike station (air pumps, tools on cables), electric car charge, car coop parking area, use fill to build long ramps for pedestrians and bikes (should negate the stairwells that might become locations for muggings). what I really want to put here is LONGBOARD DOWNHILL COURSE (Winnipeg has too few places with elevation changes and paved trails).	8/30/2017 8:30 PM
102	We don't need more open spaces and dead areas... We need businesses and vibrant coordiors that support jobs. Enough community spaces!	8/30/2017 7:57 PM
103	Thinking long term, if the potential for increased foot traffic and a bicycle friendly environment exists, accommodations for people to live close to those environments and businesses that will service those residents are crucial.	8/30/2017 3:23 PM
104	Nothing because only homeless people will gather there and there will be loads of beggars harrassing people at red lights for money	8/30/2017 3:02 PM
105	Any pedestrian access under the bridge on Dufferin needs to be wider and much more safe-looking and inviting than the narrow, dark, dungeon-like concrete tunnel shown in the picture above.	8/30/2017 2:51 PM
106	No More Housing! green and trees would be nice	8/30/2017 2:26 PM
107	Higher density development than is already there, mixed use, something suitable to an urban scale that doesn't cater to sprawl mentality. If Dufferin has to be terminated then there should be a wide, well-lit, and attractive crossing underneath for pedestrians, cyclists etc.	8/30/2017 2:09 PM
108	The more people are there, the safer the bridge will feel. The potential for this to be a MAJOR route for people on bike to get into the north end is huge. I live in Wolseley and always avoid going to the North End b/c biking there is not inviting, even though there are places I do want to go to.	8/30/2017 2:08 PM
109	A concrete barrier (ie: wall) that would keep all of the dirty people in the north end away from the serenity of the south end. I said it, but we're all thinking it and you know it folks!	8/30/2017 1:05 PM
110	Cutting Dufferin off from Arlington is a big mistake, making the vehicular access to the nearby pool and recreation facility a cut through residential streets. Unsafe, urine soaked tunnels in the North End are frankly not a good idea. Not first demolishing the existing bridge and keep a straight alignment will also prove to be another mistake. It looks like there will be too much expropriation. Extremely poor planning here.	8/30/2017 12:36 PM
111	This has the opportunity to increase the property value in the area, it needs to be livable... I am a proponent of a grand vision to get rid of the tracks and use that rail yard as a giant park. That would add huge to the property values in that neighborhood.	8/30/2017 10:42 AM
112	murals	8/30/2017 10:40 AM
113	Cafes will create jobs in the area; while making it a kind of a "tourist spot" if it is neatly done. However, security should be addressed too.	8/30/2017 10:28 AM
114	Indoor & Outdoor basketball courts. Opportunity to create a basketball multiplex (with 6 or more courts) which is greatly needed in Winnipeg	8/30/2017 9:01 AM

### Q3 Which features do you want to see the most on the new bridge? (Select up to 3.)

Answered: 503 Skipped: 4



ANSWER CHOICES	RESPONSES	
Enhanced lighting	63.42%	319
City skyline views	48.11%	242
Benches, seating	33.80%	170
Lookouts to the railyards (trainspotting)	31.21%	157
Flowers, plantings	29.82%	150
Other (please provide details below)	8.95%	45
Spaces for uses other than views (e.g. busking shops)	8.55%	43
Cafe	8.35%	42
Bathrooms	7.95%	40

## A Better Bridge for Arlington

"Selfie" spots	5.57%	28
Unsure	2.98%	15
Total Respondents: 503		

#	COMMENTS	DATE
1	Consider the wind and the sun in the design. a 37' clearance over the railway exposes you alot, at the sane time please consider te proximity to vehicle emissions, another reason to consider separating or putting off motor vehicle capacity altogether. Note that benches are not optional on a structure this long.	9/30/2017 4:06 PM
2	Nothing just a bridge Save money	9/30/2017 1:24 PM
3	None of the above. Nobody is stopping on the bridge to "chat". I've seen people throw strollers, garbage, TV's and even a dog off that bridge. People are constantly (not being hyperbolic, it's all the time) urinating and spitting on CP employees working below. This isn't a nice part of the city. Build JUST a bridge. That's all we need, and that's all we can afford (in fact we can't even afford it)! If you put lipstick on a pig, it's still a pig!	9/30/2017 2:58 AM
4	The cafe is kind of a neat idea. If the city does the cafe, then a lot of time should be spent ensuring that the communities on the north and south side of the bridge are walkable and bike friendly so that the cafe has potential traffic to draw from. Having interpretive features that explain the story of the railroads in Manitoba and in Winnipeg would be great.	9/29/2017 9:56 AM
5	garbage and recycling receptacles	9/28/2017 2:16 PM
6	surveillance cameras	9/28/2017 1:48 PM
7	Indigenous Art; should put cameras on the bridge	9/28/2017 1:44 PM
8	lighting, benches, flowers	9/28/2017 1:43 PM
9	security cameras	9/28/2017 1:40 PM
10	pillars or geometric design along walls	9/24/2017 7:50 PM
11	We don't need people sitting or clogging up the bridge. You're building this as a transportation link, community can be on the ends. Make it a bridge that is as nice to look at as it is to look off. Easy skyline view and train yard view (until the yards are moved and this bridge becomes obsolete)	9/24/2017 1:25 PM
12	There is nothing great to see from the Arlington Bridge. The railyards are gross. Can't the railyards be moved out side the city somewhere? They are an eyesore and leftovers from years ago when rail traffic was the most important thing to a city. Environmentally, noise, pollution, diesel fumes etc. are not the greatest things to make this area attractive to the improvements that the city wants to put in place for the bridge.	9/23/2017 8:58 AM
13	Efficiency and useful should be our number one priorities then the extras. I really hate how the Point Douglas bridge is beautiful but unsafe due to lightening and lowers community members connectivity because most people walk and it was designed with drivers in mind only.	9/22/2017 12:30 PM
14	For those of us who walk and cycle as part of an active lifestyle, it is a real challenge to find public washrooms. As the population ages this becomes even more important to more people. Is this not even a human rights issue for people of all ages?	9/21/2017 8:11 PM
15	Lighting is a safety issue and should always be enhanced. It would be easy to have solar powered lights. The yards should be moved though which would negate the need for a bridge	9/21/2017 3:38 PM
16	See my comments at Q1	9/21/2017 2:56 PM
17	Love the idea of being able to see in all directions when walking/cycling over the bridge, as well as down into the rail yards... all this while surrounded by green space or beautiful landscaping features/public art.	9/21/2017 8:59 AM
18	Covered bridge/glass/steele and interactive like the Edmonton bridge with water	9/20/2017 8:16 PM
19	All of the above	9/20/2017 8:11 PM

## A Better Bridge for Arlington

20	All of that costs money for the original installation and maintenance. Another million dollar bathroom we certainly don't need. What we do need is some form of railing that will ensure no one can either do a nose dive or toss someone over the railing. As I said, Winnipeg can't afford champagne tastes. I'm not saying we should do ugly, just realize that most of this is ridiculous considering the bridge goes over a rail yard. Why are you trying to make it a "destination" rather than what it should be, a tool to get from A to B? You want to splurge? Make it four lanes instead of barely two. That would be money well spent.	9/20/2017 12:50 PM
21	Get away from the mindset of a bridge.	9/19/2017 2:05 PM
22	-Open sightlines between pedestrians, vehicles, bikes to feel safe especially after dark and if help is needed people can stop	9/18/2017 12:38 PM
23	Benches popular with new comers Space for community events - local agencies would take advantage	9/18/2017 12:25 PM
24	Cafe if financially viable	9/18/2017 12:17 PM
25	-Enhanced lighting -Benches, seating -Flowers, plantings -"Selfie" spots -Bathrooms will become a safety issue -Cafe will be waste of money -Mobility considerations -Perennial/renews itself (food, corn, privacy)	9/18/2017 12:00 PM
26	Totally against bathrooms and cafes	9/18/2017 11:37 AM
27	-I like the steep bridge, it's unique -Put famous NE people pictures	9/18/2017 11:07 AM
28	Lighting, more light would be cool	9/18/2017 10:31 AM
29	Kids love lookouts	9/18/2017 10:19 AM
30	-Reflectors on lane -Vendor stops (hotdog) -Water fountains	9/18/2017 10:11 AM
31	Pieces of the old bridge. Or some kind of Memory of the old bridge.	9/18/2017 7:58 AM
32	The bridge needs to be well lit and open to provide people riding bikes or pedestrians a safe route. Don't provide dark or underground spaces for people to "hang out" . While the north end has many wonderful people there is a lot of gang and criminal activity in the area. If the bridge or surrounding area provides them dark and covered spaces others won't want to use the bridge, other than to drive over regardless of how nice it is to look at.	9/16/2017 9:56 AM
33	A cycling lane	9/16/2017 8:51 AM
34	Don't go crazy, it's a bridge. Don't waste money for your ego.	9/15/2017 3:23 PM
35	Selfie spots? Bathrooms? None of these encourage traffic movement. Getting people to and from home/work is most important!	9/15/2017 11:15 AM
36	Are you serious? This isn't the Forks.	9/15/2017 9:38 AM
37	There should be a dedicated bike path, as all new/fixed bridges should have.	9/14/2017 1:10 PM
38	protected walkways to protect from bird droppings	9/14/2017 11:30 AM
39	Needs to be well lit, and also during winter it needs to be wide enough for traffic avoiding delays. when it is closed, it causes huge traffic disruption. Make it wide enough to avoid this.	9/13/2017 12:56 PM
40	NO BRIDGE ... remove the rail yard! <a href="http://www.cbc.ca/news/canada/manitoba/railway-move-winnipeg-1.3321053">http://www.cbc.ca/news/canada/manitoba/railway-move-winnipeg-1.3321053</a>	9/13/2017 11:35 AM
41	I think anything that brings added traffic for reasons other than gangs and prostitution will be welcome additions.	9/12/2017 9:13 PM
42	Not bathrooms or cafes. Maintain issues.	9/12/2017 4:49 PM
43	Conversation	9/12/2017 4:42 PM
44	Whatever is decided, I think it would be wise to extend the bridge over Dufferin (north side) and over the street currently on the immediate south side (name escapes me). The reason is so neither street's flow of traffic is interrupted by the increased volume along Arlington.	9/12/2017 1:30 PM
45	Its a bridge meant for driving over - there's a lot of traffic - deal with that.	9/12/2017 7:43 AM
46	incorporate safety lanes..better grips for icy days	9/12/2017 1:54 AM
47	Bicycle repair station	9/11/2017 3:06 PM

## A Better Bridge for Arlington

48	Save money- if you have to build a new bridge- then keep it utilitarian. It's a bridge in a rough neighbourhood! Benches and toilets will only encourage loitering and unpleasant people hanging around... why does it need flowers?- this would be a feature that adds to long term taxpayer costs	9/11/2017 2:06 PM
49	OMG really. I think w the necessity of the bridge emphasis should be on completing the project as quickly and as cost effective as possible. Maybe allow room for improvements in the future but just get a bridge done for car and pedestrian traffic. Lightning great under bridge walking or bike fb tunnel great ideas. Bathroom did we not learn anything from the promenade bridge 1million dollar bathroom DEFINITELY NOT needed.	9/10/2017 6:03 PM
50	This is very dependant on what the bridge looks like. If old bridge is kept, then none of these things on the new bridge and many for the old revitalized bridge.	9/10/2017 5:28 PM
51	Lots of lighting... seasonal lighting.	9/10/2017 4:26 PM
52	Public art installations that give pleasure and inspire thought year-round (whereas flowers and plantings are only seasonal).	9/10/2017 9:09 AM
53	Much wider bike facilities and sidewalks than standard! Make sure to give pedestrians and cyclists distinct spaces.	9/9/2017 7:21 AM
54	Lookouts could be for railyards AND skyline	9/9/2017 12:53 AM
55	Instead of demolishing Arlington Bridge, imagine it repurposed as an amazing Winnipeg public space.	9/8/2017 8:15 PM
56	Reserve all these great ideas for the old bridge by turning it into a pedestrian only public space walkway (eg high line in New York). New bridge can be beautiful from afar, but doesn't need amenities like these. Focus the new bridge for moving people, and preserve the old bridge for the people.	9/8/2017 3:57 PM
57	no thank you to "selfie" spots lol	9/8/2017 1:46 PM
58	cycling and pedestrian corridors, use of the old bridge as cycling and pedestrian corridor, keep the old bridge as art installation, do not remove the awesomeness that is the old bridge	9/8/2017 11:25 AM
59	I require a better personal understanding on how to combine all the interesting suggestions & features while not hindering overall safety and efficiency of movement for all sorts of pedestrians and commuters with differing goals and destinations.	9/7/2017 10:04 PM
60	Dedicated bike lanes.	9/7/2017 9:35 PM
61	String lighting would be cool.	9/7/2017 8:43 PM
62	highline park new york city look it up lets do it!	9/7/2017 8:20 PM
63	Design element(s) that makes the bridge unique.	9/7/2017 7:16 PM
64	Universal access/accessible design isn't mentioned anywhere - hope it's a given. Especially some of the pictures of other bridges at the beginning do not look universally accessible (e.g. query a spiral staircase?)	9/7/2017 6:20 PM
65	Make the Bridges and open land be TOURIST ATTRACTION.	9/7/2017 6:18 PM
66	None. Just a bridge.	9/7/2017 5:50 PM
67	I choose cafe, however, the restaurant on the St Boniface bridge does not seem to be doing well. It has to be improved upon to be successful.	9/7/2017 4:53 PM
68	more public bathrooms in the city in general would be nice. Especially near the homeless downtown.	9/7/2017 4:19 PM
69	If the other land use spaces can bring more population concentration to the area, the bridge may be an attractive pedestrian destination.	9/7/2017 3:29 PM
70	The bridge should be high enough to allow a future development of the train yard below to be turned into a highway. (We wouldn't want to have to rebuild the bridge, or have it prevent that kind of development; Winnipeg is in sore need of some inner-city freeways that divert traffic from having to go through Portage and Main, etc. People needing to go from one end of the city have to go through 20 sets of traffic lights, and tie up traffic in areas they don't even need to go to.)	9/7/2017 2:25 PM
71	Trash cans. I can't understand why you'd consider any plumbing-required features when we know our winter temps often creep below -40C.	9/7/2017 12:51 PM

## A Better Bridge for Arlington

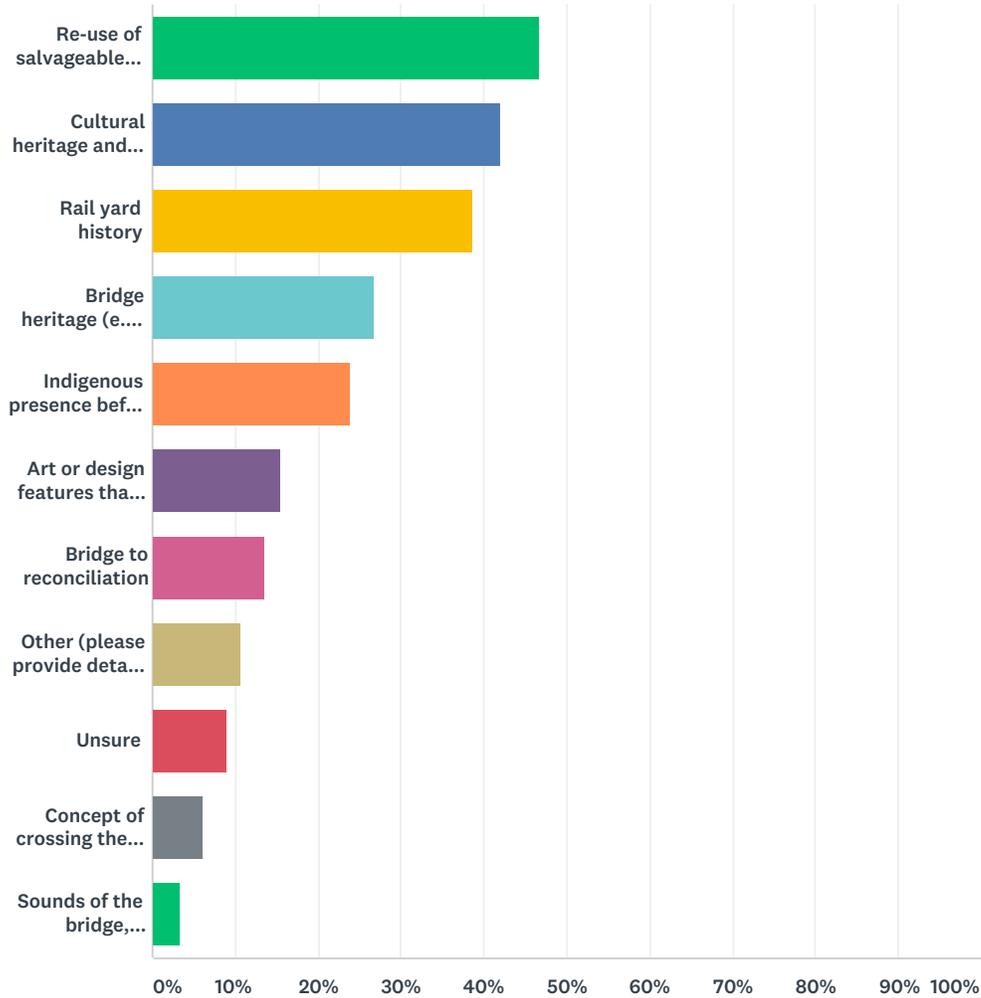
72	I travel across the Arlington bridge frequently and while the idea of having lookouts, benches, flowers, a cafe, etc. is great, I don't think it's a wise decision here. People use the bridge to cross over the railyards and connect to other parts of the city. There's not much of a view and the areas around the bridge are inflicted with high crime rates. Most people crossing on foot are not using the bridge for recreational purposes. I think the taxpayer money could be better spent elsewhere.	9/7/2017 10:04 AM
73	The bridge should be as small as possible.	9/6/2017 2:54 PM
74	Check out the New York City Highline. it is the poster child of what could be done with an old bridge, converting a former meatpacking district into an urban oasis.	9/6/2017 2:45 PM
75	We already have these features in Winnipeg. Why spend hundreds of millions in public debt on features that will be minimally utilized?	9/6/2017 7:34 AM
76	Nothing...was nothing there before doesn't need to be anything with the new one.	9/5/2017 1:13 PM
77	Keep it practical! Not really keen on any of these ideas. Flowers and plantings require maintenance and the City has difficulty keeping up with the work required.	9/5/2017 12:06 PM
78	its hard to imagine something on the bridge that would bring cafe's, selfie spots, or even lookouts for the train yards to the current or future bridge. the area needs more liveliness to support these features, but providing for now, so the bridge is 'future-proofed' makes sense. In my opinion, unless the area (north end community) does not want to make the area more lively, then these types of features would be wasteful and should be kept to views, some colour (flowers) and minimal lookouts.	9/5/2017 6:25 AM
79	Just make a bridge that people don't dawdle on. This would just attract drug dealers and other lxx lxxx to stay on the bridge making it unsafe anytime.	9/3/2017 11:23 PM
80	Bike Lanes	9/2/2017 4:42 PM
81	safety for walkers, bikes and cars good efficient traffic movement is still number 1	9/2/2017 9:56 AM
82	We are the centre port for railways and we should have a place of attraction in middle of bridge to identify and appreciate this.	9/1/2017 7:20 PM
83	Some safe method for the handicapped to get to the café, viewing.	9/1/2017 12:57 PM
84	Better traffic flow. Why on earth would you build a four lane bridge an only have three lanes going to it. What is the point? There are around 700 people that work at the CSCHAH. How will people be able to exit safely with only one lane on Arlington.	9/1/2017 6:58 AM
85	Its a bridge! Efficiently move vehicles/pedestrians/cyclists across railyards.	9/1/2017 6:53 AM
86	Hahahaha! Bathrooms on the Arlington bridge. Get rid of the tracks.	8/31/2017 9:13 PM
87	Mental Health resources - I don't know if as many people jump from Arlington, as the Redwood? But a public phone or some kind of messaging of hope or connection to the Crisis Line? Using the symbolism and history of the bridge as it connects the plight of poor Eastern European immigrant workers to the Indigenous folks?	8/31/2017 4:09 PM
88	Areas to hide from the wind (esp. for winter).	8/31/2017 3:43 PM
89	Placing a well put together engineering piece in the north end will help return pride back to the area. Part of this is incorporating bathrooms - Winnipeg is really lacking in public facilities (despite previous hardships), and finding a cost effective way to incorporate this aspect of cleanliness/hygiene for those in the area is also very important	8/31/2017 1:59 PM
90	Selfie spots??? that is ridiculous!	8/31/2017 12:43 PM
91	I've selected 4 ... #1 Lighting. Always safety #1. Everything else pales in importance. #2 Other: ART WORK .... have the bridge be interesting or pretty in some way. #3 Trainspotting (current bridge is good for this, except the mesh wires make it difficult sometimes to get the best photos) AND #3 "Selfie" spots (I don't know exactly what this means, but it sounds like a cool concept and "hip" ... anything photo related is supported) If I was to pick something else, it would be flowers / plantings. Please DO NOT put a bathroom on the bridge. Please DO NOT put a cafe on the bridge. Please BE CAREFUL about the final decision on benches & seating ... nice in theory to give people an opportunity to rest/enjoy the view/etc ... but also gives more people the opportunity to sit and wait to do bad things. When people are constantly moving it feels MUCH safer. For those who "linger" on the current bridge now (not often, but occasionally) it often seems like it is gang / drug related (meet for a pass, etc.).	8/31/2017 12:36 PM

## A Better Bridge for Arlington

92	realistically, most of us scoot over the present bridge asap as neighbourhood is sketchy. No buskers will come, no cafes will survive, without a complete change from being 'sketchy'	8/31/2017 12:23 PM
93	Design that mitigates wind when crossing on a bike or on foot in the winter.	8/31/2017 9:33 AM
94	Have the 'ceiling' of the bridge be a type of slat design so that it may deflect some of the heavy wet snow that falls in the winter. Crossing bridges in the winter time is very dangerous.	8/31/2017 8:19 AM
95	GARBAGE AND RECYCLING. this should be number 1. You want to keep that shiny new bridge clean, Yeah? Yeah. That's right. And put up signs throughout it saying to kindly dispose of your garbage instead of ruining the new bridge. Cause no one wants an ugly dirty bridge like what the current one is.	8/30/2017 9:04 PM
96	More lanes to get across faster is the biggest concerns	8/30/2017 6:30 PM
97	We don't need to replace the bridge. Abandon it and work on widening McPhillips underpass instead. Waste of money.	8/30/2017 5:51 PM
98	Feel safe to walk or ride.	8/30/2017 5:46 PM
99	Anything less than two lanes each way for vehicular traffic would be a failure.	8/30/2017 5:25 PM
100	These are strictly minor and as little money as possible should be spent on them.	8/30/2017 3:26 PM
101	Spend money on the passage of people, bikes, and cars, not hip trendy crap that will be empty and useless and an eyesore in years to come.	8/30/2017 3:02 PM
102	I don't see the bridge becoming much of a "destination" bridge for pedestrians. It will most likely be used much the same as now, a way to get to and from the north end, most often by vehicle, so I can't see the use for restaurants or busking stations. However, it would be nice to have nice plants, lighting and city views to see and make the drive or walk over the bridge more pleasant.	8/30/2017 2:51 PM
103	Two lanes on each going in both directions.	8/30/2017 2:16 PM
104	Something that celebrates the bridge itself as a major development. Not just the usual memorials to the past etc. The bridge itself is something to memorialise and be proud of, draw attention to it. Let people know about the engineering, the timeline, the parties involved, costs, design process etc.	8/30/2017 2:09 PM
105	CURB SEPARATED bike lane	8/30/2017 1:43 PM
106	the bridge is a long walk, and in many cases, people walk it as they don't have a means of transportation. Providing a spot for them to stop along the way would be beneficial. From the rendering flyover done, this bridge will be a nice spot to stop and look over the yards.	8/30/2017 1:26 PM
107	It would be best to make a proper four lane bridge, rather than waste on fluff. It would be better to accept that this short-sighted project will have a poor outcome, cancel it and make the connection between Sherbrook and McGregor the top priority. The poor design and negative impacts of the Arlington project just don't make a case to continue with it.	8/30/2017 12:36 PM
108	I have a 2 year old and he is obsessed with trains. Every time we drive over the Arlington bridge he gets very excited. It would be amazing to have space to walk, sit and watch the trains - that is also safe for kids and their parents/guardians.	8/30/2017 12:31 PM
109	Consider the neighbourhood and the people who would actually use the bridge as pedestrians. This is not a tourist area. There is no point wasting money trying to make the Arlington bridge a "destination". However the people who live here probably would appreciate a public washroom as it's a long walk over the rail yards.	8/30/2017 11:45 AM
110	I drive over bridge twice a day. I will not be using it for any other thing so above is unimportant	8/30/2017 11:16 AM

## Q4 What themes do you want to see reflected in public art on or around the bridge? (Select up to 3.)

Answered: 499 Skipped: 8



ANSWER CHOICES	RESPONSES	
Re-use of salvageable pieces of the old bridge	46.69%	233
Cultural heritage and history of the neighbourhoods - past and present	42.08%	210
Rail yard history	38.68%	193
Bridge heritage (e.g. urban myth about the bridge originally being built for the Nile River)	26.85%	134
Indigenous presence before European settlement of area	23.85%	119
Art or design features that raise awareness of Missing and Murdered Indigenous Women and Girls	15.43%	77
Bridge to reconciliation	13.63%	68
Other (please provide details below)	10.62%	53
Unsure	9.02%	45

## A Better Bridge for Arlington

Concept of crossing the bridge as a "journey" requiring endurance	6.01%	30
Sounds of the bridge, traffic, railyards	3.41%	17
Total Respondents: 499		

#	COMMENTS	DATE
1	Art is best left to the artists. You can suggest themes but it usually doesn't work out as well compared to when you leave things open. Instead encourage artists to ask questions. Be truly creative.	9/30/2017 4:06 PM
2	Just keep it simple don't spend money other than make it last and minimal maintenance costs	9/30/2017 1:24 PM
3	Oh boy, we're really going to town here aren't we. I didn't realize we were running such a large surplus. Can I get my taxes lowered, please??	9/30/2017 2:58 AM
4	The yards have always divided by class rather than by race. In my view, speaking about class division solely through the current Indigenous/European manifestation limits our true understanding of yards.	9/29/2017 9:56 AM
5	Embrace the rail yard. Don't make this a spot of contention between ethnic groups.	9/24/2017 1:25 PM
6	Remembrance of fallen heroes from Winnipeg communities.	9/23/2017 2:19 PM
7	It is nice to have some sort of statement for people to read about the history of the bridge and the area, but don't spend too much money on that part of it as only a small percentage of the people crossing the bridge are going to read the plaques etc. Only the foot traffic will have the opportunity to see the plaques. People in cars are not going to stop to read it. Art created from usable pieces of the old bridge does sound very interesting and will be something for people to talk about, this will be more visible to all the traffic crossing the bridge.	9/23/2017 8:58 AM
8	These arts are not needed, I suggest to put the funds to the construction of the bridge for durability and safety	9/23/2017 12:58 AM
9	The North end does not need anymore public money spent on aboriginal special interest art. The bridge is used by all people and their history of the neighbourhood is equally important.	9/22/2017 3:41 PM
10	If the rail yard can't be moved, acknowledgement of the presence of Indigenous peoples here that shows the strength and complexity pre-existing societies in a realistic way would be nice. If history is incorporated, how about the true history of how the land was taken	9/21/2017 3:38 PM
11	See my comments at Q1	9/21/2017 2:56 PM
12	None please.	9/21/2017 10:46 AM
13	Cultural heritage should include the concept of Indigenous presence before European settlement of area. I really love the ethos and symbolic power of the "bridge to reconciliation". We have a lot of work to do, and bridging the south and north ends of the city is a key link to this important process. This could also include art and design or interpretation about Mission and Murdered Indigenous Women, and how this "Journey" or reconciliation will require openness and endurance for our cultures to become more connected.	9/21/2017 8:59 AM
14	Again, doing things like this cost effectively might be okay. I really wonder at trying to make this bridge a monument to reconciliation or missing and murdered indigenous women . . . just because it's in the North End does not mean it has to be the "poster child" for every issue. And whatever you end up doing, make it indestructible to violence and graffiti. Even if you cover every square inch with cameras there will be those wanting to tear it all down and/or destroy it before its even built.	9/20/2017 12:50 PM
15	Reduce, reuse and recycle as much of the bridge as possible. A cultural experience and still be obtain from the use of the old bridge.	9/19/2017 2:05 PM
16	-Re-use of salvageable pieces of the old bridge -Indigenous presence before European settlement of area -Cultural heritage of neighbourhoods- past and present -Bridge to reconciliation b. acutely aware: no reminder needed i. theme that draw attention together not divide	9/18/2017 12:17 PM
17	-Re-use of salvageable pieces of the old bridge -Art of design features that raise awareness of Missing and Murdered Indigenous Women and Girls -Indigenous presence before European settlement of area with the bridge -Local artists	9/18/2017 12:08 PM

## A Better Bridge for Arlington

18	-Art or design features that raise awareness of Missing and Murdered Indigenous Women and Girls -Indigenous presence before European settlement of the area with the bridge -Bridge to reconciliation Treaty acknowledgement/we are all treaty people, focus on indigenous themes/reconciliation/Canada of today	9/18/2017 12:00 PM
19	Blend of more than 1 theme	9/18/2017 11:40 AM
20	Indigenous should be a focus diversity	9/18/2017 11:37 AM
21	-Multicultural Winnipeg -Spots for different cultural history	9/18/2017 10:31 AM
22	-Sport figures in the area -Indigenous local artists -Graffiti	9/18/2017 10:11 AM
23	the artwork shouldn't be reflective of just one culture. The North End is made up of many different people with many different cultures and backgrounds, Indigenous, Polish, Ukrainian, Irish, etc. Pictures or artwork should be reflective of people not one person or culture.	9/16/2017 9:56 AM
24	Best value for money. Keep it cheap.	9/15/2017 3:23 PM
25	I encourage Art that includes the visions of our youth & Art that reflects the Original Peoples of Winnipeg/ Manitoba, including the Metis Nation. Reconciliation is a complex matter that requires serious attention and action. A bridge to reconciliation could serve to diminish its importance and exist as a romanticized gesture to placate a very serious and tragic Canadian past & present.	9/15/2017 3:19 PM
26	Spifying up the bridge with "thing" will enhance traffic movement how?	9/15/2017 11:15 AM
27	I don't think a bridge is even necessary.	9/15/2017 9:38 AM
28	NO BRIDGE <a href="http://www.cbc.ca/news/canada/manitoba/railway-move-winnipeg-1.3321053">http://www.cbc.ca/news/canada/manitoba/railway-move-winnipeg-1.3321053</a> remove the rail yard	9/13/2017 11:35 AM
29	Anyone who has ever had to walk up, across and down Arlington bridge more than once in a day will agree that the journey concept will be laughable but also something people will find relevant lol	9/12/2017 9:13 PM
30	Joining of two communities, showing their diversity something that is multicultural to arm And the test of time	9/12/2017 4:42 PM
31	I think the theme of HOPE would be fitting, HOPE encouraged as you go south and as you go north.	9/12/2017 1:30 PM
32	not a priority	9/12/2017 7:43 AM
33	make it easier for people without cars, make it less dangerous in winter , keep welcome to the north end theme going..many nationalities, much wonderful history support local businesses	9/12/2017 1:54 AM
34	I am against public art here. This is an ugly industrial area. And it isn't of any significant historic interest either.	9/11/2017 2:06 PM
35	Again not important put a plaque up but get some free or volunteer artists to do a graffiti mural or natural art installation. Or something that can be added later on with more thought. It's a bridge doubt it will be a tourist attraction it just literally gets us from one side to the other. Period.	9/10/2017 6:03 PM
36	Again, none of this is necessary if old bridge is left and converted to something similar to the high line in New York. Instant character on top of any art projects added. Demolition of old bridge costs are zero. Beautifying the new bridge costs are zero. Money for both can be used for costs to repurposing old bridge, or frankly coming in underbudget.	9/10/2017 5:28 PM
37	The bridge should reflect ALL historical cultures that lived in the North End from it's start, which consisted mostly of settlers, not just Indigenous. There were very FEW Indigenous people when I was growing up in the North End.	9/10/2017 4:26 PM
38	Instead of demolishing Arlington Bridge, imagine it repurposed as an amazing Winnipeg public space.	9/8/2017 8:15 PM
39	Reserve all these great ideas for the old bridge by turning it into a pedestrian only public space walkway (eg high line in New York). New bridge can be beautiful from afar, but doesn't need amenities like these. Focus the new bridge for moving people, and preserve the old bridge for the people.	9/8/2017 3:57 PM
40	PLEASE do not demolish the old bridge. This is an amazing opportunity to use an existing structure to create a practical linear park (like the highline) for pedestrians and cyclists. I believe this is the most important aspect of the redevelopment, and I think we would really regret demolishing it.	9/8/2017 1:46 PM

## A Better Bridge for Arlington

41	Please combine the Indigenous historical presence with the bridge to reconciliation as a dominant artistic theme on both sides of the bridge entrance/exit. Include pieces of the rail yard history as the iron horse (positively & negatively) ties into both Indigenous history & the reconciliation journey.	9/7/2017 10:04 PM
42	Just build a bridge and don't over think it.	9/7/2017 9:35 PM
43	What 100 years passing can accomplish: The view of the past (first) 100 years: changed cityscapes, changed pressures, changed world views (from colonist to reconciliation) And the hope for the next 100 years: strong & diverse communities; engaged, active and healthy citizens	9/7/2017 9:10 PM
44	Pictures of how great that neighbourhood was before it fell to drugs and alcohol.	9/7/2017 8:43 PM
45	incorporate the whole bridge tarring it down is a waste of tax payer money and a major missed opportunity!	9/7/2017 8:20 PM
46	I hope the designs and art of the new bridge evoke a very modern, futuristic aesthetic. Something almost "space"-like that brings it into the next century. Looking to the past, at history, is passé. We have enough historical art around the city. If we keep the old bridge as an urban garden/walkway, like the High Line in New York, that would be better. That way we have an actual piece of history preserved and renewed, which is miles ahead of an art installation that acts as a tribute to it. It could serve a new life as a pedestrian walkway filled with grass, seating, and plant life. How beautiful!	9/7/2017 8:19 PM
47	Hire Kenneth Lavallee to paint it	9/7/2017 7:30 PM
48	Modern. Lighting	9/7/2017 7:16 PM
49	I include indigenous history in my cultural heritage and history of the neighbourhood choice, as well as immigrant settlers.	9/7/2017 6:20 PM
50	None. Just a bridge.	9/7/2017 5:50 PM
51	Graffiti	9/7/2017 4:19 PM
52	Expensive art is for the wealthy. Winnipeg is not wealthy.	9/7/2017 2:25 PM
53	If the longterm plan is to relocate the railyard anyway, why would the bridge need reference to it. A historic plaque about the old bridge would be enough. Build it strong and safe. Put the earrings on when its paid off.	9/7/2017 12:51 PM
54	No themes, just a multi functional bridge.	9/7/2017 12:30 PM
55	There should not be any art. The current bridge is used by wildlife.	9/6/2017 2:54 PM
56	We already have plenty of underutilized artistic displays.	9/6/2017 7:34 AM
57	None. This is not a popular place for people to go out and take a walk	9/5/2017 1:13 PM
58	re-using pieces of the old bridge to transform into art, sculptures, even statues would be really neat. local artists could be provided the metal, bolts, fencing, etc and have them create new iconic pieces for the foot of each side of the bridge, maybe even the midpoint of and other significant milestones. Maybe the art could tell the history or heritage. why does the new bridge have to relate to indigenous? regardless of the treaty land? this is a bridge similar to any other bridge in Winnipeg. Does the Disraeli bridge have European themes to bridge to the Kildonan/Elmwood area that was predominantly European immigrants or even to indigenous claims? I do not think placing the area of North end to only indigenous presence helps the community or this bridge. the bridge should be left neutral from any claims and leave the artwork and history speak for itself.	9/5/2017 6:25 AM
59	Steel and concrete. Just get it down at cheap as possible, it's not a tourist attraction, it's a bridge from point A to B. Don't go crazy on it.	9/3/2017 11:23 PM
60	wow, all great ideas I want to select more than 3 thus lots of public art and education along the bridge, please	9/2/2017 9:56 AM
61	Important to highlight the vibrant community with the different ethics and cultures!	9/1/2017 7:20 PM
62	As this bridge will be there for at least seventy-five years we should include major recognition of the Treaty but also of the immigrants who were the builders of the North End. Including murdered and missing indigenous women and girls is the current problem we are facing. What about the lack of water that caused thousands of deaths in the North End due to feces in the ground water from the wells. Do we want to highlight that? I think not.	9/1/2017 12:57 PM

## A Better Bridge for Arlington

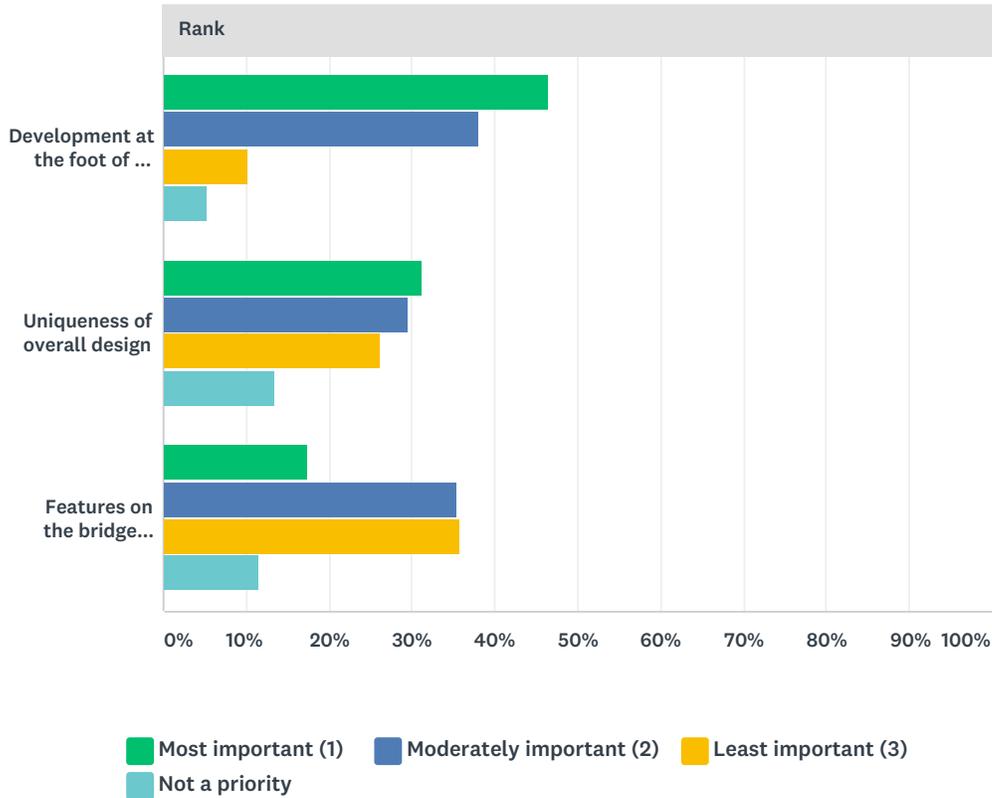
63	kal barteski art	9/1/2017 12:53 PM
64	I believe the bridge should showcase the history of the area and the past bridge as it is/was an important part of this end of town.	9/1/2017 10:29 AM
65	Not required,	9/1/2017 6:53 AM
66	The term "Polishing a turd" comes to mind. Get rid of the tracks.	8/31/2017 9:13 PM
67	Incorporating various themes would be awesome !	8/31/2017 9:11 PM
68	It's nice to acknowledge the history of the area, but I think it's most important to capture a sense of moving FORWARD.	8/31/2017 4:26 PM
69	Using the symbolism and history of the bridge as it connects the plight of poor Eastern European immigrant workers to the Indigenous folks? A site of resistance - with some General Strike info and some Idle No More info.	8/31/2017 4:09 PM
70	Keeping the bridge connected to its roots including the heritage of the area, incorporating the original bridge, and its historical importance including the CPP rail line is an important aspect of Winnipeg's history as a shipping hub	8/31/2017 1:59 PM
71	trains - metal cut outs	8/31/2017 1:17 PM
72	I've selected Rail Yard History as part of the top 3 .... I don't know that captures exactly what I'm thinking vs. what you're thinking. But some nod to the rail yard (given that's the reason FOR the bridge in the first place!) I think would be good. But an entire history of the rail yard ... no. The North End is amazing because it combines SO MANY different cultures ... living here is a day-to-day Folklorama, which is amazing and why my family chooses to live here vs. other areas of the city! If that "Folklorama" can be wrapped up in beautiful artwork, that could be nice. Maybe there could be a small "Bear Clan Patrol" nod in the artwork as well?	8/31/2017 12:36 PM
73	It is very important to acknowledge the first peoples. Honouring them with traditional artwork is a must. And please incorporate pieces of the old bridge. It's heartbreaking it has to go in the first place. Winnipeg is an old city. We pride ourselves on maintaining resilience of our historic buildings and landmarks.	8/31/2017 8:19 AM
74	COMMENT REMOVED	8/30/2017 9:23 PM
75	COMMENT REMOVED	8/30/2017 9:04 PM
76	It's a bridge. It doesn't need "public art".	8/30/2017 7:58 PM
77	We don't need some hokey and politically popular symbolism. We need a well designed and beautiful bridge that does a basic function. A bridge is not going to solve our city's problems and it's laughable to think we'll heal the past wrong doings with a bridge. Give you head a shake.	8/30/2017 7:57 PM
78	None	8/30/2017 5:51 PM
79	Another minor feature; in fact, it's even less important than the features on the bridge.	8/30/2017 3:26 PM
80	Oh my god. These suggestions are insane. Leave the history stuff to the museum's. This isn't a tourist or neighbourhood bridge, this is a commuter bridge that would be completely unnecessary if the rail yards weren't there. No art, just lighting.	8/30/2017 3:02 PM
81	It would be nice to have art representing all the major cultures that have figured in the north end's history, eg, Aboriginal, Scottish, Ukrainian, Polish, Jewish, etc, etc.	8/30/2017 2:51 PM
82	Save money by minimizing artwork. I would prefer no artwork or information.	8/30/2017 2:50 PM
83	I Would like to see Rail Yard History..and I guess some sort of indigenous stuff otherwise it'll get burnt down	8/30/2017 2:26 PM
84	Whatever art the city ends up installing please don't do something like Bears On Broadway. Or the giant marbles. Or heart touching murals. Please commit to something finer and more creative than that kindergarten classroom art. Please.	8/30/2017 2:09 PM

## A Better Bridge for Arlington

85	Why even waste our time asking. You already know, as much like everything else in our country at present time, that absolutely everything is geared towards Indigenous culture. (Is Indigenous still politically correct or has it changed in the past 12hrs?)	8/30/2017 1:05 PM
86	Using this kind of distraction while offering up cookie cutter design, is just not a sound practice. Why was everything learned through the public consultations simply discarded in favour of predetermined back room arrangements?	8/30/2017 12:36 PM
87	Unimportant	8/30/2017 11:16 AM

Q5 Now, thinking about the aspects of the project we've asked you about today, please rank from highest to lowest, which you feel is most important. If you believe one or all of the items are unimportant, please select "not a priority" from the drop down list.

Answered: 496 Skipped: 11



Rank	MOST IMPORTANT (1)	MODERATELY IMPORTANT (2)	LEAST IMPORTANT (3)	NOT A PRIORITY	TOTAL
Development at the foot of the bridge	46.47% 224	38.17% 184	10.17% 49	5.19% 25	482
Uniqueness of overall design	31.07% 151	29.42% 143	26.13% 127	13.37% 65	486
Features on the bridge (such as benches or lookouts)	17.32% 84	35.26% 171	35.88% 174	11.55% 56	485

#	WHY?	DATE
1	It will help inform us about the area	9/30/2017 4:22 PM

## A Better Bridge for Arlington

2	<p>This ranking is a bit odd. Benches are an essential part of the project from the perspective of an elderly person or a city that sees walking as essential. We don't feel they should be included here for the same reason that you wouldn't ask someone to prioritize the bridge deck or the abutments, it just doesn't make sense if both are integral to what a bridge is and does. That's why it is ranked first. Development at the foot of a bridge is not just necessary, it is natural. A chokepoint, like a corner lot or the hot dog stand near the metro station door, will always be desirable. Key is to make sure that the right kind of development goes in. We want fun and real places, not drive-thrus. Uniqueness is an indication that the right people were asking the right questions at the right time, so desirable, but uniqueness for the sake of uniqueness is unwise. Rather, ask people to ask the right questions and if the questions are new or boundaries are being pushed, then uniqueness might just naturally happen. In an era where we have mostly been doing transportation wrong, it will likely be enough to get the process right, to look for designers who know how to design for human beings instead of machines, and to ask the designer to really probe into the heart of the matter. What will most likely emerge is something special, and indeed unique, but it may only be unique to us and some functional elements are so beautiful and simple that they stand on their own. In short, these rankings seem like they require nuance, and we just want to make sure that we aren't creating a kitsch attraction that provides cover for more of the same automobile focused planning purely for the sale of it.</p>	9/30/2017 4:06 PM
3	Just keep it simple and don't overspend	9/30/2017 1:24 PM
4	I placed development at the bottom of the list largely because I find it difficult to imagine development at the corner of 2 major thoroughfares. It is meant to move traffic, and not provide an activity hub. I would like to be proven wrong.	9/30/2017 7:31 AM
5	The design should be built around the planned developments at the foot. Also, foot traffic needs to be considered with SAFETY measures (prevent jumpers)	9/30/2017 6:00 AM
6	Traffic flow is most important. Nobody likes being in traffic.	9/30/2017 2:58 AM
7	The area surrounding the bridge will determine the overall environment of the bridge and area, this should be the priority as having an unappealing environment won't encourage people to use it.	9/29/2017 12:06 PM
8	Foot of bridge development helps build a tax base to sustain the costs of the bridge but also reaffirms that bridges shouldn't just be a place to pass from A to B (by non locals) but a connection between communities over an impassable barrier.	9/29/2017 11:45 AM
9	It is going to become part of the skyline	9/29/2017 10:38 AM
10	I really think the existing bridge is beautiful. I would be disappointed if we spent a lot of money to build something unique while destroying something so beautiful.	9/29/2017 9:56 AM
11	For me, I think the most important thing is safety. In order for pedestrians to come to the bridge and cross it, they need to feel safe. This starts with the surrounding area.	9/29/2017 9:27 AM
12	We need to utilize the neighborhood and take the streets back. I'm sick of the Arlington bridge area being so sxxxx. Let's make it beautiful	9/28/2017 9:56 PM
13	The bridge has to be functional first. What the best design for the dollar spent.	9/28/2017 9:50 PM
14	I believe functionality is important, but the North End is established with businesses so I believe the Arlington bridge should in fact resemble something congruent to the new pedestrian-friendly Henderson Highway bridge	9/28/2017 9:38 PM
15	The bridge will be used regardless of the uniqueness of the design. I feel putting more effort on developing the area surrounding the bridge could have a positive impact on the community i.e. bring small businesses in, rec. centre, etc. will encourage people to spend time in the area rather than just pass through.	9/28/2017 4:29 PM
16	development of outside/beside the bridge to best suit/ limit impact on community	9/28/2017 1:27 PM
17	The bridge is a transportation route between the "North End" and the "West End" of the city. This is the purpose of the bridge and always has been. Features may look nice, but are not a necessary expense. I am, however, pleased that a new bridge at Arlington was a first choice rather than one at MacGregor, which is too close to Salter Street.	9/26/2017 2:50 PM
18		9/26/2017 5:52 AM

## A Better Bridge for Arlington

19	All aspects are important, but I think the project should focus on the bridge's construction as a functional aesthetically-pleasing symbol for the area. Future private and/or public investment can occur to develop the foot of the bridge once we learn more about how the new bridge affects traffic patterns and pedestrian use.	9/24/2017 7:50 PM
20	The other features associated with the bridge, even if more ordinary, will be that much more impressive if the underlying bridge design stands out / is unique.	9/23/2017 5:22 PM
21	I think developing community in the area around the bridge will help the neighbourhood the most.	9/23/2017 4:47 PM
22	a unique design would be something the neighbourhood could be proud of; would increase the beauty of the area if done well/ pleasing way Wherever green space or more green ie. trees/ plants/planters, river rock etc. can be added - this too will add to the aesthetics. This area is generally lacking in pleasing green space except for the Old Ex parkland....good lighting in the evening will be essential to address safety concerns for any area designed to bring people to gather...	9/23/2017 12:05 PM
23	The areas around the bridge are the most important as they will unify the whole project. If the railyards are still going to be there, it will be a hard task to make improvements that will make you forget they are there. Using the area to it's fullest potential is extremely important. Areas for kids and families to go to would be great. Parks, recreational facilities, to keep the kids out of trouble are great ideas, like the skate park, but those also come with safety considerations. I think a police patrol of the area would be needed on a regular basis so that the area is not taken over by gangs, homeless people, or thugs. Once the area is seen as secure maybe then people will feel safe to use these areas. It doesn't matter how unique the design of the bridge is if the area will not be used. We have some real eyesores in this city already, let's not have the Arlington Bridge be another one.	9/23/2017 8:58 AM
24	Since this is our oldest bridge, let us build a strong foundation and unique design	9/23/2017 12:58 AM
25	Make a place for people and not just cars	9/22/2017 3:27 PM
26	The bridge must maintain if not improve the lives of those around it. NOT makes them feel unsafe and/or make it more complicated to get from point a to b.	9/22/2017 12:30 PM
27	I think all three are very important	9/21/2017 8:11 PM
28	The rail yards should be moved out ASAP	9/21/2017 3:38 PM
29	I think all are important. But development at the foot is key to help ensure pedestrian traffic, who will then benefit from the features on the bridge. The bridge is in a unique location, with an interesting view of the skyline -- it also needs a unique overall design to show to the neighbourhood, and the entire city, that the west and north ends are important to the city.	9/21/2017 9:06 AM
30	Development at the gateways to the bridge has the potential to create very important green, naturalized areas in an industrial area without access to a lot of nature... vital to core areas! The features on the bridge will make the bridge a destination and a pleasure to cross. The design will be unique if these other factors are taken into consideration.	9/21/2017 8:59 AM
31	I'm tired of many people wanting some shining example of architectural genius that will have people flocking from around the world to see it. A bridge that will give everyone in the North End what they want. The reality is that though I have lived in the North End since 1994 and I honestly love it to bits and choose to live here, hoping to make small changes every day in my neighbourhood - I am well aware of how things work here. Many of the people demanding all the bells and whistles on this bridge have no concept of what things cost when it comes to construction, maintenance, etc. They don't pay the taxes that will fund this and really don't care how much it will cost taxpayers in the future. I do. I care that roads are crumbling, our medicare and health system is at a crisis level and that costs are rising, utility costs are accelerating faster than the few raises people are lucky enough to get, job security is a joke . . . When designing this bridge care and attention should go to being fiscally responsible and having a vision to the long term costs to maintain a tool to get us from point A to B. I had the same opinion about the last two bridges and instead our government blew the bank. Looking back, did they generate any cash or increased tourism to offset the horrendous amount of money spent? That would be a resounding NO. Now is the time to stop doubling down on the stupidity and waste and build something that is solid and sound and gets the job done for a long time with minimal maintenance. That would be a lasting legacy to all Winnipeggers.	9/20/2017 12:50 PM

## A Better Bridge for Arlington

32	By not building a bridge, you don't need all of the frivolous items such as lookouts, benches, etc. Keep the cost down. The purpose is to get traffic and people moving. I think all the luxuries will push the cost up. Whenever a new road is built, a protected pedestrian cycle and walkway requires to be built beside it. Instead of having sidewalks, build pedestrian ways. Make it 3/4 the size of a traffic lane. Leave lots of room for cyclists and people to walk.	9/19/2017 2:05 PM
33	It could help develop the area and surrounding neighbourhood	9/18/2017 6:02 PM
34	Escalator	9/18/2017 11:45 AM
35	-Develop bridge for 50 years from now -Restrain on spending -Unique but reasonable spending - Keep it on a budget	9/18/2017 11:37 AM
36	Want this to be a city that people want to visit	9/18/2017 10:55 AM
37	Lights	9/18/2017 10:19 AM
38	Lighting unique	9/18/2017 10:11 AM
39	I want Winnipeg to be different. We always copy ideas from other cities and quite frankly we suck at coming up with original ideas.	9/18/2017 7:58 AM
40	We want a bridge that gets us to where we are going fast and safe. With HSC and surrounding medical facilities growing the increase in traffic both patients and visitors as well as those employed in the area has increased substantially. Traffic over the bridge currently is very slow especially during rush hour. As well a lot of people in the area don't have a vehicle and either ride their bike, walk or take the bus. The bridge needs to provide these people a safe route to make their way to facilities such as HSC or to the downtown area. Right now with the walkway is narrow and caged people feel trapped and unsafe. As mentioned above driving is slow, one lane each direction is not sufficient.	9/16/2017 9:56 AM
41	The development at the foot of the bridge will either attract or keep people from the North End; provide additional employment opportunities for the neighbourhood.	9/16/2017 9:41 AM
42	Safety is a real concern in this area; I'm more concerned with bringing in reputable businesses and keeping the area attractive, well lit and fostering pride and safety than spending money to make it unique or a landmark.	9/16/2017 9:37 AM
43	They're all equally important	9/16/2017 8:51 AM
44	Lowest cost is most important to the people paying for it.	9/15/2017 3:23 PM
45	I think that it is important to design a bridge that focuses on the strengths, brilliance and wisdom of the community and residents around it. It should be one that promotes light and pride in Winnipeg.	9/15/2017 3:19 PM
46	An eye pleasing unique structure is useless if it doesn't do the job of moving traffic. Nowhere in your survey is mention made of the lanes of traffic north and south bound and any parking implications. I would like to hear more on this topic!	9/15/2017 11:15 AM
47	Money. Property taxes too high now and unnecessary projects like this increase them more. Now MB Gov wants to add levy for Health Care. It gets harder and harder to afford the fancy brick, mortar and Tyndall stone structures from my straw hut tax base.	9/15/2017 9:38 AM
48	Let's make the bridge somewhere people want to come and enjoy using! Then let's also bring more enjoyment to the surrounding areas!	9/14/2017 7:27 PM
49	Safety around that bridge is the highest concern.	9/13/2017 2:51 PM
50	Development at the foot of the bridge will attract a specific demographic and bring more to an area that desperately needs revitalization. A unique bridge design could put Winnipeg on the map architecturally; attracting more interest in the city by others looking to expand their businesses and put down routes. It will also harbour curiosity in people, hopefully leading to an increase in tourism and revenue for our growing city. Features on the bridge such as benches,etc...would be great, but they are not necessary. If the development at the foot of the bridge incorporates a park then there would not be an immediate need for places to sit on the bridge. One could somehow incorporate a lookout area in a park..it isn't a priority in my opinion...as long as these features could be installed elsewhere.	9/13/2017 11:41 AM
51	NO BRIDGE remove the rail yard <a href="http://www.cbc.ca/news/canada/manitoba/railway-move-winnipeg-1.3321053">http://www.cbc.ca/news/canada/manitoba/railway-move-winnipeg-1.3321053</a>	9/13/2017 11:35 AM
52	Function is more important than form.	9/13/2017 8:59 AM

## A Better Bridge for Arlington

53	Because the features of the bridge can be incorporated into the uniqueness of the design as "flex spaces" without putting that portion as a priority. I think if the design is unique, it will dictate what will develop successfully at the foot of each end, and once those areas are figured out those "flex spaces" can truly thrive.	9/12/2017 9:13 PM
54	since it will be rebuild--it should be something we are proud of in the years to come -plus green space at both ends used properly will also enhance the look , benches lookouts fancy ornamental lighting all will help with the look	9/12/2017 5:49 PM
55	I have no doubt that whatever design is chosen, it will be fine - the need is for community building. This is far more important than uniqueness. We want to celebrate unity and reconciliation, not focusing on difference (what's unique) and "best".	9/12/2017 1:40 PM
56	People like to look at the trains and views so look offs are useful. Preventing 'jumpers' from using the bridge may be more discreetly done by using glass, not chain link fence. Rerouting Dufferin under the north side would be wise (and shops, walks, plantings and traffic and businesses can carry on without stopping for bridge traffic).	9/12/2017 1:30 PM
57	this area of town needs lots more green park like space that is safe and accessible. Open outdoor space for playing, gardening,etc.	9/12/2017 8:38 AM
58	just want to get where I'm going and have improved traffic flow. I have been walking around that area extensively and it's not safe and it's dirty and unkempt ie broken beer bottles. . Until that is improved in the area it won't matter what it looks like, I'm not going sight seeing. This is a poverty stricken area - I just want to get home without sitting in traffic.	9/12/2017 7:43 AM
59	don't forget it's vital for connecting thousands every day. it's about traffic flow. dangerous if benches on bridge..suicide etc.	9/12/2017 1:54 AM
60	As long as the bridges functional for its needs being unique isn't important the foot of the bridge need to be safe so people will access it and then you could put benches on it	9/11/2017 10:30 PM
61	Keep within a reasonable budget, concentrate on safety, and lighting.	9/11/2017 6:01 PM
62	Save taxpayers money and reduce running and maintenance costs...	9/11/2017 2:06 PM
63	Uniqueness of the design will be important, but due to the rail yards and other larger industrial businesses, it's not an area that many people frequent for artistic or architectural sightseeing. It's a gateway over the rail yards first and foremost, instead of having to travel to McPhillips or Salter. If you simply replaced the bridge with an exact replica, I don't think anyone would mind.	9/11/2017 12:51 PM
64	Because	9/10/2017 6:04 PM
65	Just get er done. The logistics of traffic and pedestrian flow while the bridge is out of commission will be taxing on people's time and commute as well as the other roads across bridge Mc Phillips and salter. The sooner it's done everyone will be happy regardless of what it looks like. People now can stop and look at the trains. No ones going to camp out there all day and listen to busters except people that you don't want loitering around that have no money to give.	9/10/2017 6:03 PM
66	The most unique thing that could be done would be to put a new bridge curving slightly west of the old bridge, allowing for the best skyline views of our city on the current but hopefully repurposed historical bridge.	9/10/2017 5:28 PM
67	People travel on the bridge because of necessity, why not have people go to the bridge because they want to?	9/10/2017 4:26 PM
68	To encourage tourists to visit winnipeg	9/10/2017 12:06 PM
69	This is an opportunity to improve the north end neighbourhoods so adversely impacted by the presence of the rail lines. Let's prioritize that over the pleasure of car commuters from the suburbs.	9/10/2017 9:09 AM
70	As a resident of the North end, I would like the bridge to be unique without feeling the need to address any racial or cultural issues. I want the bridge to be something to be proud of. Something which at the end of the day I can be happy my tax dollars helped pay for. The number one thing I would like to avoid is giving anyone the chance to use the space(s) provided to sleep off a long night, or simply loiter around bothering other users of the bridge. I would like the bridge to be wide (4 lanes) and be very well eliminated, and uniquely lit.	9/9/2017 11:03 PM
71	The bridge itself needs to be unique yet represent its original "being"	9/9/2017 10:44 PM
72	I want the bridge to be a destination, to make the north end more appealing to everyone instead of it being seen as a negative place. I want it to be something people can be proud of!!	9/9/2017 8:24 PM

## A Better Bridge for Arlington

73	Development of the space around the bridge will support revitalization of the community. Support and encourage families of the community and create a sense of belonging.	9/9/2017 7:35 PM
74	I would like the end result to be a net benefit to the community. Play/green spaces would do that more than a fancy bridge. That being said, I appreciate and applaud the symbolism of a "Bridge to reconciliation, commemorating MMIWG.	9/9/2017 11:13 AM
75	For those who use the bridge as pedestrians or cyclists, especially those with no other transportation options, having the journey be enjoyable and with options for rest can improve quality of life. Development at the foot of the bridge is important to ensure that no more 'dead space' or under utilized space adds to the uncared for feeling that many in the North End feel.	9/9/2017 10:41 AM
76	All three are essential! It's most important to ensure that the land uses and urban design contribute positively to the communities on either side and support safe and healthy community activity that strengthens the safety/security and health of its community members.	9/9/2017 7:21 AM
77	Note: uniqueness should not be a substitute of GOOD, functional design. Make the bridge work for all users, not just cars. Be sure to understand the needs of the people who live there and the people who would be walking or cycling over it regularly. Make it COMFORTABLE and EXPERIENTIAL, not just an engineering solution to streamline traffic with some flashy ornament on top. Make it a bridge that people want use, and that will inherently make it a source of city pride.	9/9/2017 12:53 AM
78	Because it's not going to be a tourist destination like the esplanade bridge. It's seen as unsafe.	9/8/2017 9:54 PM
79	Instead of demolishing Arlington Bridge, imagine it repurposed as an amazing Winnipeg public space. \	9/8/2017 8:15 PM
80	At the end of the day, the bridge is a tool to get people, bikes, cars, etc. safely from point A to point B. The usability of it must be paramount.	9/8/2017 6:55 PM
81	Reserve all these great ideas for the old bridge by turning it into a pedestrian only public space walkway (eg high line in New York). New bridge can be beautiful from afar, but doesn't need amenities like these. Focus the new bridge for moving people, and preserve the old bridge for the people.	9/8/2017 3:57 PM
82	The bridge redevelopment has the potential to enhance and better connect its surrounding neighbourhoods, and it's essential that the features of the bridge are relevant to the community.	9/8/2017 2:03 PM
83	The bridge doesn't need to be a showpiece, but it does need to be a pleasing, timeless design. I would love to see some mix-used development at the foot of the bridge. Very strategic location for the north end.	9/8/2017 1:46 PM
84	the style and architectural/engineering design of the bridge is what will stand out more than anything	9/8/2017 11:25 AM
85	Truthfully I wish the rail yard would have been relocated to help us become a more modern city. But if we must have a bridge, as long as it's safe, functional and looks half decent while being kept to the lowest cost I believe that would be best.	9/8/2017 11:23 AM
86	I'd rather it be a functional bridge that makes things better rather than spend your time making a nice looking bridge that sucks	9/8/2017 11:21 AM
87	Not just a transportation or access point but a Destination. reason to bring people to the area	9/8/2017 11:18 AM
88	As I mentioned previously, walking along the bridge is a long walk and especially during the winter months. Anything to make the walk more endurable will encourage people to walk more.	9/8/2017 10:28 AM
89	we need safety first and foremost, this is a high crime area	9/8/2017 7:51 AM
90	All of these must be tied together as they will determine the overall message that Can be conveyed by a well designed and dynamic structure. I can remember bridges all over the world that have left indelible marks on my memory. Some are remnants of an ancient civilization and others thoroughly modern. Hence, each has some significant relevance in terms of impression, reflection of our knowledge, ethics & values.	9/7/2017 10:04 PM
91	The bridge has potential to become a symbol for the entire city and design will focus more on being visually striking from afar than on bench or lookout placement	9/7/2017 9:57 PM
92	To promote community use of the space	9/7/2017 9:50 PM
93	My priority is having a timely replacement of the bridge that accommodates more vehicle traffic and has dedicated protected lanes for active transport that are separate from both cars and pedestrians. All the rest of this is unnecessary nonsense. Build the bridge already.	9/7/2017 9:35 PM

## A Better Bridge for Arlington

94	Bridge functionality & features at the foot should outweigh the visual appeal. Shouldn't miss this opportunity to benefit the local community. If traffic flows and people feel safe in the area then the project will augment the community and enrich the local businesses far more than a visually appealing bridge.	9/7/2017 9:10 PM
95	From the air the bridge must stand out. Everything else will follow.	9/7/2017 8:43 PM
96	from the hearts of our city we look beyond to what the land we see so far and wide from the bashing of rail yards to the chimes of train bells to the wiz of the cars that pass over the arlington bridge an icon to stare out to see to grow to inspire so many to just stop and look out look out over our prairie home a land so far in between this great nation we call canada our own high line park to dream	9/7/2017 8:20 PM
97	Simply put: design and aesthetic are the first thing people will notice. A bridge is foremost a physical entity that demands our visual attention above all else. It should look stunning, futuristic, and commanding. It must be unique in its design in order to be associated with our city.	9/7/2017 8:19 PM
98	Opportunity to create affordable and innovative living spaces. And green space and bike and walking paths to a neighbourhood that needs it.	9/7/2017 7:30 PM
99	People in the north side of the city deserve a beautiful. Winnipeg should have pride in our bridges. This is a chance for architecture, not just some road like too many boring bridges in Winnipeg.	9/7/2017 7:16 PM
100	We have to make the bridge feel safe for people to want to visit it making it a beautiful thing for winnipeg and potentially a tourist attraction.	9/7/2017 7:01 PM
101	Needs to be functional for all users, and is a great opportunity to contribute and develop this area in a positive way.	9/7/2017 6:20 PM
102	Not to be proud the Winnipeggers but to attracts the tourists and create income for the City like Ontario and Alberta they have a lot of Tourists attractions that they attracts people for other provinces to visit their city.	9/7/2017 6:18 PM
103	Structures designed for 100 years need to be aesthetically pleasing for as long as possible so as to add the the overall beauty of the city.	9/7/2017 6:02 PM
104	All are important, as all provide an opportunity to create a dynamic new public space in the middle of two densely populated urban communities, each underserved in terms of public amenities.	9/7/2017 5:06 PM
105	I feel like if you ensure the community is well supported by ensuring development at the foot of the bridge, everything else will take care of itself. The overall design should be unique, but not to the detriment of the time or financial budget (within reason). The look outs and art are important, but can be a bit of ab after thought. But I do think they are all important.	9/7/2017 4:53 PM
106	the bridge just needs to work. does not need to look amazing. just needs to work	9/7/2017 4:21 PM
107	The bridge isn't at a central tourist spot, or even at an attractive part of the city. Unique design doesn't matter as much as the new development opportunity at the bottom of the bridge. Safety is tantamount so that's why the developments and features on the bridge take priority.	9/7/2017 4:19 PM
108	The overall bridge should be a testament to the pride Winnipeggers/Manitobans share in their community(ies). We already have some bridges that lack inspiration and merely focus on function. Let's make a bridge that focuses on a bridge as a model of our culture, environmental responsibility/sustainability and ingenuity!	9/7/2017 3:40 PM
109	All are important however success of the others is reliant upon the presence of a vibrant community - which is supported by the effective land use and development.	9/7/2017 3:29 PM
110	Cost efficiency just so traffic flow is good	9/7/2017 3:14 PM
111	Development of the land at the foot of the bridge is very important so that it is kept and used by people. People need to feel safe in that area and the community needs to be proud of the space. If it becomes a place for the hookers and vagrants to hang out-then it gets a bad name very fast and people don't want to be in the area.	9/7/2017 2:43 PM
112	My only concerns are 1) cost, 2) usability for car, bike, and foot traffic, and 3) ability to allow for future conversion of the rail yards into a freeway.	9/7/2017 2:25 PM
113	More money and more crimes	9/7/2017 2:22 PM
114	Well planned development at foot of bridge will be very important for safety in the area and has the most impact in terms of improving the neighborhood for the people living in it.	9/7/2017 2:03 PM

## A Better Bridge for Arlington

115	It's function is to bridge one company's property, not to a legacy project.	9/7/2017 12:51 PM
116	The "north end" is seen as socially lower class than our south end neighbours, this is a gateway to and from our area, would nice to be a world class entrance/exit. A	9/6/2017 11:30 PM
117	The bridge is a symbol that the railyard is not a friendly neighbour. The railyard will eventually move so any elaborate bridge is not needed.	9/6/2017 2:54 PM
118	Functional Bridge is primary goal. Everything else is Gravy. The extra space should be put to good use.	9/6/2017 10:22 AM
119	These are all costly features that are not necessary and will be underutilized.	9/6/2017 7:34 AM
120	As I work at Elias truck service just steps away from the bridge, I would love to see the area visually Improved. Cosmetically speaking the area is rather unpleasant to look at. You love to see better lighting and much more of a modern feel to the area.	9/5/2017 9:23 PM
121	Features on the bridge, and development at the foot of it, will make it something for people, not just for vehicles.	9/5/2017 7:24 PM
122	Dont waste money where it doesn't need to be wasted. Just build a plain bridge	9/5/2017 1:13 PM
123	Need to build a sustainable quality but economical bridge keeping in mind that tax dollars are being spent.	9/5/2017 12:06 PM
124	Uniqueness is important but development for affordable housing and attracting businesses is primary in my opinion. Features on the bridge also are important, and I think will come to be out of a unique design.	9/5/2017 11:54 AM
125	a unique design is nice but over some ugly railway yards makes no sense. I do not think CN/CP is going to be quick to clean up there yards to promote the bridge. development at the foots of the bridge is the most critical as this will be the place of destination and the place of coming together. People do not typically hang out on the bridge, a bridge is a vehicle to travel over an obstacle. the foot is the most relevant and the areas that need the most attention.	9/5/2017 6:25 AM
126	It will just encourage riff raff to hang out and cause trouble. Just make a boring bridge for walking, not stopping on and causing trouble. It's in the north end.	9/3/2017 11:23 PM
127	impossible to rank, sorry :)	9/2/2017 9:56 AM
128	A bridge of this size is capable of being a Winnipeg icon like Riel Esplanade. The design is crucial if this is to take place. There should also be a focus on the development at the foot of the bridge so it enhances the neighbourhood.	9/2/2017 9:48 AM
129	State of the art 20th century	9/1/2017 11:37 PM
130	It is an important structure that connects the north side of city together with all... no seperations	9/1/2017 7:20 PM
131	There's nothing to look at from bridge . There's no scenery but train tracks. It is an opportunity to create a bridge that will make the capital city beautiful like the other capital cities in other provinces.	9/1/2017 4:18 PM
132	We should be proud of the bridge as it spans one of the largest privately owned rail yards in the world. The people who reside in the area or work there every day should be recognized.	9/1/2017 12:57 PM
133	The bridge needs to be functional first. What's also important is that it's on budget!	9/1/2017 12:51 PM
134	the hopes would be that housing that is taken away will be replaced. And that the space is safe for all.	9/1/2017 11:37 AM
135	It was difficult to rank the importance of each aspect. The bridge should have a unique design that incorporates the past and the feel of the area, but I also believe that it is important that the development at the foot of the bridge is important to create and keep the uniqueness of the area.	9/1/2017 10:29 AM
136	The area at the foot of the bridge will be accessed by many people who live in the area. The bridge itself is not a hangout, it's a conduit that people use very temporarily to get from A to B. I don't envision people making the bridge a destination spot, so the design of the bridge should stand out but what is on the bridge is less important to motorists like myself. It is unlikely I will ever be walking, or cycling, over it.	9/1/2017 9:58 AM

## A Better Bridge for Arlington

137	None of these are priorities to me. My priority is in functionality of the bridge alone. The proposed changes to Arlington street, such as no more parking on the street is a huge inconvenience. I work on Arlington between Logan and William and since parking is so limited in the area, I often find myself having to park on Arlington from 9-3:30pm and then moving my car once availability on side streets has increased. Taking away parking on Arlington would be a huge inconvenience to many workplaces, both for employee parking needs and customers!	9/1/2017 9:36 AM
138	The design of the bridge is most important because it is going to be a significant part of the neighbourhood, and features on the bridge to make it more pedestrian friendly is moderately important. The current bridge is not very pedestrian friendly, it is kind of scary. Hopefully with the new bridge design, the pigeons will be taken into account and elements of the bridge design will be such that it will deter the pigeons from using the bridge. Nobody wants to be pooped on by pigeons or walk through their poop on the ground.	9/1/2017 7:41 AM
139	Engineering Design is expensive. Function over form is the best value to the tax payer. Case in point the cookie cutter overpass bridges to grand forks. Put money into construction not design. Better traffic flow should be considered as well. Why on earth would you build a four lane bridge an only have three lanes going to it. What is the point? There are around 700 people that work at the CSCHAH. How will people be able to exit safely with only one lane on Arlington. Lets focus more on how something is going to work then how it looks.	9/1/2017 6:58 AM
140	Cost to government needs to be managed, not a vanity project.	9/1/2017 6:53 AM
141	I think safety should be first and foremost before esthetics.	9/1/2017 6:43 AM
142	The areas surrounding that rail yard are the most impoverished in Canada. It is an eyesore. Look out from the Arlington bridge now, and tell me how to make it pretty. Get rid of the tracks.	8/31/2017 9:13 PM
143	I feel a bridge should be safe, functional and inviting. It's artistic design is not a priority!	8/31/2017 9:11 PM
144	It is imperative that the new bridge creates excitement about the future of the area. This is only one step in the renewal process.	8/31/2017 4:26 PM
145	This area hasn't seen a lot of improvements for many years.	8/31/2017 4:13 PM
146	I don't think that a unique design will bring tourists to the North End (like the Forks/St. B). Since Selkirk Ave will not become a shopping destination for those who do not live in the North End, I don't think resources should be invested in a fancy bridge. I am curious what kind of bridge aesthetic would be chosen by the folks who live on Dufferin, Arlington, Stella etc. and will look out their windows at it daily. I think that those who are walking over the bridge should be provided with the opportunity to feel proud of themselves and their ancestors, reflect on their path, and feel hope for the future. Right now that bridge has chain link fences to prevent people from jumping, it looks over a survival sex trade strip, many of the people who use it are commuters driving and do not live in the immediate vicinity, and the strip club and biological disease research lab at it's base are not destinations of those who live in the North End nor welcoming.	8/31/2017 4:09 PM
147	Function over form.	8/31/2017 3:28 PM
148	Arlington is on the cycling route. It is the only bridge in the area (including Salter, McPhillips, and Main) that is appropriate for cyclists. Main and McPhillips have very heavy traffic and are not safe for cyclists (at most times). Salter bridge is also narrow, and can not accommodate cycling and pedestrian traffic. Connecting cycling and footpaths in Winnipeg is very important due to the nature of our roadways, and to engage/encourage community members (including low income) in affordable modes of transportation. This is especially important as many individuals who bike as their only means of transportation already take short cuts, and may follow incorrect road/cycling rules due to the current layout/structure of cycling paths. Improvements would mean better cycling/footpaths with improved flow, which would in turn improve vehicular traffic flow.	8/31/2017 1:59 PM
149	The utility and structure of the bridge is more important than appearance. Ugly bridge doesn't necessarily mean unusable.	8/31/2017 1:34 PM
150	The most important thing is functionality of the bridge. The bridge needs to aid traffic flow in the area and the land around should promote growth and an improvement to the area. If it happens to look pretty that is a bonus	8/31/2017 1:18 PM
151	I think the areas on either side of the bridge need to be cleaned up and helped to make the thought of walking over the bridge in either direction an appealing idea. There are significant safety issues and I would never consider walking over the bridge in the evening, so stopping and taking a "selfie" for example is not even a consideration.	8/31/2017 1:17 PM
152	This is a confusing question. You should have asked these within each section above instead?	8/31/2017 12:36 PM

## A Better Bridge for Arlington

153	Needs replacing, needs BIKE LANES, needs to promote changes to the current 'sketchiness'	8/31/2017 12:23 PM
154	Functionality is most important. Does it effectively get cars and people across from one side to the other? Does it reduced traffic congestion? Does it meet anticipated future usage?	8/31/2017 12:16 PM
155	The bridge HAS to invite more than just vehicle traffic, the entrance to it must reflect that priority. Second, it will be seen and appreciated more from afar than from atop, so making it a priority to 'look at' over 'look from' seems to make sense to me. Lastly, the bridge and the areas that it joins will not likely ever be 'destinations' for their own sake, so non-transit features should be the lowest priority.	8/31/2017 10:58 AM
156	The development at the foot of the bridge needs to bring people together	8/31/2017 5:23 AM
157	Unsure. I think all three things are moderately important. I think I am most concerned about safety. If it is meant to draw people to it, we need to make sure it is safe with lots of lighting ,and not encouraging groups to linger on it.	8/30/2017 9:45 PM
158	North End always gets a bad rap and I think we should stand out for our uniqueness and beauty	8/30/2017 9:39 PM
159	4 lanes only way to build thus bridge think ahead of generations to come doit right	8/30/2017 9:35 PM
160	?	8/30/2017 9:23 PM
161	I think functionality over design is more important. You need garbage and recycling bins to start. Benches are important too. Then comes designing the space at the foot of the bridge. A recreational park with a skating rink, bathrooms, kids playground are all things I think are important to have.	8/30/2017 9:04 PM
162	The bridge needs to be functional. 4 lanes (2 south bound, 2 north bound). As well as a large bike path on both sides. Do it right the first time. The Arlington bridge if done right can help reduce traffic issues in thus area.	8/30/2017 9:02 PM
163	The affect this bridge has on the land on either end will make or break this new bridge.	8/30/2017 8:52 PM
164	Seeing the bridge mostly while I drive over having it look good over othe foot related use is more what I would use it for being from the south west end of the city .	8/30/2017 8:42 PM
165	Uniqueness ? Let's talk usability and longevity. We have one chance at this in my lifetime let's open up the north to the west end and downtown. Easier commuting and better traffic flow.	8/30/2017 8:35 PM
166	Make it unique in so far that it is difficult to vandalize, reduce areas that would make it easier to victimize people, keep the view open,	8/30/2017 8:30 PM
167	Art and beauty are nice but are not the most important, it is more beneficial to have development	8/30/2017 8:00 PM
168	We need to spur more economic development in the North End.	8/30/2017 7:57 PM
169	Because the bridge is what we want to showcase and be proud of, we're not replacing the ends of the old arlington bridge, we are replacing the bridge itself.	8/30/2017 6:44 PM
170	Abandon the bridge and widen the underpass instead.	8/30/2017 5:51 PM
171	Because the north deserves a good looking bridge and a welcome sign to the north end of winnipeg	8/30/2017 5:42 PM
172	The bridge's chief goal is to improve traffic flow, and if it succeeds in doing this, that's good enough. We don't have to run up bills for extraneous items like the ones listed above.	8/30/2017 3:26 PM
173	As downtown expands and grows, we should be increasing the number of residential units to increase the population living downtown more aggressively than we are with urban sprawl (development towards perimeter. Building mixed use units (first floor commercial/business, second floor office space, third and fourth floor units for families or parents with kids (mix of 1,2, and 3 bedroom), floors above for students (small cheap bachelor suites), and suites for lower income on the top floors. These should be considered for the development at the bottoms of the bridges. Having a bicycle friendly area will attract those who will utilize those features and what better way than to increase residency to attract that segment of our population. As the density of population increases it will increase the opportunity for business investment in the area to support a larger population, ideally enhancing what features the bridge chooses.	8/30/2017 3:23 PM
174	Economic development.	8/30/2017 3:12 PM

## A Better Bridge for Arlington

175	The priority should be the ease of traffic and the passage of people, cars, and bikes. Leave out all the art, selfie spots, and other useless buzz words and add another lane going south. One lane is ridiculous.	8/30/2017 3:02 PM
176	If we can give Winnipeg a new, practical piece of infrastructure, while still making it memorable and pleasing to the eye and also provide some useful and welcome amenities to an underserved neighbourhood, why not?	8/30/2017 2:51 PM
177	The bridge must be affordable, functional, and easy to maintain. Those are my priorities.	8/30/2017 2:50 PM
178	Since the agenda is already set here for a new bridge at Arlington Street as opposed to the one that had been proposed in the late 70s, early 80s to cross at McGregor St. and join with Sherbrooke St. which would have been the logical choice to improve traffic flows in the north end of the city, lets at the very least make this bridge design reflective of the constituent elements that make up the north end of the city of what it once was and what it is today, while invoking the neighbourhood's potential with a contemporary design.	8/30/2017 2:16 PM
179	Ensuring that the land at either end of the bridge is developed to its fullest potential is more important than the uniqueness of the bridge because there are many unique public projects in Winnipeg that failed or are underutilised because no consideration was given to the area around them. The Forks comes to mind, seperated from the city by acres of parking and a busy street. The surrounding lands are more important that prioritising a unique design as the design is the first thing that gets cut back when projects go over budget, so it's best not to put so much faith in what is a fairly expendable part of the project.	8/30/2017 2:09 PM
180	A fantastic design will attract positive attention to our North End. Joining the south to the north sides of the bridge by developing the area will create a positive experience.	8/30/2017 2:08 PM
181	I'm not worried about uniqueness, just that it is right for that space and the people. It sounds like you have gotten a lot of great input into the project already, with all those great ideas. I think reflecting that is the most important.	8/30/2017 1:45 PM
182	I think they area the bridge lives in could use support in being a safe community. Selkirk used to be a vary vibrant community, and I think this can continue to help the area along with the other work being done there further down Selkirk Ave	8/30/2017 1:26 PM
183	The bridge should be designed to discourage people from attempting to throw themselves off the bridge	8/30/2017 1:05 PM
184	City of Winnipeg public consultations are a complete farce. Every aspect of this project has been hashed out in the back room.	8/30/2017 12:36 PM
185	Having a functional new bridge quickly and on budget, not architectural embellishment, should be the highest priority.	8/30/2017 11:45 AM
186	Build something unique that will be rivaled worldwide. Unique like The Helix Bridge or the Henderson Waves Bridge of Singapore or the Peace Bridge in Calgary.	8/30/2017 11:36 AM
187	Honestly, this has taken so long, and anticipated to begin construction in 2020 "if" funding is in place....for crying out loud, build a bridge, function over style at this point wins the day. Slap some paint on it later if you have any funds remaining.	8/30/2017 11:15 AM
188	This is a big opportunity to change the fortunes of the property around this structure.	8/30/2017 10:42 AM
189	This area is an area that attracts prostitution, having benches may not be the best idea.	8/30/2017 10:40 AM
190	Currently development is lackluster at either side of the bridge and doesn't promote safety of community, this needs to change.	8/30/2017 10:20 AM
191	I still am not convinced the design is the most key aspect here. Will selfie spots help revitalize this neighbourhood at a high cost? Not convinced.	8/30/2017 10:19 AM
192	Me, I'm probably only ever going to drive over it.	8/30/2017 10:16 AM
193	The overall design of the bridge will be something that people cannot destroy (litter, garbage, etc.). As it's in an area that isn't deemed "the most beautiful" it would be a good idea to put something that attracts the eye....like a rose in a field of weeds.	8/30/2017 10:08 AM
194	The area is not a particularly beautiful space and this is a great opportunity to change this. Uniqueness to mean also means beautification.	8/30/2017 7:21 AM

## Q6 What are the first three digits of your postal code? (e.g. R3B)

Answered: 483 Skipped: 24

#	RESPONSES	DATE
1	r3r	9/30/2017 5:33 PM
2	R3R	9/30/2017 4:24 PM
3	R3G	9/30/2017 4:07 PM
4	R3J	9/30/2017 2:22 PM
5	R2w	9/30/2017 1:26 PM
6	R3G	9/30/2017 12:14 PM
7	R2G	9/30/2017 9:00 AM
8	R2L	9/30/2017 7:34 AM
9	R2R	9/30/2017 7:01 AM
10	R2Y	9/30/2017 6:01 AM
11	R2M	9/30/2017 2:58 AM
12	R2c	9/29/2017 11:48 PM
13	R2R	9/29/2017 12:21 PM
14	r2v	9/29/2017 12:10 PM
15	R3C	9/29/2017 11:46 AM
16	R2w	9/29/2017 11:14 AM
17	R3L	9/29/2017 10:39 AM
18	R3G	9/29/2017 9:57 AM
19	R3K	9/29/2017 9:50 AM
20	R2W	9/29/2017 9:36 AM
21	R2Y	9/29/2017 9:28 AM
22	R3g	9/28/2017 9:57 PM
23	R2X	9/28/2017 9:50 PM
24	R2N	9/28/2017 9:39 PM
25	R2X	9/28/2017 7:59 PM
26	R2M	9/28/2017 4:30 PM
27	R2W	9/28/2017 2:22 PM
28	R3E	9/28/2017 2:19 PM
29	R3L	9/28/2017 2:17 PM
30	R1A	9/28/2017 2:11 PM
31	R2W	9/28/2017 2:09 PM
32	R2Y	9/28/2017 2:06 PM
33	R24	9/28/2017 2:03 PM
34	R3G	9/28/2017 2:00 PM
35	R3P	9/28/2017 1:58 PM

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36	R2Y	9/28/2017 1:56 PM
37	R2C	9/28/2017 1:53 PM
38	2M6	9/28/2017 1:51 PM
39	R2V	9/28/2017 1:49 PM
40	R2W	9/28/2017 1:46 PM
41	R2L	9/28/2017 1:43 PM
42	R2M	9/28/2017 1:41 PM
43	R2P	9/28/2017 1:39 PM
44	R2H	9/28/2017 1:36 PM
45	R3L	9/28/2017 1:33 PM
46	R2C	9/28/2017 1:28 PM
47	R2W	9/28/2017 1:24 PM
48	R2x	9/27/2017 7:38 PM
49	R3E	9/26/2017 2:54 PM
50	R2W	9/26/2017 5:54 AM
51	R3j	9/25/2017 1:26 PM
52	R3E	9/24/2017 7:54 PM
53	R2G	9/24/2017 1:26 PM
54	R3n	9/23/2017 8:59 PM
55	R3L	9/23/2017 5:23 PM
56	R2V	9/23/2017 4:48 PM
57	R3G	9/23/2017 3:11 PM
58	R3K	9/23/2017 2:21 PM
59	R2X	9/23/2017 12:07 PM
60	R2N	9/23/2017 10:23 AM
61	R2J	9/23/2017 9:00 AM
62	r2v	9/23/2017 7:30 AM
63	R2B	9/23/2017 1:01 AM
64	R2W	9/22/2017 3:43 PM
65	R2x	9/22/2017 3:29 PM
66	r2w	9/22/2017 12:31 PM
67	R2w	9/22/2017 7:14 AM
68	R2G	9/21/2017 9:07 PM
69	R2N	9/21/2017 8:42 PM
70	R2X	9/21/2017 8:21 PM
71	R2P	9/21/2017 8:14 PM
72	R3R	9/21/2017 7:44 PM
73	R3G	9/21/2017 3:39 PM
74	R3G	9/21/2017 2:57 PM
75	R2W	9/21/2017 11:55 AM
76	R3E Home R3E Work	9/21/2017 10:51 AM

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77	R3t	9/21/2017 10:48 AM
78	R3B	9/21/2017 10:32 AM
79	r3l	9/21/2017 9:07 AM
80	R3G	9/21/2017 9:00 AM
81	r3g	9/21/2017 8:53 AM
82	R2V	9/21/2017 8:10 AM
83	R3C	9/20/2017 8:17 PM
84	R2W	9/20/2017 8:12 PM
85	R3E	9/20/2017 8:07 PM
86	R2X	9/20/2017 7:50 PM
87	R2X	9/20/2017 7:47 PM
88	R2W	9/20/2017 7:42 PM
89	R2X	9/20/2017 7:39 PM
90	R2W	9/20/2017 12:51 PM
91	R3G	9/20/2017 12:21 PM
92	R2H	9/20/2017 9:22 AM
93	R3W	9/19/2017 2:07 PM
94	R2X	9/19/2017 1:31 PM
95	R2X	9/18/2017 6:04 PM
96	R3t	9/18/2017 2:27 PM
97	R3J	9/18/2017 12:48 PM
98	R2L	9/18/2017 12:46 PM
99	R3L	9/18/2017 12:43 PM
100	R2W	9/18/2017 12:40 PM
101	R3T	9/18/2017 12:31 PM
102	R3G	9/18/2017 12:26 PM
103	R3T	9/18/2017 12:18 PM
104	R3A	9/18/2017 12:08 PM
105	R3G	9/18/2017 12:02 PM
106	R3A	9/18/2017 11:46 AM
107	R3G	9/18/2017 11:40 AM
108	R3R	9/18/2017 11:38 AM
109	M3A	9/18/2017 11:24 AM
110	R2W	9/18/2017 11:11 AM
111	R3A	9/18/2017 11:07 AM
112	R3A	9/18/2017 10:55 AM
113	R2N	9/18/2017 10:42 AM
114	R3A	9/18/2017 10:37 AM
115	R3A	9/18/2017 10:31 AM
116	R3A	9/18/2017 10:20 AM
117	R3A	9/18/2017 10:12 AM

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118	r2x	9/18/2017 7:59 AM
119	R2X	9/18/2017 5:28 AM
120	R3G	9/17/2017 9:58 PM
121	R2w	9/17/2017 5:43 PM
122	R2W	9/17/2017 9:10 AM
123	R3G	9/16/2017 7:32 PM
124	R2v	9/16/2017 4:33 PM
125	R2p oc7	9/16/2017 11:05 AM
126	R0c	9/16/2017 10:46 AM
127	R2R	9/16/2017 10:00 AM
128	r2x	9/16/2017 9:57 AM
129	R2X	9/16/2017 9:44 AM
130	R2W	9/16/2017 9:38 AM
131	R2p	9/16/2017 9:34 AM
132	R2W	9/16/2017 9:21 AM
133	R2V	9/16/2017 9:14 AM
134	R3E	9/16/2017 9:04 AM
135	R2W	9/16/2017 8:53 AM
136	R2W	9/15/2017 3:20 PM
137	R3N	9/15/2017 12:25 PM
138	r2w	9/15/2017 11:36 AM
139	R2R	9/15/2017 11:16 AM
140	R3J	9/15/2017 9:39 AM
141	R2X	9/15/2017 9:21 AM
142	R3G	9/14/2017 7:28 PM
143	R2W	9/14/2017 7:24 PM
144	R0G	9/14/2017 4:30 PM
145	R2G	9/14/2017 1:10 PM
146	R2X	9/14/2017 11:30 AM
147	R1A	9/14/2017 7:10 AM
148	R2E	9/14/2017 7:00 AM
149	R3N	9/13/2017 2:52 PM
150	R2v	9/13/2017 2:05 PM
151	R2P	9/13/2017 12:57 PM
152	R0C	9/13/2017 11:42 AM
153	r2p	9/13/2017 9:31 AM
154	R2W	9/13/2017 9:00 AM
155	R3g	9/12/2017 10:17 PM
156	R2X	9/12/2017 9:14 PM
157	r3e	9/12/2017 5:50 PM
158	R3e	9/12/2017 4:52 PM

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159	R2x	9/12/2017 4:43 PM
160	R2X	9/12/2017 4:15 PM
161	R3G	9/12/2017 1:41 PM
162	R2W	9/12/2017 1:31 PM
163	R2X	9/12/2017 12:18 PM
164	R3E	9/12/2017 9:40 AM
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166	R3G	9/12/2017 8:39 AM
167	R2X	9/12/2017 7:44 AM
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182	R2W	9/10/2017 8:03 PM
183	R2W	9/10/2017 7:16 PM
184	R2w	9/10/2017 6:05 PM
185	R2w	9/10/2017 6:04 PM
186	R3n	9/10/2017 5:29 PM
187	R2W	9/10/2017 4:28 PM
188	r2p	9/10/2017 1:38 PM
189	R2R	9/10/2017 12:08 PM
190	R3X	9/10/2017 10:25 AM
191	R2W	9/10/2017 9:49 AM
192	R2L	9/10/2017 9:40 AM
193	R2W	9/10/2017 9:10 AM
194	r3y	9/10/2017 7:29 AM
195	r2w	9/10/2017 1:05 AM
196	R2W	9/9/2017 11:05 PM
197	R2N	9/9/2017 10:46 PM
198	R2X	9/9/2017 8:25 PM
199	R2G	9/9/2017 7:36 PM

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200	R2v	9/9/2017 5:16 PM
201	R2W	9/9/2017 11:14 AM
202	R2W 1N4	9/9/2017 10:42 AM
203	R2W	9/9/2017 10:31 AM
204	R3b	9/9/2017 8:50 AM
205	R3g	9/9/2017 7:23 AM
206	r3c	9/9/2017 6:58 AM
207	R3H	9/9/2017 12:54 AM
208	R2W	9/9/2017 12:51 AM
209	R2V	9/8/2017 9:55 PM
210	R2g	9/8/2017 8:55 PM
211	R3N	9/8/2017 8:34 PM
212	R2k	9/8/2017 8:15 PM
213	R2x	9/8/2017 7:58 PM
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235	R2w 2a5	9/8/2017 12:38 AM
236	R2N	9/7/2017 10:07 PM
237	R2V	9/7/2017 10:07 PM
238	R3m	9/7/2017 9:58 PM
239	R3L	9/7/2017 9:53 PM
240	R3E	9/7/2017 9:51 PM

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241	R3C 1Y4	9/7/2017 9:36 PM
242	R3M	9/7/2017 9:30 PM
243	R3g	9/7/2017 9:15 PM
244	R3G	9/7/2017 9:13 PM
245	R2n	9/7/2017 9:12 PM
246	R2w	9/7/2017 8:45 PM
247	R3B	9/7/2017 8:36 PM
248	R2P	9/7/2017 8:24 PM
249	r2w	9/7/2017 8:22 PM
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252	R1C	9/7/2017 8:02 PM
253	R2h	9/7/2017 8:01 PM
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255	R3g	9/7/2017 7:32 PM
256	R2x	9/7/2017 7:28 PM
257	R2M	9/7/2017 7:17 PM
258	R2n	9/7/2017 7:05 PM
259	R2k	9/7/2017 7:02 PM
260	R2M	9/7/2017 6:21 PM
261	R2X	9/7/2017 6:21 PM
262	R3C	9/7/2017 6:09 PM
263	R2K	9/7/2017 6:04 PM
264	R5H	9/7/2017 5:50 PM
265	r2x	9/7/2017 5:13 PM
266	r3g	9/7/2017 5:07 PM
267	r2c	9/7/2017 4:22 PM
268	r3t	9/7/2017 4:20 PM
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273	R2V	9/7/2017 2:44 PM
274	R3R	9/7/2017 2:40 PM
275	R3J	9/7/2017 2:36 PM
276	R2m	9/7/2017 2:30 PM
277	R3g	9/7/2017 2:30 PM
278	R2V	9/7/2017 2:27 PM
279	r2x	9/7/2017 2:24 PM
280	R2J	9/7/2017 2:10 PM
281	R3N	9/7/2017 2:04 PM

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282	R3P	9/7/2017 1:53 PM
283	r3g	9/7/2017 1:53 PM
284	R2V	9/7/2017 1:53 PM
285	R2W	9/7/2017 12:52 PM
286	R2w	9/7/2017 12:35 PM
287	R2w	9/7/2017 11:15 AM
288	R2X	9/7/2017 10:06 AM
289	r2v	9/7/2017 8:28 AM
290	R3N	9/7/2017 8:19 AM
291	r2w	9/7/2017 8:12 AM
292	r2x	9/7/2017 7:13 AM
293	R2V	9/6/2017 11:31 PM
294	r3c	9/6/2017 6:03 PM
295	R2v	9/6/2017 5:38 PM
296	R2W	9/6/2017 4:34 PM
297	R2X	9/6/2017 2:56 PM
298	R3P	9/6/2017 2:46 PM
299	R2W	9/6/2017 1:10 PM
300	R2N	9/6/2017 10:24 AM
301	R3R	9/6/2017 7:38 AM
302	R2C	9/5/2017 9:24 PM
303	R3T	9/5/2017 7:27 PM
304	R3G 2P1	9/5/2017 4:12 PM
305	R2W	9/5/2017 3:44 PM
306	R3G	9/5/2017 2:42 PM
307	R0a	9/5/2017 1:14 PM
308	R3N	9/5/2017 1:13 PM
309	R3M	9/5/2017 12:07 PM
310	R3K	9/5/2017 11:55 AM
311	R3R	9/5/2017 10:04 AM
312	R2N	9/5/2017 9:59 AM
313	R2K	9/5/2017 9:52 AM
314	r2m	9/5/2017 8:48 AM
315	R3E	9/5/2017 8:28 AM
316	R2C	9/5/2017 7:41 AM
317	r1a	9/5/2017 7:27 AM
318	r2h	9/5/2017 7:09 AM
319	R3L	9/5/2017 6:27 AM
320	R3G	9/4/2017 10:51 AM
321	R2C	9/3/2017 11:24 PM
322	R2K	9/2/2017 8:52 PM

## A Better Bridge for Arlington

323	r3g	9/2/2017 4:43 PM
324	r2x	9/2/2017 9:57 AM
325	R2M	9/2/2017 9:49 AM
326	R2W	9/2/2017 9:13 AM
327	R2k	9/1/2017 11:38 PM
328	R2X	9/1/2017 7:22 PM
329	R2R	9/1/2017 4:19 PM
330	R2J 4A6	9/1/2017 1:01 PM
331	R3T	9/1/2017 12:59 PM
332	R3J	9/1/2017 12:54 PM
333	R3T	9/1/2017 12:53 PM
334	R3N	9/1/2017 12:20 PM
335	R2X	9/1/2017 12:05 PM
336	R2M	9/1/2017 12:02 PM
337	R2X	9/1/2017 11:38 AM
338	R2H	9/1/2017 11:30 AM
339	R2P	9/1/2017 10:30 AM
340	R3G	9/1/2017 10:25 AM
341	r2m	9/1/2017 10:00 AM
342	R2X	9/1/2017 10:00 AM
343	R2V	9/1/2017 9:39 AM
344	R2R	9/1/2017 9:37 AM
345	R4A	9/1/2017 9:28 AM
346	R2H	9/1/2017 9:23 AM
347	R3E	9/1/2017 9:07 AM
348	R2J	9/1/2017 8:27 AM
349	R3R	9/1/2017 8:24 AM
350	r3r	9/1/2017 7:53 AM
351	R2Y	9/1/2017 7:52 AM
352	R2K	9/1/2017 7:46 AM
353	R2P	9/1/2017 7:44 AM
354	R3G	9/1/2017 7:44 AM
355	R3E	9/1/2017 7:39 AM
356	R3C	9/1/2017 7:32 AM
357	R3E	9/1/2017 6:57 AM
358	R3J	9/1/2017 6:54 AM
359	r2y	9/1/2017 6:52 AM
360	R3R	9/1/2017 6:44 AM
361	R3L	9/1/2017 6:34 AM
362	R2H	9/1/2017 6:34 AM
363	R2N	9/1/2017 1:20 AM

## A Better Bridge for Arlington

364	R3R	8/31/2017 9:14 PM
365	R2G	8/31/2017 9:12 PM
366	R3E	8/31/2017 4:28 PM
367	R3E	8/31/2017 4:14 PM
368	R2W	8/31/2017 4:10 PM
369	r3w	8/31/2017 4:00 PM
370	R2W	8/31/2017 3:55 PM
371	R2W	8/31/2017 3:44 PM
372	R2P	8/31/2017 3:28 PM
373	R2P	8/31/2017 3:23 PM
374	R2H	8/31/2017 3:20 PM
375	R3X	8/31/2017 2:44 PM
376	R2V	8/31/2017 2:00 PM
377	R3R	8/31/2017 1:36 PM
378	R2W	8/31/2017 1:19 PM
379	R2E	8/31/2017 1:18 PM
380	R3E	8/31/2017 12:44 PM
381	R3Y	8/31/2017 12:44 PM
382	r2h	8/31/2017 12:40 PM
383	R2W	8/31/2017 12:38 PM
384	R2N	8/31/2017 12:37 PM
385	R2P	8/31/2017 12:25 PM
386	R2V	8/31/2017 12:17 PM
387	R3R	8/31/2017 12:14 PM
388	R3E	8/31/2017 10:59 AM
389	R2W	8/31/2017 10:48 AM
390	R2J	8/31/2017 10:29 AM
391	r3m	8/31/2017 10:06 AM
392	R3C	8/31/2017 9:33 AM
393	R2x	8/31/2017 9:08 AM
394	R3m	8/31/2017 9:05 AM
395	R3J	8/31/2017 8:23 AM
396	R3G	8/31/2017 7:43 AM
397	R2K	8/31/2017 7:00 AM
398	R2w	8/31/2017 5:25 AM
399	R3T	8/31/2017 12:03 AM
400	r2r	8/30/2017 10:37 PM
401	R2W	8/30/2017 10:20 PM
402	R3G	8/30/2017 9:45 PM
403	R2X	8/30/2017 9:40 PM
404	R3N	8/30/2017 9:37 PM

## A Better Bridge for Arlington

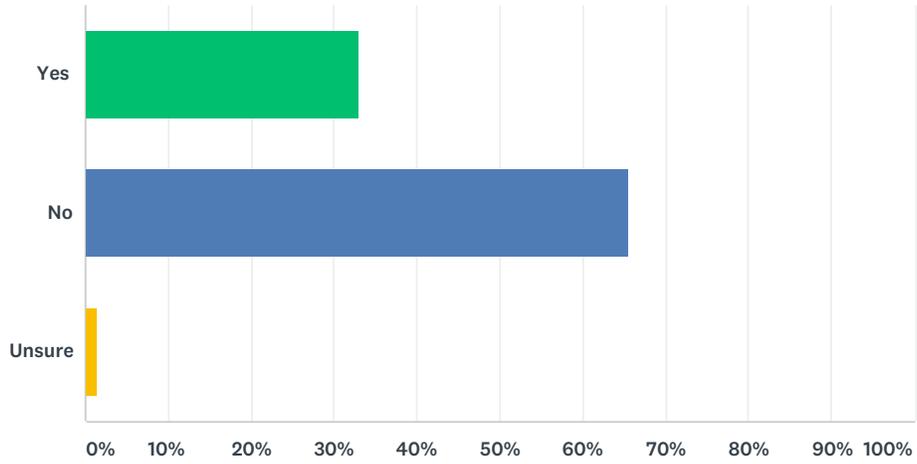
405	R3L	8/30/2017 9:24 PM
406	R2G	8/30/2017 9:12 PM
407	r3m	8/30/2017 9:08 PM
408	R2k	8/30/2017 9:05 PM
409	R1C	8/30/2017 9:03 PM
410	R3G	8/30/2017 8:53 PM
411	R2w0c4	8/30/2017 8:48 PM
412	R3R	8/30/2017 8:44 PM
413	R3G	8/30/2017 8:31 PM
414	R2G	8/30/2017 8:01 PM
415	R3T	8/30/2017 7:59 PM
416	R2V	8/30/2017 7:58 PM
417	R2x	8/30/2017 7:56 PM
418	R2C	8/30/2017 7:42 PM
419	R3G	8/30/2017 6:45 PM
420	R2V	8/30/2017 6:32 PM
421	R3b	8/30/2017 5:52 PM
422	R3T	8/30/2017 5:47 PM
423	R3C	8/30/2017 5:26 PM
424	R2V	8/30/2017 5:06 PM
425	r3l	8/30/2017 3:47 PM
426	R3N	8/30/2017 3:44 PM
427	R3R	8/30/2017 3:42 PM
428	r3b	8/30/2017 3:28 PM
429	R2P	8/30/2017 3:27 PM
430	R2J	8/30/2017 3:24 PM
431	R2N	8/30/2017 3:13 PM
432	R2V	8/30/2017 3:03 PM
433	R2W	8/30/2017 2:54 PM
434	R3J	8/30/2017 2:52 PM
435	R3R	8/30/2017 2:50 PM
436	R3G	8/30/2017 2:41 PM
437	R3B	8/30/2017 2:37 PM
438	R2V	8/30/2017 2:27 PM
439	R2H	8/30/2017 2:26 PM
440	R2V	8/30/2017 2:17 PM
441	R3G	8/30/2017 2:09 PM
442	R2K	8/30/2017 1:50 PM
443	R3C	8/30/2017 1:46 PM
444	R3C	8/30/2017 1:44 PM
445	r2w	8/30/2017 1:40 PM

## A Better Bridge for Arlington

446	R3G	8/30/2017 1:31 PM
447	R2V	8/30/2017 1:27 PM
448	R2P	8/30/2017 1:10 PM
449	R3E	8/30/2017 1:10 PM
450	R3T	8/30/2017 1:06 PM
451	R2X	8/30/2017 1:04 PM
452	R2V	8/30/2017 12:39 PM
453	R3M	8/30/2017 12:34 PM
454	r3g	8/30/2017 12:33 PM
455	R2V	8/30/2017 12:19 PM
456	R2G	8/30/2017 12:01 PM
457	R2J	8/30/2017 11:54 AM
458	R3G	8/30/2017 11:45 AM
459	R2P	8/30/2017 11:36 AM
460	R0C	8/30/2017 11:35 AM
461	R3L	8/30/2017 11:27 AM
462	R2V	8/30/2017 11:19 AM
463	r3e	8/30/2017 11:17 AM
464	R2L	8/30/2017 11:16 AM
465	R3N	8/30/2017 11:05 AM
466	R2G	8/30/2017 11:03 AM
467	r2v	8/30/2017 10:58 AM
468	R3X	8/30/2017 10:42 AM
469	R2V	8/30/2017 10:41 AM
470	R2W	8/30/2017 10:29 AM
471	R3Y	8/30/2017 10:26 AM
472	R3K	8/30/2017 10:25 AM
473	R3N	8/30/2017 10:21 AM
474	R3N	8/30/2017 10:20 AM
475	R2J	8/30/2017 10:19 AM
476	R0C	8/30/2017 10:17 AM
477	R3M	8/30/2017 10:14 AM
478	r3x	8/30/2017 10:11 AM
479	R2V	8/30/2017 10:09 AM
480	r2j	8/30/2017 10:06 AM
481	R3A	8/30/2017 9:02 AM
482	R3L	8/30/2017 7:22 AM
483	R0E	8/29/2017 6:58 PM

### Q7 Do you own a business or work near the Arlington Bridge?

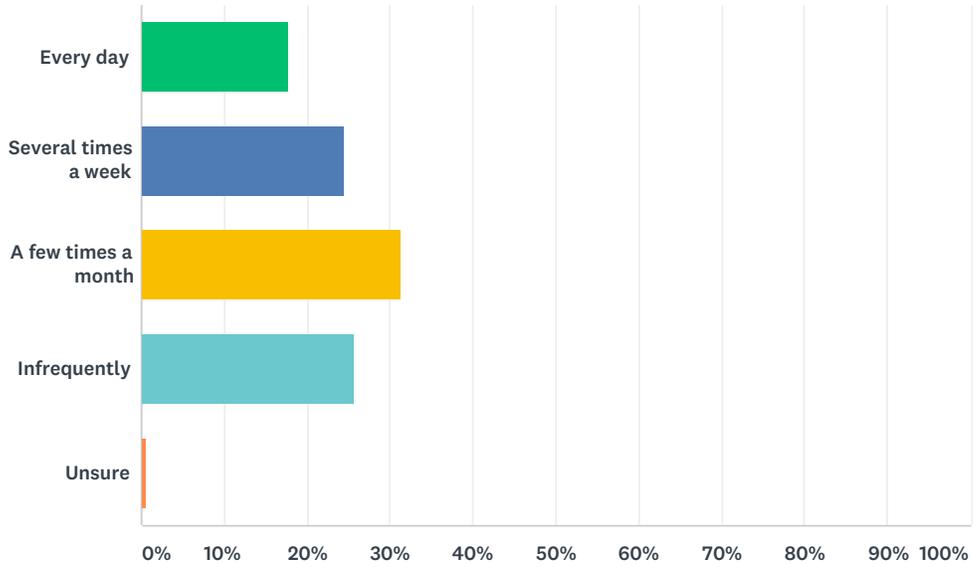
Answered: 491 Skipped: 16



ANSWER CHOICES	RESPONSES	
Yes	32.99%	162
No	65.58%	322
Unsure	1.43%	7
TOTAL		491

## Q8 How often do you use the Arlington Bridge?

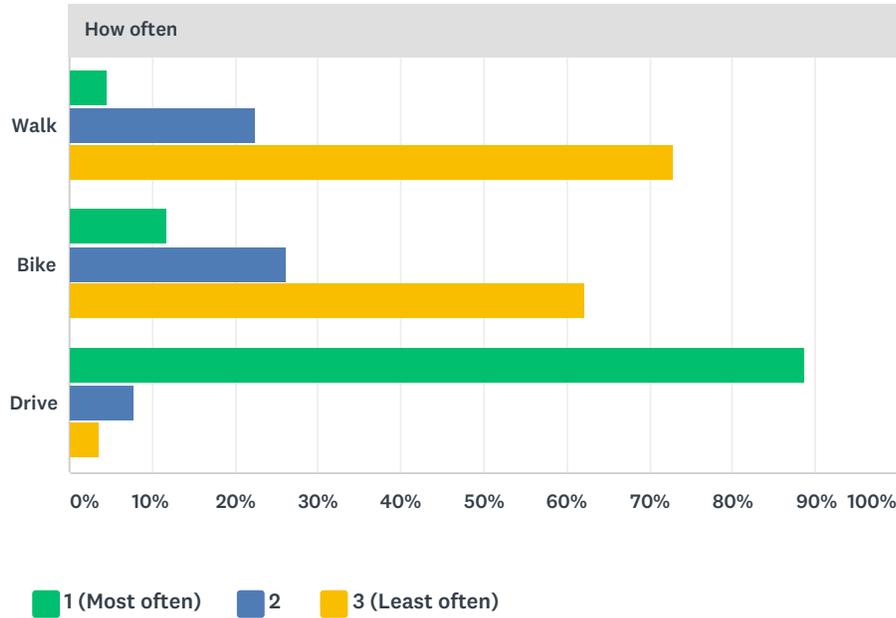
Answered: 497 Skipped: 10



ANSWER CHOICES	RESPONSES	
Every day	17.71%	88
Several times a week	24.55%	122
A few times a month	31.39%	156
Infrequently	25.75%	128
Unsure	0.60%	3
<b>TOTAL</b>		<b>497</b>

## Q9 From most often to least often, please tell us how you typically cross the bridge currently?

Answered: 487 Skipped: 20



How often				
	1 (MOST OFTEN)	2	3 (LEAST OFTEN)	TOTAL
Walk	4.66% 17	22.47% 82	72.88% 266	365
Bike	11.75% 45	26.11% 100	62.14% 238	383
Drive	88.63% 421	7.79% 37	3.58% 17	475

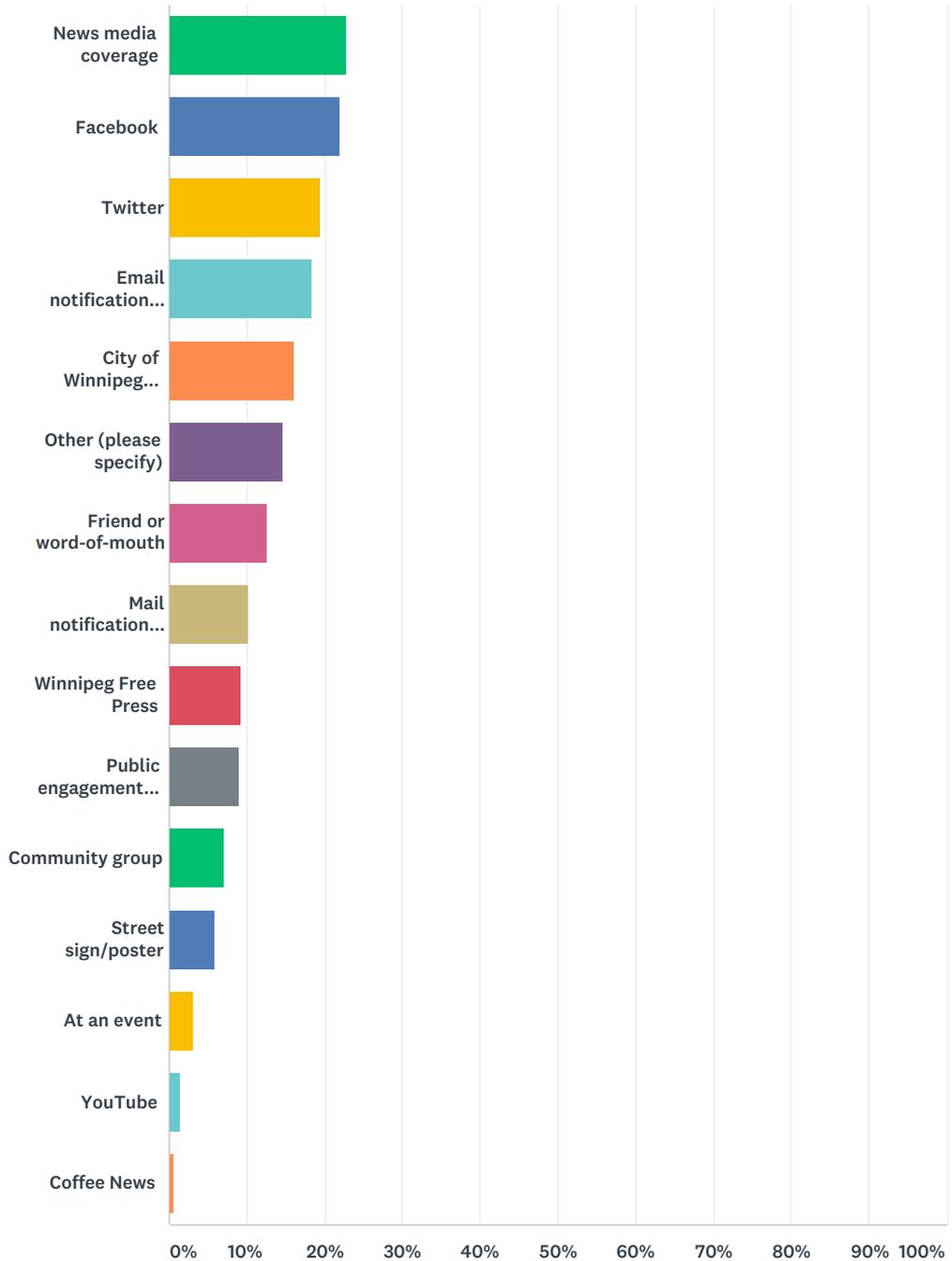
#	OTHER/UNSURE	DATE
1	No motor vehicle, dangerous to cross	9/29/2017 11:46 AM
2	I only drive over the bridge. I don't feel any other method is safe.	9/27/2017 7:38 PM
3	I substitute in 7 Oaks S.D. & Attend St. John's Cathedral.	9/26/2017 2:54 PM
4	I used to live in the north end and walk across the bridge.	9/23/2017 9:00 AM
5	do not feel safe biking across.	9/21/2017 8:14 PM
6	I only pass by this bridge daily to commute to work. I wish I could bike to work from Transcona to Route 90, but there is not enough protected bike paths.	9/19/2017 2:07 PM
7	Freight House event (September 6)	9/18/2017 11:38 AM
8	Unsure	9/18/2017 10:12 AM
9	Used to walk across daily to work.	9/15/2017 9:39 AM
10	NO BRIDGE remove the rail yard	9/13/2017 11:36 AM
11	I would never bike or walk across the bridge because there's too much pigeon faeces	9/11/2017 10:33 PM
12	I do not use the bridge. It is an eyesore and a planning nightmare.	9/11/2017 2:07 PM

## A Better Bridge for Arlington

13	More lanes for traffic great idea	9/10/2017 6:04 PM
14	Biking over the bridge sucks. Please make the bridge easier to cycle over.	9/8/2017 2:17 PM
15	I would consider using it as a transit route northbound, but not in its current state	9/7/2017 9:13 PM
16	I really do not use this bridge very much.	9/7/2017 8:21 PM
17	ride on a bus	9/5/2017 7:27 PM
18	I was a kid and walked the bridge	9/5/2017 7:41 AM
19	only drive over, i would not want to walk/bike with the current condition of the bridge and the area.	9/5/2017 6:27 AM
20	I never walk or bike there, not safe	9/2/2017 9:57 AM
21	As a child I rode my bicycle over the bridge. I pulled my wagon over the bridge. I used to go over the bridge every day of my life, but now live in another part of the city and do not use it frequently.	9/1/2017 1:01 PM
22	never bike or walk	9/1/2017 8:24 AM
23	passenger in vehicle	8/31/2017 1:36 PM
24	I do all the above, equally.	8/31/2017 12:38 PM
25	.	8/30/2017 7:59 PM
26	I rarely use this bridge b/c I commute via bike, and it's not friendly currently.	8/30/2017 2:09 PM
27	i walk my bike across the bridge - i do not feel safe riding my bike as it currently is	8/30/2017 12:33 PM

Q10 How did you hear about this project? (Please check all that apply.)

Answered: 497 Skipped: 10



ANSWER CHOICES	RESPONSES	
News media coverage	22.74%	113
Facebook	21.93%	109
Twitter	19.52%	97

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Email notification from the City	18.51%	92
City of Winnipeg website	16.10%	80
Other (please specify)	14.69%	73
Friend or word-of-mouth	12.47%	62
Mail notification from the City	10.26%	51
Winnipeg Free Press	9.26%	46
Public engagement newsletter	9.05%	45
Community group	7.04%	35
Street sign/poster	5.84%	29
At an event	3.22%	16
YouTube	1.41%	7
Coffee News	0.60%	3
Total Respondents: 497		

#	OTHER (PLEASE SPECIFY)	DATE
1	class	9/28/2017 2:19 PM
2	school	9/28/2017 2:17 PM
3	in class presentation	9/28/2017 2:11 PM
4	telephone survey	9/28/2017 2:09 PM
5	classroom	9/28/2017 2:06 PM
6	professor invited to speak at U of M	9/28/2017 1:53 PM
7	school	9/28/2017 1:49 PM
8	from my prof	9/28/2017 1:41 PM
9	from my prof	9/28/2017 1:24 PM
10	principal of King Edward school	9/24/2017 7:54 PM
11	Saw that the bridge was closed for repairs a few times.	9/23/2017 9:00 AM
12	North Centennial Neighbours meetings	9/21/2017 8:21 PM
13	phone call from city rep	9/20/2017 9:22 AM
14	North Centennial Popup (September 12)	9/18/2017 12:43 PM
15	Work email	9/18/2017 12:31 PM
16	Work	9/18/2017 12:08 PM
17	Freight House event (September 6)	9/18/2017 11:46 AM
18	Freight House event (September 6)	9/18/2017 11:40 AM
19	Freight House event (September 6)	9/18/2017 11:11 AM
20	flyers	9/18/2017 11:07 AM
21	Freight House event (September 6)	9/18/2017 10:55 AM
22	Freight House event (September 6)	9/18/2017 10:20 AM
23	Freight House event (September 6)	9/18/2017 10:12 AM
24	Multiple sources	9/17/2017 9:10 AM

## A Better Bridge for Arlington

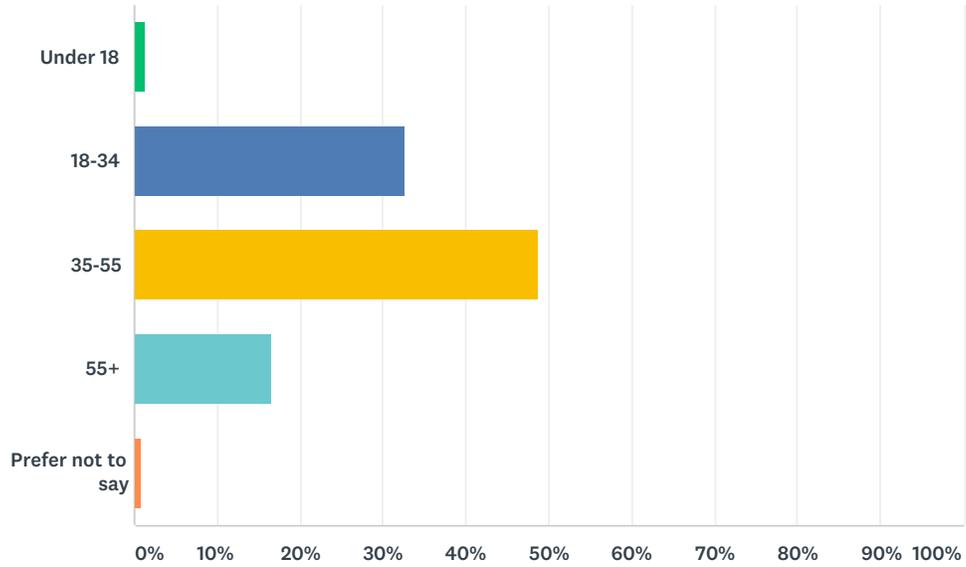
25	School posters	9/16/2017 9:04 AM
26	Work	9/14/2017 1:10 PM
27	work building-wide email	9/13/2017 2:52 PM
28	Metro news	9/13/2017 2:05 PM
29	in the hallway at work	9/13/2017 11:36 AM
30	E-mail at work	9/12/2017 9:40 AM
31	seniors groups using North Centennial pool	9/12/2017 1:57 AM
32	The times	9/11/2017 6:03 PM
33	Employed at nearby school	9/11/2017 5:58 PM
34	North end times	9/11/2017 4:10 PM
35	I work at the PHAC National Microbiology Lab occassionally, and someone who works there sent out the survey.	9/11/2017 12:53 PM
36	Why do you list the Free Press but not The Sun?	9/10/2017 10:25 AM
37	CBC	9/8/2017 9:55 PM
38	online forums	9/8/2017 1:48 PM
39	info forwarded at my place of work regarding City's consultation process	9/7/2017 9:13 PM
40	Google news headline	9/5/2017 7:27 PM
41	email at work	9/5/2017 2:42 PM
42	Colleague	9/5/2017 1:13 PM
43	Email notification at work	9/5/2017 12:07 PM
44	work	9/5/2017 10:04 AM
45	News from work	9/5/2017 9:52 AM
46	employer	9/5/2017 8:48 AM
47	email sent from our work	9/5/2017 7:27 AM
48	workplace	9/5/2017 7:09 AM
49	WCA email letter	9/5/2017 6:27 AM
50	Work Email	9/1/2017 12:54 PM
51	Workplace	9/1/2017 12:53 PM
52	Engagement at work	9/1/2017 12:20 PM
53	Winnipeg Construction Association newsletter	9/1/2017 11:30 AM
54	Through Winnipeg Construction Association	9/1/2017 10:30 AM
55	Email notification from workplace	9/1/2017 10:25 AM
56	Email notification from my workplace	9/1/2017 9:37 AM
57	Work email was sent out	9/1/2017 9:28 AM
58	Workplace email	9/1/2017 7:53 AM
59	Email from our community outreach team here at work	9/1/2017 7:44 AM
60	Work Email	9/1/2017 7:39 AM
61	Our employer circulated the survey	9/1/2017 6:44 AM
62	Work wide email	9/1/2017 6:34 AM
63	work	9/1/2017 6:34 AM
64	email at work	8/31/2017 3:55 PM

## A Better Bridge for Arlington

65	Radio	8/31/2017 3:23 PM
66	Work email	8/31/2017 2:44 PM
67	WORKPLACE	8/31/2017 2:00 PM
68	workplace shared the survey	8/31/2017 1:19 PM
69	Work	8/31/2017 12:17 PM
70	Reddit.com/r/winnipeg	8/31/2017 5:25 AM
71	Skyscraperpage.com	8/30/2017 2:37 PM
72	Other	8/30/2017 12:39 PM
73	Youtube	8/29/2017 6:58 PM

## Q11 What is your age?

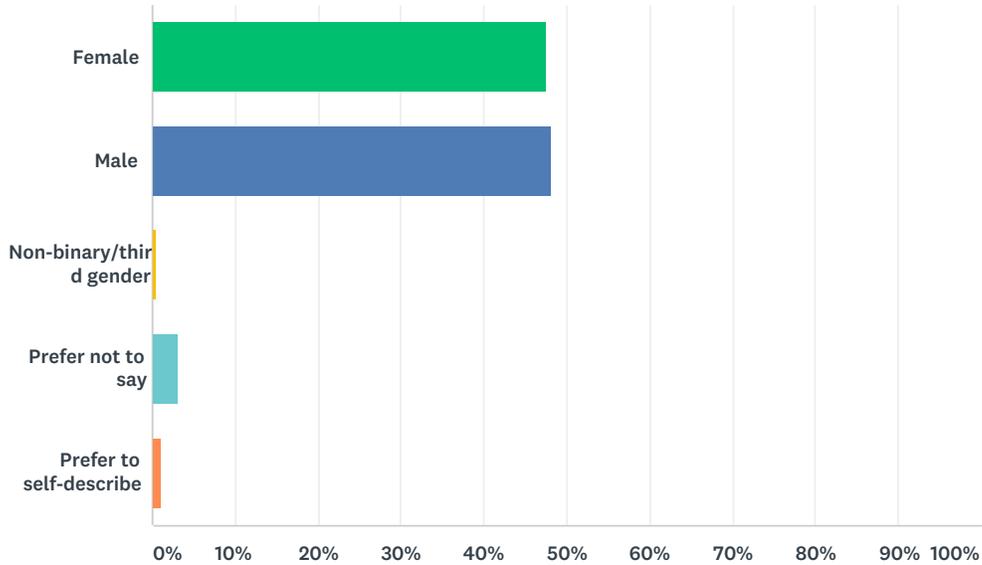
Answered: 492 Skipped: 15



ANSWER CHOICES	RESPONSES
Under 18	1.22% 6
18-34	32.72% 161
35-55	48.78% 240
55+	16.46% 81
Prefer not to say	0.81% 4
<b>TOTAL</b>	<b>492</b>

## Q12 What is your gender?

Answered: 491 Skipped: 16

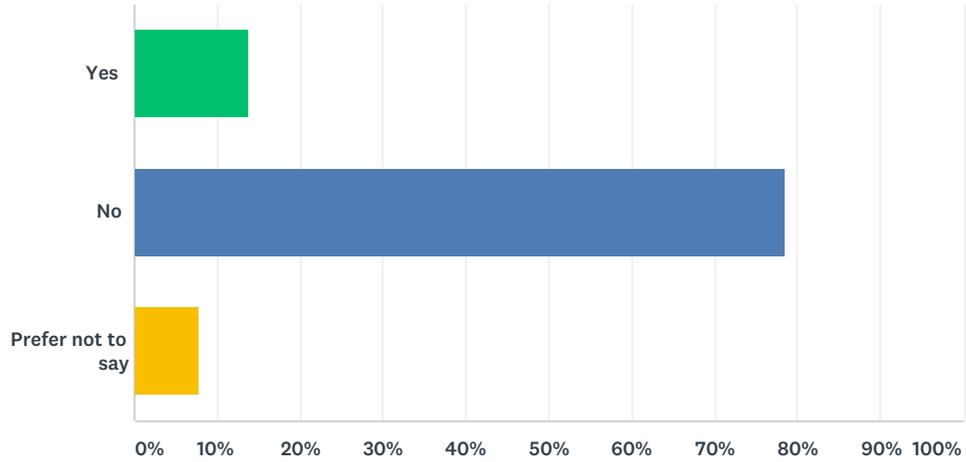


ANSWER CHOICES	RESPONSES
Female	47.45% 233
Male	48.07% 236
Non-binary/third gender	0.41% 2
Prefer not to say	3.05% 15
Prefer to self-describe	1.02% 5
<b>TOTAL</b>	<b>491</b>

#	PREFER TO SELF-DESCRIBE	DATE
1	I'm the one with Y chromosomes.	9/30/2017 2:58 AM
2	Attack Helicopter	9/24/2017 1:26 PM
3	Thanks	9/8/2017 8:55 PM
4	purple unicorn, but actually just a male who thinks this is a stupid question.	9/7/2017 8:28 AM
5		8/30/2017 1:06 PM

### Q13 Do you identify yourself as an Indigenous person? Here, we're including First Nations, Métis and Inuit people.

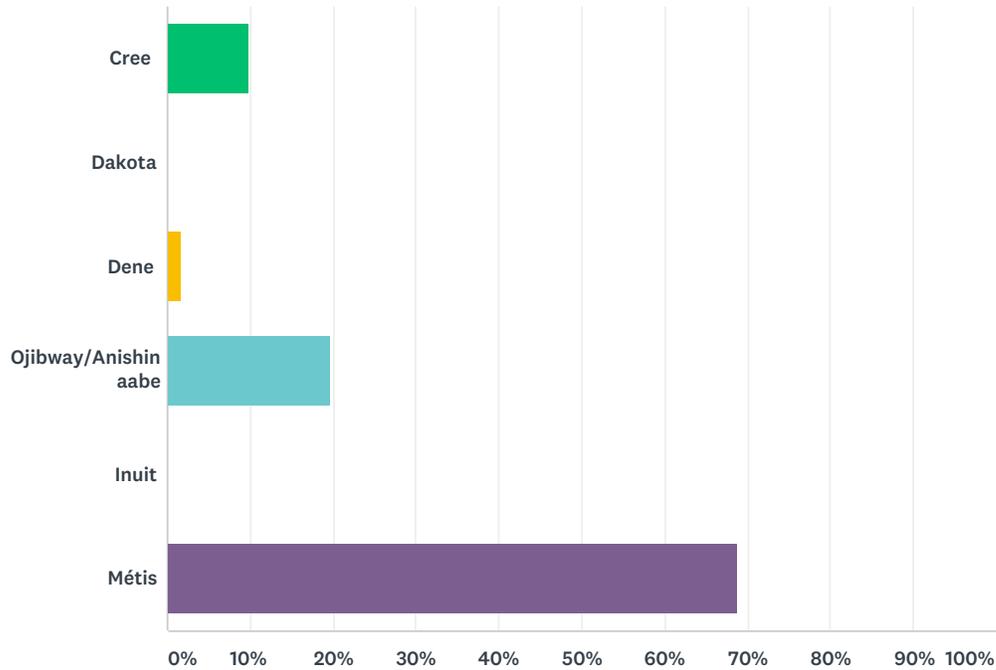
Answered: 493 Skipped: 14



ANSWER CHOICES	RESPONSES	
Yes	13.79%	68
No	78.50%	387
Prefer not to say	7.71%	38
TOTAL		493

## Q14 Please tell me a little more about your indigenous identity? Are you... (Please check all that apply)

Answered: 61 Skipped: 446



ANSWER CHOICES	RESPONSES
Cree	9.84% 6
Dakota	0.00% 0
Dene	1.64% 1
Ojibway/Anishinaabe	19.67% 12
Inuit	0.00% 0
Métis	68.85% 42
<b>TOTAL</b>	<b>61</b>

#	OTHER (PLEASE SPECIFY, IF YOU WISH)	DATE
1	and Metis	9/28/2017 2:19 PM
2	and Metis	9/28/2017 2:17 PM
3	Cree, Ojibway, Metis	9/28/2017 1:37 PM
4	Objibway/Anishinaabe & Metis	9/18/2017 11:43 AM
5	Mi'kmaq	9/7/2017 6:25 PM
6	This is a racist line of questioning.	8/30/2017 12:40 PM

**Q15 Why did you take time to respond today? Please share the aspects of the project that brought you here today and/or any other comments about the bridge and its design here.**

Answered: 341 Skipped: 166

#	RESPONSES	DATE
1	Because I think it is important to share my view on projects that effect us all	9/30/2017 4:26 PM
2	The question "do bridges make better cities". Kudos to the comms team for that one	9/30/2017 4:08 PM
3	tired of seeing the City waste money where not required	9/30/2017 2:23 PM
4	I don't want money wasted other than a way to cross the tracts	9/30/2017 1:28 PM
5	I responded because I have used the bridge often in the past, and because it is an iconic piece of infrastructure in this city.	9/30/2017 7:36 AM
6	Appreciate the opportunity to have a say in this project	9/30/2017 6:02 AM
7	I like wasting my time and deluding myself into believing some common sense is going to prevail in this goofy city.	9/30/2017 3:01 AM
8	Winnipeg infra is important to building a better city. Planning for sustainable community living is essential to improving Winnipeg.	9/29/2017 11:47 AM
9	This is an exciting project	9/29/2017 10:40 AM
10	I like the bridge	9/29/2017 9:57 AM
11	This is an opportunity to build an iconic bridge with features that contribute to the history of the area	9/29/2017 9:53 AM
12	Im interested in making Winnipeg as good as it can be	9/29/2017 9:29 AM
13	I love winnipeg	9/28/2017 9:58 PM
14	There are times that I would use the Arlington Bridge, but instead chose to use McPhillips Street or Salter Street because of better traffic flows. I feel that the Arlington Bridge needs to be a 4 lane bridge for cars, trucks, buses, etc.	9/28/2017 9:50 PM
15	Important for the strategic growth of the city, and optics for tourism	9/28/2017 9:39 PM
16	I use the bridge frequently and can see that it needs replacing.	9/28/2017 8:00 PM
17	This project is something I was interested in providing an opinion on.	9/28/2017 4:31 PM
18	I use this bridge on my commute. I also use it to get to other areas of the city on weekends. This bridge has a big affect on my daiy life.	9/27/2017 7:44 PM
19	Frequent use during my whole life.	9/26/2017 2:55 PM
20	I see the Arlington Bridge as an integral part of living in the North End.	9/26/2017 5:56 AM
21	curiosity and having always liked the bridge for biking and as a lookout point since youth	9/24/2017 7:54 PM
22	Because then I can criticize the final decision saying I gave my input.	9/24/2017 1:27 PM
23	bettere for winnipeg	9/23/2017 9:00 PM
24	This type of redevelopment / initiative is very important for the whole of the city.	9/23/2017 5:24 PM
25	I use the bridge for my commute. It's one of the few ways go travel from North to South in the city and I've seen how closing one of these routes affects traffic on all the others. I also think it's an important design feature in the city and I think the loss of it completely would be sad.	9/23/2017 4:54 PM
26	a volunteer at the north centennial pool	9/23/2017 3:12 PM

## A Better Bridge for Arlington

27	I care about construction here in WPG. As a taxpayer I want my bridges to be built right he first time.	9/23/2017 2:23 PM
28	\My family has lived in the neighbourhood for over 80 years. We understand there will be the expropriation of some houses and wonder whether or how we might be affected...	9/23/2017 12:12 PM
29	Because of there being only 3 traffic lanes in the design	9/23/2017 10:24 AM
30	The bridge is part of my history. I loved going over it as a kid. Please don't make some horrible looking thing, unique design has to have its limitations.	9/23/2017 9:02 AM
31	put input	9/23/2017 7:31 AM
32	It will improve the surrounding area, and attract more clientele for businesses	9/23/2017 1:04 AM
33	It's important residents have a voice in city projects and decisions.	9/22/2017 3:44 PM
34	Love the city and we should be proud of the really great people in the North end and our city	9/22/2017 3:31 PM
35	I think its important for the little guys vice to be heard. low income vioces usually get ignore in these kind of projects.	9/22/2017 12:32 PM
36	I think it's important spot for the community	9/22/2017 7:15 AM
37	Bridges are a city project and I am a member of the city.	9/21/2017 9:08 PM
38	cool project	9/21/2017 8:42 PM
39	I am passionate about cycling and walking to get where I need to go! I would like to use the Arlington bridge more. Even on the bus! It would be great to have a bridge that we could all be proud of.	9/21/2017 8:17 PM
40	Love the old bridge	9/21/2017 7:45 PM
41	strong feeling the rail yards should be moved	9/21/2017 3:40 PM
42	I think demolishing the existing bridge is waste full and the City looses an opportunity to be really creative.	9/21/2017 2:58 PM
43	I live and work in the vicinity of the bridge	9/21/2017 10:52 AM
44	I think bridges are important opportunities to define ourselves and create livable & innovative public spaces	9/21/2017 9:08 AM
45	I see a lot of potential for this project to bring some vitality and positive energy to this neighbourhood and area.	9/21/2017 9:01 AM
46	I would like to see a nice new bridge in that location that is functional but also offers spaces for people to enjoy being in the area, including the rail yards which I think are fascinating to have right in the middle of the city. It adds to the character of our home.	9/21/2017 8:13 AM
47	Having attended several meetings and heard the ridiculous items people wanted included with no regard to cost I had to speak up. The coffers are not limitless and I thought a little common sense should be included in the discussion.	9/20/2017 12:53 PM
48	I would like to see public safety as a first priority for those using the bridge as well as Arlington and nearby streets. I've heard there will be no parking on Arlington, and feel there still should be as many who work in the area need parking. Also, I've heard there will be no left turns off William onto Arlington (at the light). Please keep this as an option! So many of the streets around this area are conjested, and this is the most convenient spot for many to make the turn.	9/20/2017 12:25 PM
49	I am concerned about traffic flows south of the bridge and it's impact on Arlington Street.	9/20/2017 9:23 AM
50	I believe my participation may create an idea that may save time and money. I also want cycling in Winnipeg to be a major priority. I can become healthier, there is one less car on the road, less road repair and better for the environment.	9/19/2017 2:10 PM
51	Its important to me and my family	9/19/2017 1:32 PM
52	I use the bridge fairly frequently and it is in my area	9/18/2017 6:04 PM
53	-Because it matters. I work in this community. I want to see people have something to be proud of.	9/18/2017 12:31 PM
54	-We work with new comers + many are making area home. We want our communities to be inclusive + bridge can be a tool for inclusivity.	9/18/2017 12:28 PM

## A Better Bridge for Arlington

55	-Interested in the community	9/18/2017 12:09 PM
56	-I do believe in community renewal, insufficient focus on this area of the city. This is long overdue/embarrassing.	9/18/2017 12:04 PM
57	Interest	9/18/2017 10:55 AM
58	I love this city	9/18/2017 10:43 AM
59	I was born in the North end and I'm gonna die in the North end!	9/18/2017 8:00 AM
60	I would like to see 2 useable lanes for transportation/vehicles especially going towards the South as this is a busy route especially for people going to work coming from the North End. If possible, both sides of the bridge should have 2 lanes useable for vehicles at any time to decongest McPhillips St. There have been a lot of new housing developments in the North Side of the City increasing the population in this area. It is such a busy area not only for residents going to work, but also for people commuting taking their kids to school and vice versa.	9/18/2017 5:35 AM
61	It is an important connection between downtown and the north end	9/17/2017 5:44 PM
62	I slice the bridge should be 4 lanes, not 3	9/17/2017 9:11 AM
63	Development of active transport & "city-making" projects are important to me	9/16/2017 7:33 PM
64	Cause I care about wpg	9/16/2017 10:00 AM
65	I am a resident of the neighbourhood and use the bridge often to get to the other side of Arlington.	9/16/2017 9:47 AM
66	I live in the North End and care about having a say in development	9/16/2017 9:38 AM
67	This affects my commute and my community....	9/16/2017 9:22 AM
68	The bridge is important to the people.	9/16/2017 9:15 AM
69	Opinions on the bridge for the north end community. Mainly minorities.	9/16/2017 9:05 AM
70	I care about my neighborhood and the future state of our city	9/15/2017 3:22 PM
71	I think it is important to have a say in our city. The arlington bridge is a highlight of the city and we need to make sure that we have a say in what comes next	9/15/2017 12:25 PM
72	I am concerned about converting the streets from 2 lane to single lanes. I believe this will not help traffic in the area. It will worsen it.	9/15/2017 11:37 AM
73	I care about this city, and its ability to attract business. Having a bridge that moves taffic is key to business and public in the area.	9/15/2017 11:17 AM
74	Concerned about my tax dollars at work.	9/15/2017 9:41 AM
75	I live in the neighborhood,the bridge serves very well.	9/15/2017 9:24 AM
76	Creating a community that we all are able to travel in and enjoy, no matter our income or social status, is very important to me.	9/14/2017 7:29 PM
77	I have lived in the north end my whole life, and have crossed the bridge thousands of times. I am excited to see new development in the north end.	9/14/2017 7:26 PM
78	The development/construction of the new bridge will affect my daily commute.	9/14/2017 11:31 AM
79	Because I travel the bridge several times a week	9/14/2017 7:11 AM
80	The arlington bridge is part of my daily commute so the functionality is important to me. There is definitely a need for increased traffic flow over the bridge and particularly at the intersection of Logan and Arlington. Two northbound lanes should help to alleviate this conjestion.	9/14/2017 7:02 AM
81	the more people who discuss the safety of the bridge, the more it should be read	9/13/2017 2:52 PM
82	Important to the NE	9/13/2017 2:06 PM
83	It is imperative that a new one be built. That bridge looks like a disaster in the happening with all the rust	9/13/2017 12:58 PM
84	The city is my home. While I live just outside the limits now, it's growth and development are important to me and my future generations. Being able to share my input and potentially contribute to the bigger picture drew me to answer the survey.	9/13/2017 11:45 AM
85	<a href="http://www.cbc.ca/news/canada/manitoba/railway-move-winnipeg-1.3321053">http://www.cbc.ca/news/canada/manitoba/railway-move-winnipeg-1.3321053</a>	9/13/2017 11:36 AM

## A Better Bridge for Arlington

86	Due to the frequency at which I must cross over/under the CPR Tracks, I have a strong interest in the Arlington Bridge.	9/13/2017 9:01 AM
87	I know not enough people take the time to give their input, and too many take the time to complain afterwards. I like to speak up for my community whenever given the opportunity.	9/12/2017 9:15 PM
88	it is good to be a part of the city of wpg developments-since we are tax payers & our \$\$ contributions do count as well as our voices	9/12/2017 5:51 PM
89	Part of community, wan to learn	9/12/2017 4:54 PM
90	I saw the design on your website and I don't like the fact that there is two traffic lanes going in one direction and only one going in the opposite direction. I fail to see the point. Rush hour traffic going one way will likely travel back going the other way.	9/12/2017 4:41 PM
91	I think community voices are important	9/12/2017 1:44 PM
92	The health and safety of the area is important to me.	9/12/2017 1:32 PM
93	Heard that there would be changes to area traffic flow. May be problematic for people working in the area. I was also looking to see if active transportation on Arlington was being discussed.	9/12/2017 9:42 AM
94	I notice that governments concentrate on appearances rather than practical uses. A simple design with 4 lanes would be best similar to the Slaw Rebchuk bridge.	9/12/2017 8:46 AM
95	Wanted to express a vote for active transportation, especially bike lanes on both approach and bridge itself. Presently it is a bit of an adventure that most people are not up to to go by bike over the Arlington street bridge. Really hoping that is going to change with the new structure.	9/12/2017 8:43 AM
96	this project is way over due and likely not finished in my lifetime. Traffic flow and other infrastructure in north end has been lacking development	9/12/2017 7:45 AM
97	I love the North End. It's my home..We support local businesses, restaurants, use facilities, credit union,etc. don't disdain, ignore or not cherish what makes a city liveable..we can't all afford or want the dead no name feel of the crazy build up outside the inner core	9/12/2017 2:01 AM
98	This is my way to my work every day at home again this is important for me I was also hoping to find out when they might start working on this project yes we have a 20 23 finish date when will they start	9/11/2017 10:35 PM
99	I lived in that area all my life, and we need a bridge there.	9/11/2017 6:04 PM
100	Want to make sure Metis are represented	9/11/2017 4:11 PM
101	I cross the bridge and want the project to be a source of pride for the city	9/11/2017 3:09 PM
102	It will increase traffic congestion at my place of work, reduce onstreet parking and stop left-turns from the works exit onto Arlington- very inconvenient	9/11/2017 2:09 PM
103	Work in and around the community	9/11/2017 12:59 PM
104	I used to drive the bridge every day, so I am familiar with it and wanted to have my say.	9/11/2017 12:54 PM
105	Civic issues and infrastructure are important to me	9/11/2017 12:45 PM
106	Because I do not see great imagination in build projects in Winnipeg	9/10/2017 8:05 PM
107	I saw the mail out and wanted to be involved in some way because i cross that bridge often.	9/10/2017 7:17 PM
108	Bxxx bxxxxx told me to	9/10/2017 6:06 PM
109	Long time resident of north end and feel it's important that this project work out well on time and on budget.	9/10/2017 6:06 PM
110	Because I think it will be a major loss if they don't consider proposals, such as they have done in countless cities, to repurpose the old bridge. Make it a contest, integrate it. Create an attraction that people may actually want to come see. Give the north end area a beautiful and prideful attraction that may bring other that otherwise may never go.	9/10/2017 5:32 PM
111	Because I like that bridge, current one. I have always envisioned it painted yellow, like the ones in Pittsburgh.	9/10/2017 4:30 PM
112	The bridge allows major north /south access for people who live in the north part of the city but work south of the rail tracks.	9/10/2017 1:42 PM

## A Better Bridge for Arlington

113	I would love to see our city build an amazing structure and add some nice gardens or park area to this part off our city. The residents close to there deserve it.	9/10/2017 10:28 AM
114	I use the bridge daily and want some say in design	9/10/2017 9:49 AM
115	I have been interested in this project for the past 3 years, ever since I first heard about it. I used to live 2 blocks away, though now live further east in the North End.	9/10/2017 9:11 AM
116	It's a change in the inner city community	9/10/2017 7:30 AM
117	very important to be a beautiful bridge safe for cyclists and pedestrians as well as drivers	9/10/2017 1:06 AM
118	Although the trend is to try to address various cultural issues that may be pertinent to an area, I wanted to put forth my opinion in trying to avoid attaching any relationship between the bridge and indigenous peoples. Although the North end may indeed be the home of the majority of the indigenous people in Winnipeg, the prevalence of other cultures and races has exploded in the area. Placing an aboriginal/indigenous relationship to the bridge doesn't do the North end justice and borders on profiling the area. I would far prefer the bridge to be attached to all cultures, and not just one. Let's do the area justice, and recognize the many varieties of people who populate it.	9/9/2017 11:18 PM
119	I care	9/9/2017 10:46 PM
120	I really am passionate about deminishing the stigma around the north end and think that the new bridge will allow this!	9/9/2017 8:26 PM
121	It's an important landmark. It's a well used bridge and needs to be rebuilt. Opportunity for community to grow. I'm invested in the community.	9/9/2017 7:37 PM
122	I use the bridge everyday so it is important to me. When the bridge is closed commuting becomes a nightmare	9/9/2017 5:17 PM
123	I love the bridge, and it an important North-South artery of my community.	9/9/2017 11:15 AM
124	I saw the article in the North End times and am concerned about transportation during bridge construction.	9/9/2017 10:32 AM
125	Because the existing bridge is iconic and the project spans neighbourhoods which should see great positive impact as a result of it.	9/9/2017 7:26 AM
126	Winnipeg needs more opportunities for active transportation.	9/9/2017 12:56 AM
127	Would love to see major improvements to the North End Community	9/9/2017 12:53 AM
128	My children lovingly refer this bridge as Nanay's Bridge. It's the bridge we cross to go to my parents home (Nanay is their grandmother). Looking forward to the changes and the fact it's being replaced and not completely closed down.	9/8/2017 9:59 PM
129	Hoping to provide support for cyclists.	9/8/2017 8:35 PM
130	It's important to be heard.	9/8/2017 8:17 PM
131	It's a good opportunity for the city to improve so I wanted to support this initiative.	9/8/2017 6:57 PM
132	Coz I know with that bridge the impression that the north end is getting will change	9/8/2017 6:02 PM
133	I am interested in the city diverting the money allocated for beautification and public art of the new bridge to go into repurposing the old bridge for a pedestrian only public space walkway. Yes, we need a new bridge for traffic, but we have an opportunity to create an iconic public space (e.g. high line in New York) to re-activate this neighbourhood.	9/8/2017 4:00 PM
134	I love my community, the north end, and the bridge is an important link to downtown and the rest of the city. The rail yard segregates my community from downtown.	9/8/2017 2:18 PM
135	The potential for this project to serve the community its in is exciting.	9/8/2017 2:05 PM
136	I care about the improvement of the North End and I think it currently struggles from being poorly connected to downtown. Building a bridge that works for pedestrians and cyclists is important, especially a neighbourhood with low car ownership. It would be sad to see the old bridge go down, when we have the opportunity to reuse it for an amazing public space, rather than building a showpiece.	9/8/2017 1:51 PM
137	would like to see the old bridge continue to exist in some capacity, both as art, as history, but also as an active living transportation corridor for cyclists	9/8/2017 11:26 AM

## A Better Bridge for Arlington

138	I think it's foolish to put this much money into a bridge when moving the rail yards should have been the priority and I wanted the opportunity to voice my opinion	9/8/2017 11:25 AM
139	Because Winnipeg usually sucks at construction projects	9/8/2017 11:22 AM
140	I have general interest in the betterment of the City	9/8/2017 11:20 AM
141	I care about Winnipeg and its people.	9/8/2017 10:30 AM
142	Interested in final design	9/8/2017 9:31 AM
143	I feel it is an important site in the heart of the city.	9/8/2017 8:37 AM
144	Preserving the old bridge, community building and vision for the future are incredibly important to me as a citizen who wants to see placemaking measures incorporated to improve the Winnipeg experience. This is a rare opportunity to learn from other jurisdictions and really make an impact for generations to come on a neighbourhood, increase and enhance connectivity and showcase the incredible culture and art in our city.	9/8/2017 7:34 AM
145	I think this is an important project for the city and an important link to the 'north end' that can connect two different neighbourhoods and businesses	9/8/2017 7:09 AM
146	It's important to have community input	9/8/2017 5:54 AM
147	I value doing my part as a community member. And also am willing to assist in any way as a community volunteer.	9/7/2017 10:09 PM
148	The city needs better direction with its planning... Stop the dependance on cars.	9/7/2017 10:08 PM
149	I care about city design and infrastructure	9/7/2017 9:59 PM
150	I want better connectivity between areas of the city. This bridge could help create an amazing bike route from the North to the core of the city.	9/7/2017 9:37 PM
151	I was concerned about bridge affects on traffic flow in the area; opportunity to consult appreciated; would like to see more active transportation options throughout the city for recreational riding and commuting by bike.	9/7/2017 9:16 PM
152	Its a beautifull bridge. It should represent the hard work ethic that brought it into being in the first place.	9/7/2017 8:46 PM
153	Attended public meetings to speak for saving the bridge.many people spoke in favour if that idea. None of it was reflected in summary of comments.	9/7/2017 8:38 PM
154	Please incorporate active transportation. Consider using the old bridge as a community space with bike path.	9/7/2017 8:25 PM
155	for the love of our city for the love of the north end for the love of this iconic bridge	9/7/2017 8:23 PM
156	I think it is our civic duty to participate in the future of our community. That's why I am here. Also, I find this project of interest because it will have a large impact on the landscape of our city. I would love to participate in more important municipal discussions like this - I want my voice to be heard and I want to help shape our city.	9/7/2017 8:23 PM
157	I believe in urban revitalisation	9/7/2017 8:05 PM
158	Don't tear down the old bridge... repurpose as a park, bikeway, public space for entertainment and recreation. Cost effective, cheaper than tearing it down.	9/7/2017 8:05 PM
159	I care about our city and the opportunities we have to better it!	9/7/2017 8:04 PM
160	Arrlington bridge has always held special meaning... Lived near it as a kid. Was a challenge to walk over!	9/7/2017 7:47 PM
161	I think it's important to hav input in the city's growth. Want to see more bikeways more mixed business/residential including affordable housing. Want to see a space that meets needs of existing community	9/7/2017 7:39 PM
162	Tired of boring looking bridges in Winnipeg.	9/7/2017 7:18 PM
163	I hope that the bridge is unique and incorporates pedestrians and cyclists and is a community space!	9/7/2017 7:06 PM
164	I used to love in the north end and used that bridge frequently. I loved seeing the train tracks and always thought it was a bridge that needed a facelift and allowed for pedestrians to enjoy the view	9/7/2017 7:04 PM

## A Better Bridge for Arlington

165	As an Civil Engineer in my Country, during my first year in Winnipeg Canada (2009) when I passed at the Arlington bridge I saw that the bridge need to replace or need retropiting due to its weak condition and I saw alot of vacant land that doesn't use and doesn't the city to attracts tourist. That why when I received the information about the bridge I',m so excited to see what the design and development for that area because it is my dream to see the bridge bridge in a strong, unique and attrarctive design to attracts tourist and make a city can create income.	9/7/2017 6:31 PM
166	I work and go to school at the U of M health sciences campus. I have worked and studied in this area since 2001 and have grown to think of this neighbourhood as one of my homes.	9/7/2017 6:27 PM
167	To encourage creative development of this city, with an eye towards long term planning.	9/7/2017 6:07 PM
168	The fact that the City loves to waste money on making "pretty" structures instead of focusing on core infrastructure and using beautification money for better purposes.	9/7/2017 5:51 PM
169	Interested in the project	9/7/2017 5:14 PM
170	I read Brent Bellamy's Winnipeg Free Press article proposing the repurposing of the existing Arlington Bridge into an urban park, similar to the High Line in New York. I think this would be an incredibly forward-thinking, exciting idea with almost endless potential (a park? commerce? a community centre bridging the north & south end? a skating rink? etc). I'm excited by the potential of the new bridge, and would be thrilled to see the feasibility of repurposing the old bridge studied.	9/7/2017 5:14 PM
171	wanted to	9/7/2017 4:22 PM
172	I believe my voice in city activities is important. I'm very upset that i missed budget consultation.	9/7/2017 4:20 PM
173	I am interested in the creative process, my civic responsibility to my community as well as the promotion and pride I have in being from Winnipeg and wanting to show the rest of the world that there are reasons to come here to live or just to check out. We don't need a plain old bridge replacing the industrial mess already present. We need a piece of art that is a living extension of the surrounding communities and the natural environment here in Manitoba!	9/7/2017 3:45 PM
174	It is an important opportunity to stregthen a largely disadvantaged community	9/7/2017 3:31 PM
175	To say don't spend on extras just put a new bridge up	9/7/2017 3:17 PM
176	The Arlington Bridge is vital to many Winnipegger's commutes-including mine. I personally love the bridge and will be sad to see it go, but will appreciate a more modern & efficient bridge in its place. I think it is important to express my opinion on this project as it affects me in my daily life.	9/7/2017 2:48 PM
177	Very important communiury defining project	9/7/2017 2:30 PM
178	I live in Garden City, so I use the bridge frequently. I'm very interested in the topics of urban development and freeways in Winnipeg (we're the only large city with no freeway close to downtown; the perimeter highway is way too far away from where people live.).	9/7/2017 2:29 PM
179	Just my input	9/7/2017 2:25 PM
180	Pride in my city	9/7/2017 2:10 PM
181	City development for recreation and nature preserve important to me	9/7/2017 2:04 PM
182	Engineering interest	9/7/2017 1:54 PM
183	I think that bridge is an important and iconic part of the Winnipeg landscape. I would prefer for it not to be torn down/replaced at all, so ideally most of the bridges 'look' and character will remain after it is replaced.	9/7/2017 1:54 PM
184	I use the bridge, since I live 5 blocks North of it.	9/7/2017 12:54 PM
185	I,we, live in the neighbour hood and we would like a voice.	9/7/2017 12:40 PM
186	My neighborhood	9/7/2017 11:15 AM
187	Because I feel it's important that the City of Winnipeg know how the people of the area feel and what's important to us.	9/7/2017 10:08 AM
188	grew up in garden city	9/7/2017 8:29 AM
189	i was interested in the traffic changes that were not addressed in this survey	9/7/2017 8:20 AM
190	The area of the city seems to get left behind in large development project, it only gets bad press. Simply, it deserves something nice.	9/7/2017 7:15 AM

## A Better Bridge for Arlington

191	Proud of my community and want what's best for not only today but generations to come.	9/6/2017 11:33 PM
192	I use the bridge every day on my commute to and from work. It is a project that needs to get started.	9/6/2017 5:42 PM
193	was curious and wanted to give my input for major projects in my community.	9/6/2017 4:38 PM
194	The design divides the north side residents into east and west of the bridge. The design should not be used.	9/6/2017 2:57 PM
195	City Pride -	9/6/2017 2:47 PM
196	Came to see if there was information regarding changes to near-by work location	9/6/2017 10:25 AM
197	Where is the cost/benefit analysis ? where is the condition report?	9/6/2017 7:42 AM
198	I enjoy keeping updated with projects around the city and love to give feedback on ideas that could shape our city	9/5/2017 9:27 PM
199	I love Winnipeg's bridges and I like participatory democracy.	9/5/2017 7:30 PM
200	I love that bridge - it's nostalgic both because of how fun it was for my son (when younger) to go up/down a steep bridge and also because I used to drive it every day for work. It feels like an important part of Winnipeg, with character.	9/5/2017 4:13 PM
201	I work near the bridge, traffic in the area affects my commute	9/5/2017 2:43 PM
202	Because my tax dollars are no being spent in the proper places	9/5/2017 1:15 PM
203	I was told the the project will affect traffic flow at William Avenue and Arlington. This traffic flow should not be disrupted as it will create many other issues at neighbouring intersections.	9/5/2017 1:14 PM
204	Participating in any opportunity that involves public engagement is important to me.	9/5/2017 12:08 PM
205	I have become very interested in improving the city in terms of making it a beautiful and inclusive place.	9/5/2017 11:57 AM
206	I work within a block of the bridge and I do use it multiple times every week.	9/5/2017 10:04 AM
207	I work at a lab near by and like to walk over the bridge at lunch.	9/5/2017 10:00 AM
208	Because they are planning on reducing Northbound traffic to one lane on Arlington which would take away parking space for employees at the National Microbiology Lab	9/5/2017 9:53 AM
209	work close by and it affects traffic flows	9/5/2017 8:49 AM
210	Work right near south end.	9/5/2017 8:30 AM
211	The bridge needed to be updated	9/5/2017 7:43 AM
212	I use Arlington Bridge every single day going to work. When it is not available, the traffic is tremendous. It cannot close!	9/5/2017 7:28 AM
213	I want to see a successful project that will help the area of the North End. There is lots of negativite thoughts travelling into the area and I think this bridge could become a bright spot for the future of the area.	9/5/2017 6:28 AM
214	The existing bridge is extremely important. Public consultation has not given oroper consideration to retaining the bridge.	9/4/2017 10:52 AM
215	I pay taxes, therefore I have a say in what happens to my money.	9/3/2017 11:24 PM
216	I hope thos bridge is done right for that area.	9/2/2017 8:53 PM
217	been involved since the beginning w. community consultation	9/2/2017 9:57 AM
218	I have strong feelings about the Arlington Bridge. We should be moving the railyard.	9/2/2017 9:51 AM
219	The development in our neighbourhood is important to me.	9/2/2017 9:14 AM
220	Its the growth and future of winnipeg	9/1/2017 11:41 PM
221	I have resided in the Northend of City of Winnipeg all my life. it is an important structure to connect our neighborhood with the rest of city. It should be treated as a Hub	9/1/2017 7:25 PM
222	It is time to make the north end, visually, beautiful and get rid of the saying "the dirty north"	9/1/2017 4:20 PM

## A Better Bridge for Arlington

223	When you are born in the North End it is always in your heart. I have great respect for everyone. I still drive by the neighbourhood where I lived over sixty-five years ago.	9/1/2017 1:04 PM
224	Changes will affect my commute to and from work. I'm concerned about only 1 southbound lane on Arlington. At which point does it become 2 lanes? Also, Arlington northbound has a diamond/bike lane? Is it combined? I'm sure all the details are still being worked out but more details would be nice.	9/1/2017 12:58 PM
225	I don't want to see city money being put into a "fancy new bridge" when it could be put towards other beneficial projects.	9/1/2017 12:55 PM
226	Wanted to provide some input on the bridge design and outcome.	9/1/2017 12:20 PM
227	I took time to reply because the Arlington street bridge is a very important feature of the North End.	9/1/2017 12:06 PM
228	Have not enjoyed biking the current bridge!	9/1/2017 12:02 PM
229	because I feel that the people/ community is not of any real concern to the project managers, and they very well should	9/1/2017 11:39 AM
230	The bridge is an important piece of urban infrastructure. Its reconstruction is an opportunity to connect or reconnect communities on either side of the railyard.	9/1/2017 11:34 AM
231	I felt it was important to have a say in this	9/1/2017 10:31 AM
232	I am concerned about the intersection at William and Arlington. In the video it is indicated that there will be no more left hand turns onto Arlington from William. This is a bad idea. Watch that corner during evening rush hour and you'll see most people use that intersection to turn (people from the hospital, nml, etc).	9/1/2017 10:28 AM
233	Wanted a say in the bridge I've been using for 40 years and will continue to use in the future.	9/1/2017 10:01 AM
234	I work on Arlington between Logan and William Ave and frequently have to park my vehicle on Arlington street. It would be a huge inconvenience if the proposal goes through with eliminating parking on Arlington street. Parking is so limited already, I think that would be a terrible idea!	9/1/2017 9:37 AM
235	I work at CSCHAH and it will affect the parking on Arlington in front of CSCHAH	9/1/2017 9:29 AM
236	I work near the bridge and can see it from my building and the traffic in the neighborhood affects me	9/1/2017 9:24 AM
237	I'd like to try and help make the area more inviting and enjoyable for all people.	9/1/2017 8:27 AM
238	Worried how the new bridge will affect traffic coming and going from my workplace.	9/1/2017 8:25 AM
239	Work encouraged us to do so.	9/1/2017 7:54 AM
240	I try to avoid using the current Arlington bridge because it is very steep and I hate driving over it. I am hoping the new bridge will be an improvement to the neighbourhood and improve traffic flow on and around the bridge.	9/1/2017 7:45 AM
241	Workplace is very close by, the work on Arlington Brindge will affect my commute	9/1/2017 7:40 AM
242	concern about changes to Arlington and William Street, I work at the National Microbiology Laab and proposed street changes will make it difficult to get to work	9/1/2017 7:33 AM
243	Want a fast replacement that does not mess up traffic flow.	9/1/2017 6:54 AM
244	I work near by and my commute will be affected. It is already difficult to turn north onto Arlington between william and logan and I see that northbound will be reduced to one lane at this point. I am concerned that this change will increase the difficulties my staff and I will endure in the future.	9/1/2017 6:54 AM
245	The design is flawed. I work at the lab and live in the west. Exiting from the lab is already challenging onto Arlington from the front of the building and eliminating turning left from William to head south is very restrictive to those that live and work in the area. A new design should enhance access not restrict it. The lab and surrounding hospitals employ a lot of people and restricting how they can access Arlington from William is a mistake.	9/1/2017 6:48 AM
246	Because it will effect my drive to work	9/1/2017 6:35 AM
247	I work close to the bridge.	9/1/2017 6:35 AM
248	I hate the idea of building a bridge over a rail yard that needs to go.	8/31/2017 9:16 PM
249	A voice in the plan.	8/31/2017 9:13 PM

## A Better Bridge for Arlington

250	Wondering why no more left turns from William to Arlington? That's a route I use a lot, so not sure how this will impact me.	8/31/2017 4:14 PM
251	I am interested in urban planning and think that the North End has been severely neglected for over a century. I want to see the city do a better job of consulting with stakeholders - not depending on digital media - because so many lower income families are not consulted this way.	8/31/2017 4:12 PM
252	Because I frequently use the bridge	8/31/2017 3:55 PM
253	I am concerned about access to William from Arlington and vice-versa	8/31/2017 3:29 PM
254	I use the bridge often and I think it's important to share an opinion on a structure that is developed in our city	8/31/2017 3:28 PM
255	I work at the CSCHAH on Arlington, so will be quite affected by the proposed changes. I love the importance being placed on appearance and culture in this new plan. Unrelated to the bridge, eliminating left hand turns from William onto Arlington will hugely inconvenience a lot of people. It's also not a particularly hard turn to make.	8/31/2017 3:22 PM
256	I thought this survey would also deal with other aspects of the project. The changes to Arlington St south of the bridge to be specific. The plans appear to have northbound traffic on Arlington reduced to a single lane to accommodate the addition of bike lanes. The second lane on Arlington is used as parking for many employees of the businesses nearby. With the limited amount of parking available on side streets it would be in poor taste to eliminate treasured parking space. Additionally, reducing northbound Arlington to one lane will also cause severe congestion during morning commutes as there are many who want to turn left onto the side streets. I think the project managers/engineers should re-evaluate reducing northbound Arlington to a single lane.	8/31/2017 2:56 PM
257	improving winnipeg, and gateways to the north end improve our city and the stigmas associated with the north end (and the city as a whole)	8/31/2017 2:00 PM
258	I believe that a new bridge is very important, and the only solution.	8/31/2017 1:37 PM
259	I am disappointed that you didn't ask about our thoughts on the functionality of the bridge and only the aesthetics. I use the bridge multiple times a day and although I'm sure most of the suggestions or thoughts would be similar, you may get a unique idea you never considered.	8/31/2017 1:21 PM
260	I use the bridge almost everyday and it is my main route to work.	8/31/2017 1:21 PM
261	Because I use this bridge every day and it very important to me.	8/31/2017 12:45 PM
262	It is a significant investment of my money as a taxpayer. This project is overdue and since the opportunity to provide feedback is there, I want to give it.	8/31/2017 12:41 PM
263	Bridge used frequently. Important to me. SAFETY IS ALWAYS #1 ... please remember this :)	8/31/2017 12:39 PM
264	See it out the window, when looking north, from work.	8/31/2017 12:25 PM
265	When Arlington Bridge was closed for maintenance recently, I realized how vital it is to traffic flow. I think it is a very important piece of architecture.	8/31/2017 12:18 PM
266	Interest in building, architecture, long-time resident of North and West ends	8/31/2017 11:00 AM
267	Nice to have a say in a project in the area.	8/31/2017 10:51 AM
268	The current bridge is rather unique and carries the Nile River "myth".	8/31/2017 10:30 AM
269	This is an important gateway in our city.	8/31/2017 10:06 AM
270	I use the bridge everyday and live close by	8/31/2017 9:09 AM
271	It is important to me as a community member. I am very proud to be from Winnipeg.	8/31/2017 8:24 AM
272	I think it's time to give a new Arlington bridge because every year it's being fixed	8/31/2017 7:45 AM
273	It's a big deal as the bridge is our connection to the rest of the city. And when I heard of the space at the bottom of the bridge I saw it as a good place for the community	8/31/2017 5:31 AM
274	design, user experience	8/31/2017 12:03 AM
275	to make the north end a better place	8/30/2017 10:37 PM
276	I think it is exciting you are getting the community's thoughts.	8/30/2017 9:46 PM
277	I want my voice heard	8/30/2017 9:41 PM

## A Better Bridge for Arlington

278	I love the Arlington Bridge and will miss it. You're undertaking a huge project that will be around for years (maybe 100 or more!) please do something that is a worthy successor to the beautiful Arlington Bridge.	8/30/2017 9:25 PM
279	I responded because I don't want the project to turn out like the disrelli bridge. It was rebuilt with not enough lanes for traffic to flow as well as bicycles to ride safely. Please do not make the same mistake with this bridge project. Winnipeg is growing and traffic is becoming a problem. Build for the future.	8/30/2017 9:06 PM
280	It's part of Winnipeg. I think everyone should be involved with a major renovation like this.	8/30/2017 9:05 PM
281	It may be the only chance I get to give my input.	8/30/2017 8:54 PM
282	I've lived in the north end for a large part of my life. It's always felt close but cut off by the train yards. If there is a way to open it up and make it more inviting for people to explore their city. I know we would love to take family bike rides south down Arlington to Wosely and on to Assiniboine park.	8/30/2017 8:50 PM
283	Interested in the changes to Winnipeg	8/30/2017 8:45 PM
284	I cross it each day on a bike between Wolseley and Seven Oaks Hospital. I want to ensure bike riders were included in the results.	8/30/2017 8:32 PM
285	It's part of the city in which I live.	8/30/2017 8:00 PM
286	To make sure we get a beautiful bridge	8/30/2017 7:59 PM
287	The Current bridge is my favorite bridge in the city and I prefer cycling over it vs. the slaw rebchuck because it just has one quick hill, then its flat all the way. Instead of a huge arc like the slaw rebchuck which is just a never ending hill.	8/30/2017 7:58 PM
288	The design should really be part of our city's skyline.	8/30/2017 6:47 PM
289	We need to just have more available options to go from one side of city to the other	8/30/2017 6:33 PM
290	I think there are better ways to spend city money than on replacing this bridge.	8/30/2017 5:53 PM
291	The bridge should represent the heart of the north end and have a design based on native values	8/30/2017 5:45 PM
292	I responded because I want to see more lanes available.	8/30/2017 5:27 PM
293	The bridge is my favourite in the city so I'm going to miss it. If we have to replace it, I hope it's not something bland.	8/30/2017 5:07 PM
294	exercising my right to voice an opinion	8/30/2017 3:47 PM
295	Think rail relocation is a much better idea than rebuilding this bridge and building occasional underpasses	8/30/2017 3:46 PM
296	Bridges in Winnipeg are important!	8/30/2017 3:43 PM
297	The bridge is long overdue, and Arlington St. itself has become an improved route ever since it was resurfaced a few years ago north of the bridge in particular. The only thing that has interrupted the ride is a bridge that has been totally inadequate for four decades at least.	8/30/2017 3:32 PM
298	Because I care about the vision and direction of our city	8/30/2017 3:25 PM
299	300 Million could be spent better elsewhere	8/30/2017 3:06 PM
300	Because I think it's ridiculous the design ideas for this and the fact that you show only one lane going south.	8/30/2017 3:04 PM
301	I am concerned about the section of Arlington with one lane. Emergency vehicles will have a hard time getting through these during rush hour. Single lane will also create accidents with cars changing lanes in traffic.	8/30/2017 2:59 PM
302	I don't want to sacrifice function or affordability for form or extras.	8/30/2017 2:51 PM

## A Better Bridge for Arlington

303	This is a major project for the city and will have a lasting impact on our future. It's important it is done right, the bridge itself is something to be celebrated. I would like to add that the city of Winnipeg and any level of government involved using public funds to memorialise MMIW would be grossly cynical lipservice. I would not mind a memorial to the cause, but it is something best addressed by NGO's. MMIW is not a dark part of our history it is an ongoing problem and for any level of government to put funds in to something like this instead of focusing its resources on aspects it has authority over such as RCMP training, education, outreach etc. would be hypocritical and would amount to nothing more than positive PR for whichever politician is at the ribbon cutting ceremony.	8/30/2017 2:46 PM
304	I don't want the bridge to close!	8/30/2017 2:41 PM
305	the city budget should be better balanced	8/30/2017 2:28 PM
306	I Use the bridge 3-4 times daily.. the current one is Junk	8/30/2017 2:27 PM
307	To provide input on the bridge design and to make a statement on why there was never any consideration given to the bridge that was proposed in the early 80s connecting Sherbrook and McGregor streets.	8/30/2017 2:21 PM
308	I care very much about connecting our communities via healthy, sustainable transportation. I want all communities to be accessible.	8/30/2017 2:10 PM
309	I have been happy since the announcement to keep a bridge at this location. It concerned me when a few years ago there was talk to remove the bridge and not replace it with another option other than one of the other existing routes. I appreciate the investment in the North End of the city, as I have lived here all my life.	8/30/2017 2:04 PM
310	Accessibility and active transportation	8/30/2017 1:46 PM
311	use the bridge often, interested in its future	8/30/2017 1:44 PM
312	Would definitely use bridge more if safe cycling routes existed, as a local resident who cycle-commutes, there is no traffic-safe crossing the railroads from Keewatin, McPhillips, or Salter bridge.	8/30/2017 1:41 PM
313	I care about the positive, community oriented growth of our city.	8/30/2017 1:32 PM
314	For the benefits of everyone	8/30/2017 1:10 PM
315	I want to save the city from the heathens of the North End.	8/30/2017 1:07 PM
316	I am a member of the neighbourhood and use the bridge often!	8/30/2017 1:05 PM
317	Sxxxx Sxxxxxxx stonewalled sharing any and all information concerning existing traffic counts and volumes. City of Winnipeg public consultations are a worthless expense. Double lanes need to be consistent throughout, otherwise don't bother with double lanes.	8/30/2017 12:44 PM
318	i live in the west end and the bridge affects my community. i'd also like to use the bridge more - not just quickly drive over it in our car.	8/30/2017 12:34 PM
319	Important chance to create another landmark	8/30/2017 12:02 PM
320	It's an opportunity to be heard.	8/30/2017 11:55 AM
321	We like the Arlington bridge as it is and are sad it's getting replaced! (Although of course it must need to be structurally). Want the new bridge to fit the area and not be an endless boondoggle.	8/30/2017 11:46 AM
322	I was hoping that the rail yard would be moved completely eliminating the need for a bridge but if the city has chosen to proceed with a new bridge then I would like it to be architectually beautiful	8/30/2017 11:37 AM
323	I want my voice to be heard	8/30/2017 11:28 AM
324	Add voice. Raise the idea of moving the rail yards instead of replacing the bridge - far more valuable investment of funds!	8/30/2017 11:21 AM
325	My hope is that the current bridge will stay open until the new one is built. Two north bound lanes is imperative for evening rush hour!!! all other tech details I will leave to your engineers! GET IT DONE!!	8/30/2017 11:19 AM
326	To give my input on much needed services.	8/30/2017 11:16 AM
327	to be a voice in the planning for the community aspect	8/30/2017 11:04 AM

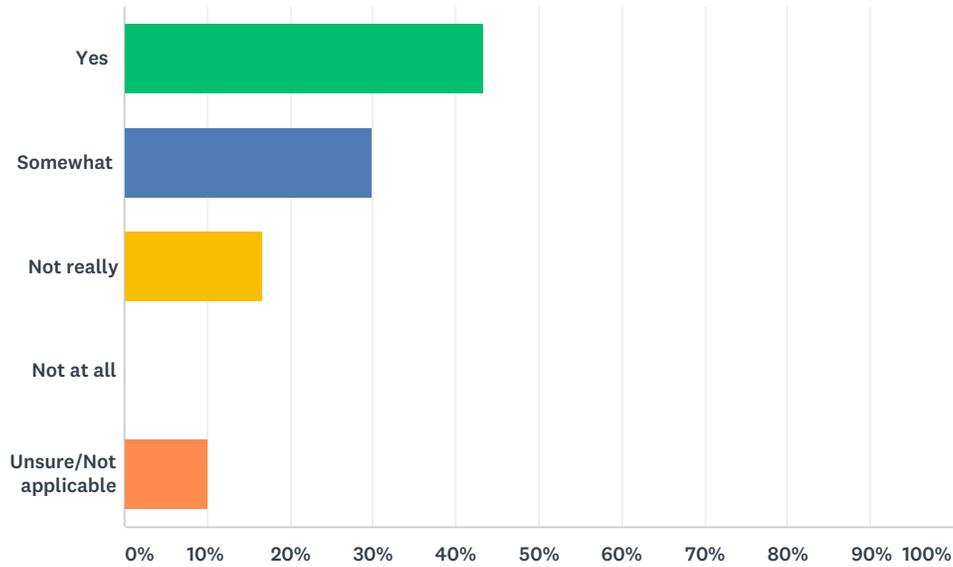
## A Better Bridge for Arlington

328	I care about my city	8/30/2017 10:48 AM
329	This is a	8/30/2017 10:43 AM
330	I hope that the bridge gets improved.	8/30/2017 10:42 AM
331	I live in the North End; I often hear people say the North End is not a safe place and ugly. I hate it. Its about time that the North End is seen as modern, safe and a great hang out place too.	8/30/2017 10:30 AM
332	Important civic issue	8/30/2017 10:25 AM
333	I have family in the north end and use the Arlington bridge when I visit, It is important to me to see this connection between the north and south strengthened	8/30/2017 10:21 AM
334	Like the input	8/30/2017 10:21 AM
335	I think it's important that the public voice their opinions	8/30/2017 10:20 AM
336	It is an important bridge around my workplace	8/30/2017 10:11 AM
337	I use the bridge daily and I love the City of Winnipeg, we need to build beauty where it's hard to find.	8/30/2017 10:10 AM
338	Because I want the city to continue developping and being a better place to live. We need to convince the rest of the country we aren't "just winnipeg". We are a place people actually want to come visit.	8/30/2017 10:07 AM
339	Arlington is very important bridge in the City, and the area around the bridge has a lot of potential to be redeveloped to help strengthen Winnipeg. I want to hear that Mayor Bowman is supporting building a new bridge. The bridge is an important connecting point between the inner city and north end.	8/30/2017 9:07 AM
340	Citizens should be proud when they see the new bridge.	8/30/2017 7:23 AM
341	I am a youth interested in the design and engineering of cities and this interested me.	8/29/2017 6:59 PM

Pages 77 to 84 were removed from the Survey Monkey summary as they contained respondents personal email addresses and contact information.

## Q1 During the project have you been able to find the information you were looking for?

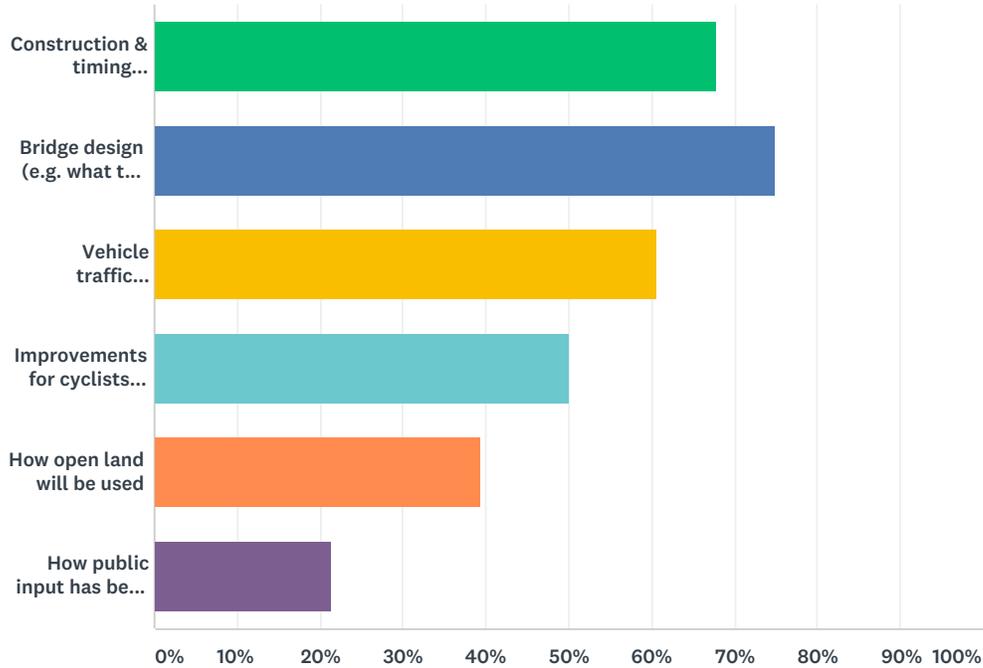
Answered: 30 Skipped: 1



ANSWER CHOICES	RESPONSES	
Yes	43.33%	13
Somewhat	30.00%	9
Not really	16.67%	5
Not at all	0.00%	0
Unsure/Not applicable	10.00%	3
<b>TOTAL</b>		<b>30</b>

## Q2 Specifically, what information were you looking for? (Select all that apply)

Answered: 28 Skipped: 3



ANSWER CHOICES	RESPONSES
Construction & timing information (e.g. when will it be built, closures)	67.86% 19
Bridge design (e.g. what the new bridge will look like, features, art)	75.00% 21
Vehicle traffic improvements or changes	60.71% 17
Improvements for cyclists and pedestrians	50.00% 14
How open land will be used	39.29% 11
How public input has been incorporated	21.43% 6
Total Respondents: 28	

#	OTHER:	DATE
1	Reasons why the bridge cannot be repurposed for another non-vehicular (i.e. cars and trucks) use. Structural arguments provided are poorly explained. Although repairs would be needed, the elimination of vehicular traffic would lessen the structural requirements of this bridge.	2/15/2018 11:56 AM
2	art	2/11/2018 1:04 PM
3	costs ????	2/10/2018 6:46 PM
4	What buildings/businesses will have to be expropriated. How will it affect businesses in terms of traffic flow, street lighting, views, and property values.	2/8/2018 2:02 PM
5	I've heard rumors that you're looking at putting Aboriginal designs in the new bridge. If so, why? This is a bridge for all citizens,	2/4/2018 11:28 AM

## A Better Bridge for Arlington

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6	Beautification	2/1/2018 10:57 PM
7	Condition assessment of existing bridge; attempts undertaken to reuse bridge	2/1/2018 12:07 PM

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## A Better Bridge for Arlington

### Q3 Do you have any questions, comments or concerns that weren't addressed?

Answered: 19 Skipped: 12

#	RESPONSES	DATE
1	No	2/15/2018 7:35 PM
2	The historical experience .. the steep climb up the old bridge .. the arrival at the overhead complexity of the steel trusses .. and the steel descent - is a fond memory of many Winnipeggers.	2/15/2018 11:56 AM
3	not at this time	2/11/2018 1:04 PM
4	Not that I can think of	2/11/2018 1:02 PM
5	how are cost overruns going to be handled How about evening and weekend work to get the project finished ASAP.	2/10/2018 6:46 PM
6	1. Why do we have to have two bike lanes starting at Notre Dame Ave. One lane should be more than adequate for the number of people using bikes in that area. 2. You have reduced the parking availability between Notre Dame and Logan Ave. 3. There is no immediate turn of for North bound traffic wanting to enter the Recreation facility. They have to travel at least 2-3 blocks further and then come back South to access the Centre	2/10/2018 3:11 PM
7	What will happen to the old bridge materials. Will some be saved for museums?	2/8/2018 2:02 PM
8	Preliminary bridge design appears to prioritize improvements for drivers over safety and convenience of pedestrians.	2/7/2018 11:31 AM
9	Will there be increased traffic on Arlington, if so please decide if it's two lane or not through the West End.	2/5/2018 10:24 PM
10	No	2/5/2018 7:23 PM
11	No	2/5/2018 1:54 PM
12	Is this the most long term sustainable approach available ? Is moving the rail yards still an option ?	2/5/2018 11:54 AM
13	why is there only one lane going south? That is a little short-sighted, if you're building a new bridge, put in 2 lanes.	2/3/2018 2:09 PM
14	All of Arlington from Notre Dame north to the bridge and then the bridge and then north of the bridge needs such work. Can't we design beauty into this project?	2/1/2018 10:57 PM
15	Instead of continuing to spend billions of dollars going over and under the railways and living in the roaring Winnipeg days of 1900, we should be getting into the 21st century and spending these billions on getting the railways out of Winnipeg. These billions will be spent afterwards anyway to get them out. Very progressive Winnipeg, and Manitoba, in not looking into the future!!!	2/1/2018 9:54 PM
16	No.	2/1/2018 9:53 PM
17	If arlington is going to be redesigned to included bike lanes (which is great) they should be seperated from traffic, and also include bike boxes at major intersections.	2/1/2018 5:00 PM
18	No, however I'm a little concerned about the proposed cycling features at Logan. Cyclists might be obscured to drivers with the "curved" routing shown through the intersection, not to mention awkward interaction with pedestrians. I realize the final solution might not look like that.	2/1/2018 4:38 PM
19	Reuse of existing structure for alternative to vehicle traffic	2/1/2018 12:07 PM

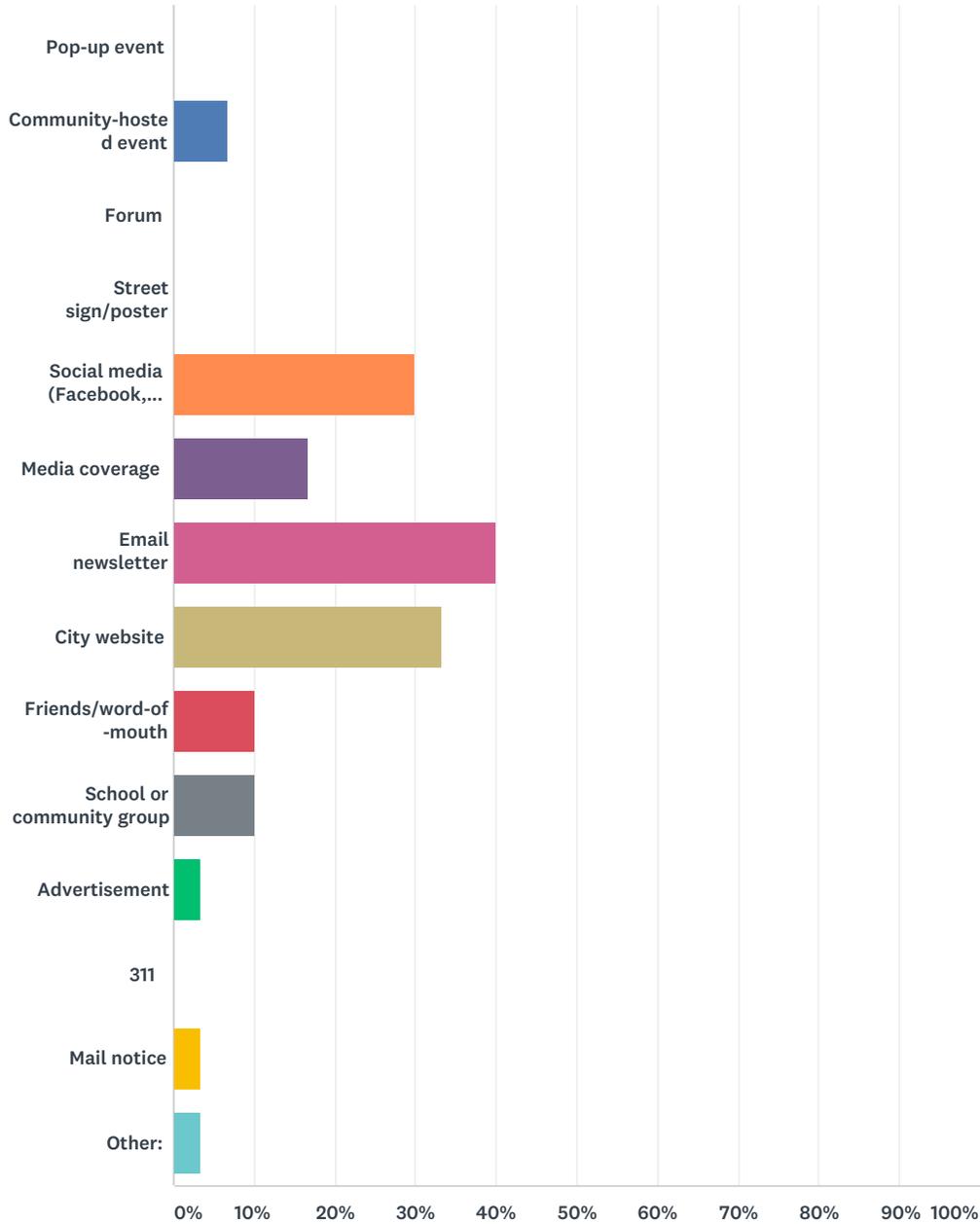
## Q4 Do you have any specific suggestions for how we can improve project information and public engagement in the future?

Answered: 13 Skipped: 18

#	RESPONSES	DATE
1	No	2/15/2018 7:35 PM
2	Do not have a pre-conceived solution.	2/15/2018 11:56 AM
3	Don't make accomodation for cyclists too elaborate.	2/10/2018 6:46 PM
4	Stantec has done a tremendous job!	2/10/2018 3:11 PM
5	Have a plaque/monument/public art describing the history and impact of the old bridge.	2/8/2018 2:02 PM
6	These online surveys are great, I will never go to an open meeting. Online questions live streamed and submitted for a project manager to answer on YouTube.	2/5/2018 10:24 PM
7	No	2/5/2018 7:23 PM
8	suicide barriers	2/5/2018 2:40 PM
9	No	2/5/2018 1:54 PM
10	Ads in the North End Times; School notices informing folks; coffee meetings in diners and eateries and Neechi Commons	2/1/2018 10:57 PM
11	No.	2/1/2018 9:53 PM
12	Overall the method used was effective, however I'm not certain I saw a lot of detail on engagement presented in the news.	2/1/2018 4:38 PM
13	I attended several sessions. None of the comments were included in summary if it did not suit the proposed steps	2/1/2018 12:07 PM

### Q5 How did you hear about this project? (Select all that apply)

Answered: 30 Skipped: 1



ANSWER CHOICES	RESPONSES
Pop-up event	0.00% 0
Community-hosted event	6.67% 2
Forum	0.00% 0
Street sign/poster	0.00% 0
Social media (Facebook, Twitter, YouTube)	30.00% 9

## A Better Bridge for Arlington

Media coverage	16.67%	5
Email newsletter	40.00%	12
City website	33.33%	10
Friends/word-of-mouth	10.00%	3
School or community group	10.00%	3
Advertisement	3.33%	1
311	0.00%	0
Mail notice	3.33%	1
Other:	3.33%	1
Total Respondents: 30		

#	OTHER:	DATE
1	emailed Public Engagement Newsletter	2/15/2018 11:56 AM

Page 8 was removed from the Survey Monkey summary as it contained respondents personal email addresses and contact information.